

# Exhibited

This planning application is open for  
public comment until  
25 May 2026

Reference no	<b>PLN-25-0218</b>
Site	<b>23 NORFOLK STREET PERTH</b>
Proposed Development	<b>Multiple Dwellings (1existing &amp; 5 new)</b>
Zone	<b>8.0 General Residential</b>
Use class	<b>Residential</b>

Written representations may be made during this time to the General Manager;  
mailed to PO Box 156, Longford, Tasmania 7301,  
delivered to Council offices or  
a pdf letter emailed to [planning@nmc.tas.gov.au](mailto:planning@nmc.tas.gov.au)

(no special form required)



NORTHERN  
MIDLANDS  
COUNCIL

## PLANNING APPLICATION

FOR BUILDINGS, WORKS AND CHANGE OF USE  
(E.g. Residential houses, sheds, carports, retaining walls, visitor accommodation, commercial development, signage etc.)

Office Use Only:

### The Proposal

Description of proposal:

MULTIPLE DWELLINGS  
1 EXISTING , 5 NEW DWELLINGS  
DEMOLISH EXISTING OUTBUILDINGS

Driveway construction material:

### The Land

Site address:

23 NORFOLK ST PERTH

Title reference:

C/T: 142477 / 1.

Existing buildings on site:

1

Existing use of site:

RESIDENTIAL

Applicant justification of any variation/discretion to the  
*Tasmanian Planning Scheme – Northern Midlands*

REFER TO DESIGN STATEMENT.

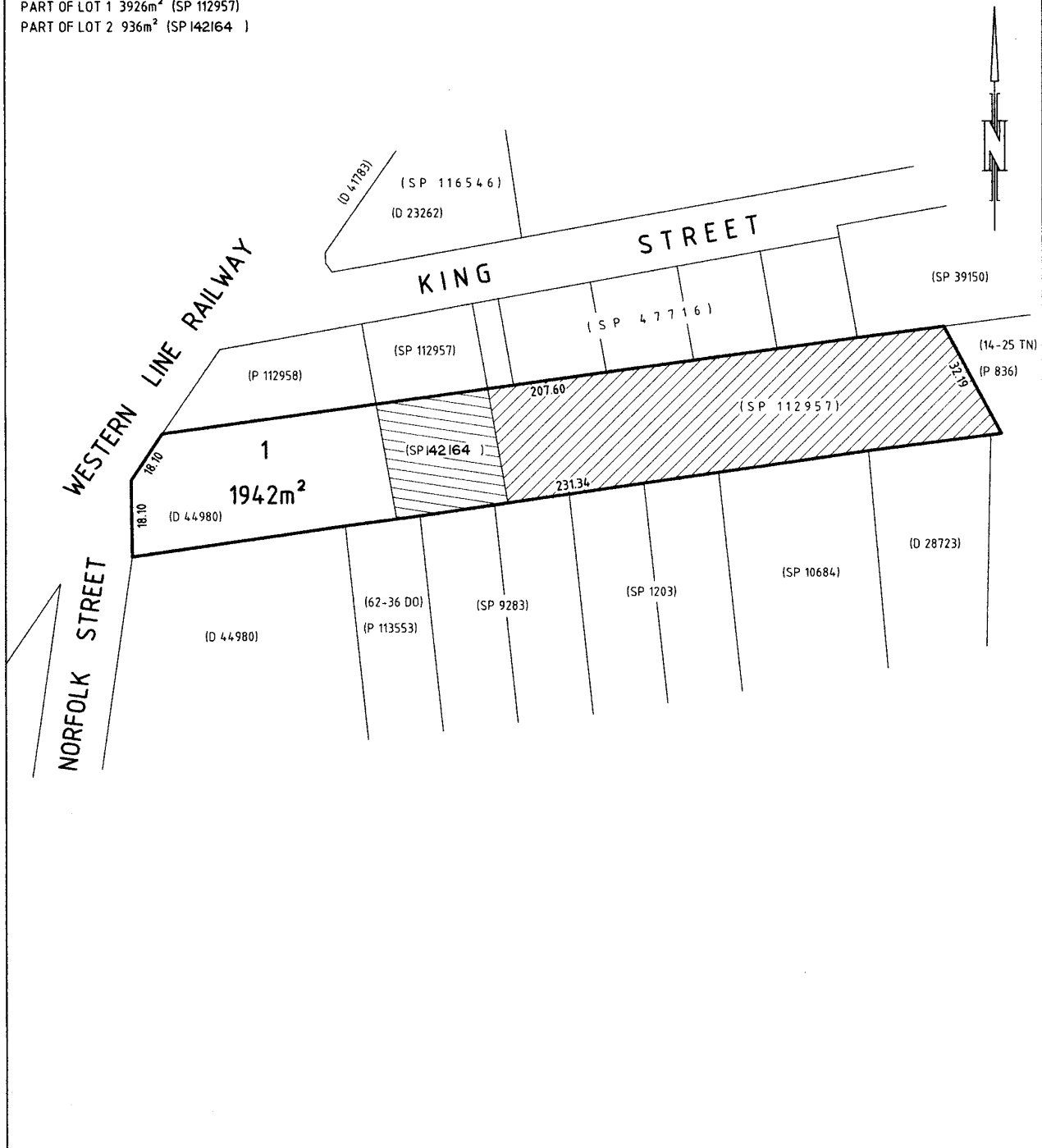
OWNER ERIC CLIVE FITZMAURICE & CLAUDIA MAISIE FITZMAURICE  FOLIO REFERENCE C.T. 112958-1  GRANTEE PART OF LOCATION TO JAMES FENNER & PART OF 20 ACRES GTD TO THOMAS & DALRYMPLE JOHNSON		<b>PLAN OF TITLE</b>  LOCATION <b>TOWN OF PERTH SEC. T</b>  FIRST SURVEY PLAN No. D 48984  COMPILED BY CAMPBELL SMITH PHELPS PEDLEY PTY LTD  NOT TO SCALE      LENGTHS IN METRES		REGISTERED NUMBER <b>P142477</b>  APPROVED - 1. FEB. 2005. <i>Alice Kawa</i> Recorder of Titles
MAPSHEET MUNICIPAL CODE No. 123 (5039-24)	LAST UPI No. 5604405	LAST PLAN No. P 112958	ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN	

**SKETCH BY WAY OF ILLUSTRATION ONLY**

**BALANCE PLAN**

"EXCEPTED LANDS"

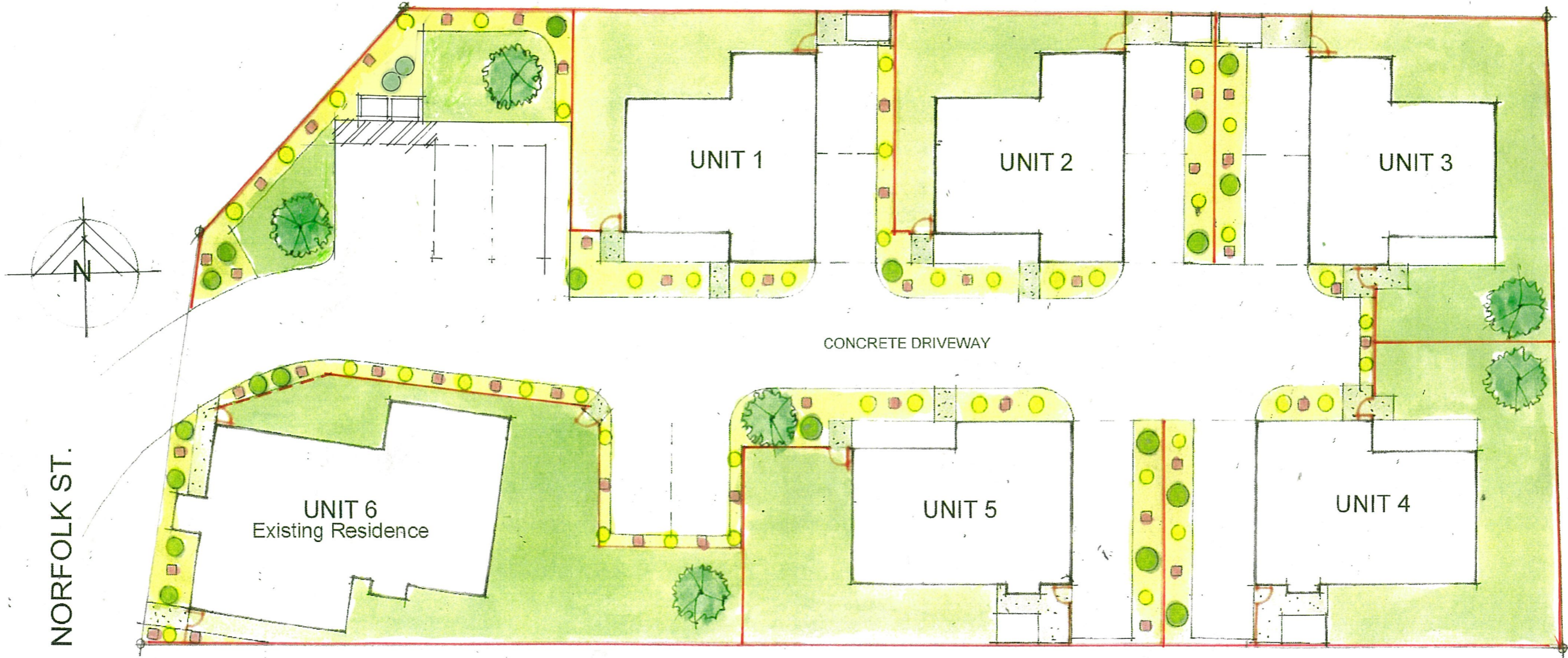
PART OF LOT 1 3926m<sup>2</sup> (SP 112957)  
 PART OF LOT 2 936m<sup>2</sup> (SP 142164 )



# Exhibited

## LEGEND

- Nandina Domestica
- Pittosporum "Golf Ball"
- Lavander
- Pyrus "Capitol" ornamental pear tree
- Pebble Mulch
- Turf
- 900 mm wide concrete path
- 1.8 m high colorbond fence
- 1.2 m high colorbond fence



## LANDSCAPE PLAN

**Response to Additional Information Required for****Planning Application PLN-25-0218****Multiple Dwellings ( 1 existing and 5 new) at 23 Norfolk St Perth.**

1. Elevations have been amended to show finished ground levels in relation to existing natural ground levels. See plans attached.
2. Landscape plan updated to show fencing. Fencing generally will be 1.8m high, solid colorbond metal fencing for security and privacy. A section of fencing to the existing dwelling within 4.5m of the front boundary will be 1.2m high . See plan.
3. Floor plan and elevations of the existing dwelling included in new plans attached.
4. Privacy screens included as required in compliance with clause 8.4.6
5. The proposal achieves a site area per dwelling of 323.6m<sup>2</sup> and does not comply. The site density of the proposal is not overly crowded with good space between individual dwellings. The site area percentage breakdown is as follows:

Dwellings 29.4%

Roadways and parking 26.9%

Open space 43.7%

The site is not surrounded by neighbours with the Western Line Railway to the East. In addition, Norfolk St is discontinued at the property with no thoroughfare beyond the site.

While existing developments at 70a Frederick St ,62 Frederick St and 15 Little Mulgrave St appear to be compliant re density, the proposed development is comparable in a number of aspects more strongly than strictly site area per dwelling compliance and the density aligns with existing neighbourhood character and does not create unreasonable amenity impacts.

- I. Building setbacks are appropriate and generous.
- II. Dwellings are well articulated and provide good separation to increase daylight access and reduce overshadowing, overlooking is mitigated.
- III. Landscape and provision of private open space to individual dwellings is increased.
- IV. Will not create visual bulk.

In our view the proposal meets a demand for affordable ,accessible housing and aligns with the objective of the planning scheme even if it varies from the numerical standard performance based assessment.

6. Traffic Impact Assessment for the site attached.
7. Rail Environmental Noise and Ground Vibration Assessment attached.
8. Waste and recycling collection at the frontage has sufficient space. Refer to landscape plan.

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## DESIGN COMPLIANCE STATEMENT

### RESIDENTIAL- MULTIPLE DWELLINGS

#### PROJECT DETAILS

Project address:	23 NORFOLK STREET, PERTH. 7300
Property Identification Number:	2578279
Locality:	PERTH
Planning Zone:	GENERAL RESIDENTIAL
Certificate of Title:	142477/1
Municipality:	NORTHERN MIDLANDS
Planning Codes Overlay:	AIRPORT OBSTACLE LIMITATION AREA
Planning Scheme:	TASMANIAN PLANNING SCHEME
Local Provisions Schedule:	PERTH SPECIFIC AREA PLAN

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## 1. 8.0 General Residential Zone

### 8.4.1 Residential density for multiple dwellings

Acceptable Solutions	Performance Criteria
<p><b>A1</b> Multiple dwellings must have a site area per dwelling of not less than 325m<sup>2</sup>.</p>	<p><b>P1</b> Multiple dwellings must only have a site area per dwelling that is less than 325m<sup>2</sup>, if the development will not exceed the capacity of infrastructure services and:</p> <ul style="list-style-type: none"><li>(a) is compatible with the density of existing development on established properties in the area; or</li><li>(b) provides for a significant social or community benefit and is:<ul style="list-style-type: none"><li>(i) wholly or partly within 400m walking distance of a public transport stop; or</li><li>(ii) wholly or partly within 400m walking distance of an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone, Central Business Zone or</li></ul></li></ul> <p><b>The proposed development achieves a site area per dwelling of 323.6 m<sup>2</sup>. Refer to NOR-S7.7.1 at the end of this statement.</b></p>

### 8.4.2 Setbacks and Building envelope for all dwellings

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Acceptable Solutions	Performance Criteria
<p><b>A1</b></p> <p>Unless within a building area on a sealed plan, a dwelling, excluding garages, carports and protrusions that extend not more than 0.9m into the frontage setback, must have a setback from a frontage that is: (a) if the frontage is a primary frontage, not less than 4.5m, or, if the setback from the primary frontage is less than 4.5m, not less than the setback, from the primary frontage, of any existing dwelling on the site; (b) if the frontage is not a primary frontage, not less than 3m, or, if the setback from the frontage is less than 3m, not less than the setback, from a frontage that is not a primary frontage, of any existing dwelling on the site; (c) if for a vacant site and there are existing dwellings on adjoining properties on the same street, not more than the greater, or less than the lesser, setback for the equivalent frontage of the dwellings on the adjoining sites on the same street; or (d) if located above a non-residential use at ground floor level, not less than the setback from the frontage of the ground floor level.</p>	<p><b>P1</b></p> <p>A dwelling must have a setback from a frontage that is compatible with the streetscape, having regard to any topographical constraints</p> <ul style="list-style-type: none"> <li>a) Existing dwelling</li> <li>b) N/A</li> <li>c) Complies</li> <li>d) N/A</li> </ul>
<p><b>A2</b></p> <p>A garage or carport for a dwelling must have a setback from a primary frontage of not less than:</p> <ul style="list-style-type: none"> <li>(a) 5.5m, or alternatively 1m behind the building line;</li> <li>(b) the same as the building line, if a portion of the dwelling gross floor area is located above the garage or carport; or</li> <li>(c) 1m, if the existing ground level slopes up or down at a gradient steeper than 1 in 5 for a distance of 10m from the frontage.</li> </ul>	<p><b>P2</b></p> <p>A garage or carport for a dwelling must have a setback from a primary frontage that is compatible with the setbacks of existing garages or carports in the street, having regard to any topographical constraints.</p> <ul style="list-style-type: none"> <li>a) N/A</li> <li>b) N/A</li> <li>c) N/A</li> </ul>
<p><b>A3</b></p> <p>A dwelling, excluding outbuildings with a building height of not more than 2.4m and protrusions that extend not more than 0.9m horizontally beyond the building envelope, must:</p> <ul style="list-style-type: none"> <li>(a) be contained within a building envelope (refer to Figures 8.1, 8.2 and 8.3) determined by: <ul style="list-style-type: none"> <li>(i) a distance equal to the frontage setback or, for an internal lot, a distance of 4.5m from</li> </ul> </li> </ul>	<p><b>P3</b></p> <p>The siting and scale of a dwelling must:</p> <ul style="list-style-type: none"> <li>(a) not cause an unreasonable loss of amenity to adjoining properties, having regard to: <ul style="list-style-type: none"> <li>(i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining property;</li> <li>(ii) overshadowing the private open space of a dwelling on an adjoining property;</li> <li>(iii) overshadowing of an adjoining vacant property, and</li> </ul> </li> </ul>

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<p>the rear boundary of a property with an adjoining frontage; and</p> <p>(ii) projecting a line at an angle of 45 degrees from the horizontal at a height of 3m above existing ground level at the side and rear boundaries to a building height of not more than 8.5m above existing ground level; and</p> <p>(b) only have a setback of less than 1.5m from a side or rear boundary if the dwelling:</p> <p>(i) does not extend beyond an existing building built on or within 0.2m of the boundary of the adjoining property; or</p> <p>(ii) does not exceed a total length of 9m or one third the length of the side boundary (whichever is the lesser )</p>	<p>(iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining property;</p> <p>(b) provide separation between dwellings on adjoining properties that is consistent with that existing on established properties in the area; and</p> <p>(c) not cause an unreasonable reduction in sunlight to an existing solar energy installation on:</p> <p>(i) an adjoining property; or</p> <p>(ii) another dwelling on the same site.</p> <p>(a)(i) Complies (ii) Complies (b) N/A</p>
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## 8.4.3 Site Coverage and private open space for all dwellings

Acceptable Solutions	Performance Criteria
<p><b>A1</b></p> <p>Dwellings must have:</p> <p>(a) a site coverage of not more than 50% (excluding eaves up to 0.6m wide); and</p> <p>(b) for multiple dwellings, a total area of private open space of not less than 60m<sup>2</sup> associated with each dwelling, unless the dwelling has a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer)</p>	<p><b>P1</b></p> <p>Dwellings must have:</p> <p>(a) site coverage consistent with that existing on established properties in the area;</p> <p>(b) private open space that is of a size and with dimensions that are appropriate for the size of the dwelling and is able to accommodate:</p> <p>(i) outdoor recreational space consistent with the projected requirements of the occupants and, for multiple dwellings, take into account any common open space provided for this purpose within the development; and</p> <p>(ii) operational needs, such as clothes drying and storage; and</p> <p>(c) reasonable space for the planting of gardens and landscaping.</p> <p>(a) Complies -site cover 29.4 % (b) Complies</p>
<p><b>A2</b></p> <p>A dwelling must have private open space that: (a) is in one location and is not less than:</p> <p>(i) 24m<sup>2</sup>; or</p> <p>(ii) 12m<sup>2</sup>, if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 1.8m above the finished</p>	<p><b>P2</b></p> <p>A dwelling must have private open space that includes an area capable of serving as an extension of the dwelling for outdoor relaxation, dining, entertaining and children's play and is:</p> <p>(a) conveniently located in relation to a living area of the dwelling; and</p> <p>(b) orientated to take advantage of sunlight.</p>

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<p>ground level (excluding a garage, carport or entry foyer);</p> <p>(b) has a minimum horizontal dimension of not less than:</p> <p>(i) 4m; or</p> <p>(ii) 2m, if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer);</p> <p>(c) is located between the dwelling and the frontage only if the frontage is orientated between 30 degrees west of true north and 30 degrees east of true north; and</p> <p>(d) has a gradient not steeper than 1 in 10.</p>	<p>(a)(i) Complies</p> <p>(b)(i) Complies</p> <p>(ii) N/A</p> <p>(c) N/A</p> <p>(d) Complies</p>
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## 8.4.4 Sunlight to private open space of multiple dwellings

Acceptable Solutions	Performance Criteria
<p><b>A1</b></p> <p>A multiple dwelling, that is to the north of the private open space of another dwelling on the same site, required to satisfy A2 or P2 of clause 8.4.3, must satisfy(a) or (b), unless excluded by (c):</p> <p>(a) the multiple dwelling is contained within a line projecting (see Figure 8.4):</p> <p>(i) at a distance of 3m from the northern edge of the private open space; and</p> <p>(ii) vertically to a height of 3m above existing ground level and then at an angle of 45 degrees from the horizontal;</p> <p>(b) the multiple dwelling does not cause 50% of the private open space to receive less than 3 hours of sunlight between 9.00am and 3.00pm on 21st June; and</p> <p>(c) this Acceptable Solution excludes that part of a multiple dwelling consisting of:</p> <p>(i) an outbuilding with a building height not more than 2.4m; or</p> <p>(ii) protrusions that extend not more than 0.9m horizontally from the multiple dwelling.</p>	<p><b>P1</b></p> <p>A multiple dwelling must be designed and sited to not cause an unreasonable loss of amenity by overshadowing the private open space, of another dwelling on the same site, which is required to satisfy A2 or P2 of clause 8.4.3 of this planning scheme.</p> <p>(a) Complies</p> <p>(b) All POS to have northern aspect. Complies</p> <p>(c) N/A</p>

## 8.4.5 Width of openings for garages and carports for all dwellings.

Acceptable Solutions	Performance Criteria
<p><b>A1</b></p> <p>A garage or carport for a dwelling within 12m of a primary frontage, whether the garage or carport is free-standing or part of the</p>	<p><b>P1</b></p> <p>A garage or carport for a dwelling must be designed to minimise the width of its openings that are visible from the street, so</p>

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<p>dwelling, must have a total width of openings facing the primary frontage of not more than 6m or half the width of the frontage (whichever is the lesser).</p>	<p>as to reduce the potential for the openings of a garage or carport to dominate the primary frontage.</p> <p style="text-align: center;">A1 N/A</p>
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## 8.4.6 Privacy for all dwellings

Acceptable Solutions	Performance Criteria
<p><b>A1</b> A balcony, deck, roof terrace, parking space, or carport for a dwelling (whether freestanding or part of the dwelling), that has a finished surface or floor level more than 1m above existing ground level must have a permanently fixed screen to a height of not less than 1.7m above the finished surface or floor level, with a uniform transparency of not more than 25%, along the sides facing a:</p> <p>(a) side boundary, unless the balcony, deck, roof terrace, parking space, or carport has a setback of not less than 3m from the side boundary;</p> <p>(b) rear boundary, unless the balcony, deck, roof terrace, parking space, or carport has a setback of not less than 4m from the rear boundary; and</p> <p>(c) dwelling on the same site, unless the balcony, deck, roof terrace, parking space, or carport is not less than 6m:</p> <p>(i) from a window or glazed door, to a habitable room of the other dwelling on the same site; or</p> <p>(ii) from a balcony, deck, roof terrace or the private open space of the other dwelling on the same site.</p>	<p><b>P1</b> A balcony, deck, roof terrace, parking space or carport for a dwelling (whether freestanding or part of the dwelling) that has a finished surface or floor level more than 1m above existing ground level, must be screened, or otherwise designed, to minimise overlooking of:</p> <p>(a) a dwelling on an adjoining property or its private open space; or</p> <p>(b) another dwelling on the same site or its private open space.</p> <p style="text-align: center;">(a) Carspaces to be screened from other dwellings on site by a 1.8m high colorbond fence.</p> <p style="text-align: center;">(b) Ditto</p> <p style="text-align: center;">(c) Ditto</p>
<p><b>A2</b> A window or glazed door to a habitable room of a dwelling, that has a floor level more than 1m above existing ground level, must satisfy (a), unless it satisfies (b):</p> <p>(a) the window or glazed door:</p> <p>(i) is to have a setback of not less than 3m from a side boundary;</p> <p>(ii) is to have a setback of not less than 4m from a rear boundary;</p> <p>(iii) if the dwelling is a multiple dwelling, is to be not less than 6m from a window or</p>	<p><b>P2</b> A window or glazed door to a habitable room of a dwelling that has a floor level more than 1m above existing ground level, must be screened, or otherwise located or designed, to minimise direct views to:</p> <p>(a) a window or glazed door, to a habitable room of another dwelling; and</p> <p>(b) the private open space of another dwelling.</p> <p style="text-align: center;">(a) Complies</p> <p style="text-align: center;">(b) Complies</p>

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<p>glazed door, to a habitable room, of another dwelling on the same site; and</p> <p>(iv) if the dwelling is a multiple dwelling, is to be not less than 6m from the private open space of another dwelling on the same site.</p> <p>(b) the window or glazed door:</p> <p>(i) is to be offset, in the horizontal plane, not less than 1.5m from the edge of a window or glazed door, to a habitable room of another dwelling;</p> <p>(ii) is to have a sill height of not less than 1.7m above the floor level or have fixed obscure glazing extending to a height of not less than 1.7m above the floor level; or</p> <p>(iii) is to have a permanently fixed external screen for the full length of the window or glazed door, to a height of not less than 1.7m above floor level, with a uniform transparency of not more than 25%.</p>	
<p><b>A3</b></p> <p>A shared driveway or parking space (excluding a parking space allocated to that dwelling) must be separated from a window, or glazed door, to a habitable room of a multiple dwelling by a horizontal distance of not less than:</p> <p>(a) 2.5m; or</p> <p>(b) 1m if:</p> <p>(i) it is separated by a screen of not less than 1.7m in height; or</p> <p>(ii) the window, or glazed door, to a habitable room has a sill height of not less than 1.7m above the shared driveway or parking space, or has fixed obscure glazing extending to a height of not less than 1.7m above the floor level.</p>	<p><b>P3</b></p> <p>A shared driveway or parking space (excluding a parking space allocated to that dwelling), must be screened, or otherwise located or designed, to minimise unreasonable impact of vehicle noise or vehicle light intrusion to a habitable room of a multiple dwelling</p> <p style="text-align: center; color: blue;">A3 Windows less than 2.5m to the driveway will be screened not less than 1.7m in height</p>

## 8.4.7 Frontage fences for all dwellings

Acceptable Solutions	Performance Criteria
<p><b>A1</b></p> <p>No Acceptable Solution.</p>	<p><b>P1</b></p> <p>A fence (including a free-standing wall) for a dwelling within 4.5m of a frontage must:</p> <p>(a) provide for security and privacy while allowing for passive surveillance of the road; and</p> <p>(b) be compatible with the height and transparency of fences in the street, having regard to:</p> <p>(i) the topography of the site; and</p>

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	(ii) traffic volumes on the adjoining road.  A1 Will Comply
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## 8.4.8 Waste storage for multiple dwellings

Acceptable Solutions	Performance Criteria
<p><b>A1</b></p> <p>A multiple dwelling must have a storage area, for waste and recycling bins, that is not less than 1.5m<sup>2</sup> per dwelling and is within one of the following locations:</p> <p>(a) an area for the exclusive use of each dwelling, excluding the area in front of the dwelling; or</p> <p>(b) a common storage area with an impervious surface that:</p> <p>(i) has a setback of not less than 4.5m from a frontage;</p> <p>(ii) is not less than 5.5m from any dwelling; and</p> <p>(iii) is screened from the frontage and any dwelling by a wall to a height not less than 1.2m above the finished surface level of the storage area.</p>	<p><b>P1</b></p> <p>A multiple dwelling must have storage for waste and recycling bins that is:</p> <p>(a) capable of storing the number of bins required for the site;</p> <p>(b) screened from the frontage and any dwellings; and</p> <p>(c) if the storage area is a common storage area, separated from any dwellings to minimise impacts caused by odours and noise.</p> <p>a) Exclusive bin areas to be provided to each dwelling</p> <p>b) N/A</p> <p>c) N/A</p>

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## NOR-S7.7 Development Standards for Buildings and Works

### NOR-S7.7.1 Residential density for multiple dwelling

Acceptable Solutions	Performance Criteria
<p>A1</p> <p>Multiple dwellings must have a site area per dwelling means the area of a site, excluding any access strip, divided by the number of dwellings on that site. of not less than 400m<sup>2</sup>.</p>	<p>P1</p> <p>Multiple dwellings must only have a site area per dwelling that is less than 400m<sup>2</sup>, if the development will not exceed the capacity of infrastructure services and:</p> <ul style="list-style-type: none"><li>(a) is compatible with the density of existing development on established properties within the area; or</li><li>(b) provides for a significant social or community benefit and is:<ul style="list-style-type: none"><li>(i) wholly or partly within 400m walking distance of a public transport stop; or</li><li>(ii) wholly or partly within 400m walking distance of a Village Zone, Local Business Zone, or General Business Zone.</li></ul></li></ul> <p>A1 The proposal achieves a site area per dwelling of 323.6m<sup>2</sup> and does not comply. The site density of the proposal is not overly crowded with good space between individual dwellings. The site area percentage breakdown is as follows:</p> <ul style="list-style-type: none"><li>Dwellings 29.4%</li><li>Roadways and parking 26.9%</li><li>Open space 43.7%</li></ul> <p>The site is not surrounded by neighbours with the Western Line Railway to the East. In addition, Norfolk St is discontinued at the property with no thoroughfare beyond the site. The proposed density is compatible with existing development within the area. Similar multiple dwelling developments are at 72 Frederick St and 62 Frederick St .</p> <p>72 Frederick St is a 4 unit development with a <b>site area density of 376.2 m<sup>2</sup> per</b></p>

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	<p><b>unit.</b> The site area breakdown is as follows: Dwellings 24.7% Roadway and parking 30.2% Open space 45.1%</p> <p>62 Frederick St is an 8 unit development with a <b>site area density of 379.6 m2 per unit.</b>The site area breakdown is as follows: Dwellings 24.0% Roadways and parking 41.2% Open space 34.8%</p>
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Received

20.2.2026

Exhibited

Tarkarri  
Engineering



Air Quality • Acoustics • Environment • Vibration

## Technical Memo

9 February 2026

Abode Designer Homes  
Warehouse 8/13/17 Merino St,  
Kings Meadows TAS 7249

7134\_AC/VIB\_R  
HH/AJM

Attn: Mr Simon Hughes

Dear Sir,

RE: 23 Norfolk St, Perth, rail environmental noise and ground vibration assessment.

Please find below a rail environmental noise and ground assessment for a proposed residential development at 23 Norfolk St, Perth.

### 1. INTRODUCTION

Tarkarri Engineering has been engaged by Abode Designer Homes to assess rail noise and ground vibration levels for a proposed residential development at 23 Norfolk St, Perth. The testing was commissioned to assess ground vibration and airborne noise generated from the nearby railway line. The proposed development is located on the southern side of TasRail's Western Line.

The assessment is applicable under clause C3.6.1 *Habitable buildings for sensitive uses within a road or railway attenuation area* of the Tasmanian Planning Scheme with the assessment addressing A1(c) with non-compliance triggering an assessment under P1. Relevant sections of the planning scheme are provided below.

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Tarkarri Engineering Pty Ltd  
ABN 98 009 561 488  
PO Box 506 Kings Meadows  
Tasmania 7249 Australia



ADH – 23 Norfolk St, Perth, rail environmental noise and ground vibration assessment.

Objective:	To minimise the effects of noise, vibration, light and air emissions on sensitive uses within a road or railway attenuation area, from existing and future major roads and the rail network.	
<b>Acceptable Solutions</b>	<b>Performance Criteria</b>	
<b>A1</b> Unless within a building area on a sealed plan approved under this planning scheme, habitable buildings for a sensitive use within a road or railway attenuation area, must be:  (a) within a row of existing habitable buildings for sensitive uses and no closer to the existing or future major road or rail network than the adjoining habitable building;  (b) an extension which extends no closer to the existing or future major road or rail network than:  (i) the existing habitable building; or  (ii) an adjoining habitable building for a sensitive use; or  (c) located or designed so that external noise levels are not more than the level in Table C3.2 measured in accordance with Part D of the <i>Noise Measurement Procedures Manual, 2<sup>nd</sup> edition, July 2008</i> .	<b>P1</b> Habitable buildings for sensitive uses within a road or railway attenuation area, must be sited, designed or screened to minimise adverse effects of noise, vibration, light and air emissions from the existing or future major road or rail network, having regard to:  (a) the topography of the site;  (b) the proposed setback;  (c) any buffers created by natural or other features;  (d) the location of existing or proposed buildings on the site;  (e) the frequency of use of the rail network;  (f) the speed limit and traffic volume of the road;  (g) any noise, vibration, light and air emissions from the rail network or road;  (h) the nature of the road;  (i) the nature of the development;  (j) the need for the development;	

Table C3.2 Acceptable noise levels within a road or railway attenuation area

<b>Railways</b>
A 24-hour Leq and Lmax noise level of 65 dB(A) and 87dB(A) Lmax assessed as a single event maximum sound pressure level.

Tarkari Engineering proposes the following to address Performance Criteria requirements for ground vibration as follows:

- Measure ground vibration levels from rail pass-by events at the site of the proposed development and assess against 'NSW Department of Environment and Conservation (2006) *Assessing Vibration: a technical guideline*' criteria. Provide recommendations for mitigation if required.

**NB:** Air pollution impacts are not addressed in this report due to the infrequent nature of train pass-bys on the Western Line precluding significant air emission impacts. Light emissions are not addressed here.

Figure 1-1 presents an aerial view of 23 Norfolk St, Perth, with the approx. measurement location indicated in turquoise. Figure 1-2 presents a site plan of the proposed development with the approx. GVM and SLM location marked.



ADH – 23 Norfolk St, Perth, rail environmental noise and ground vibration assessment.

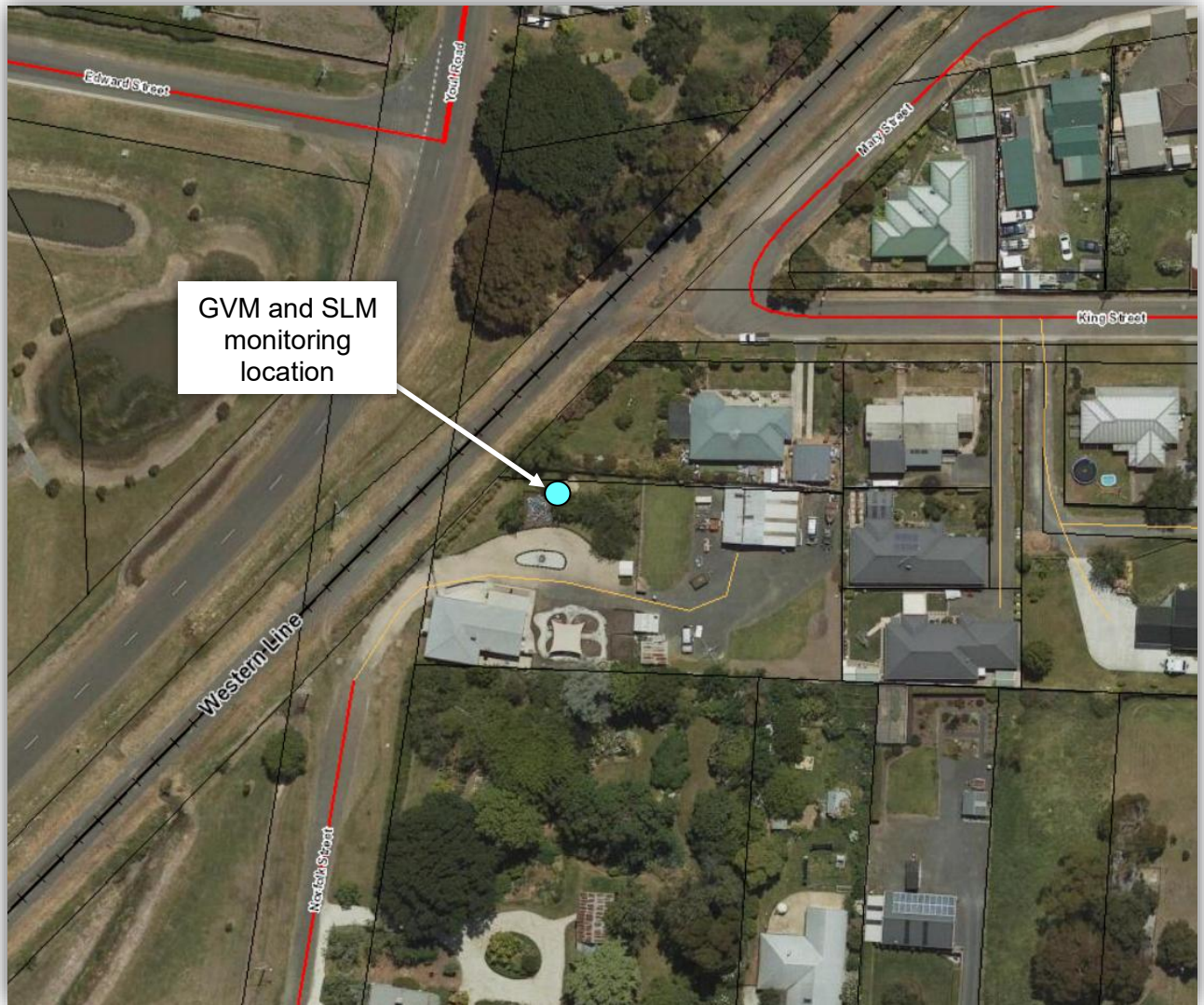


Figure 1-1: Aerial view of 23 Norfolk St, Perth, and surrounds.



ADH – 23 Norfolk St, Perth, rail environmental noise and ground vibration assessment.

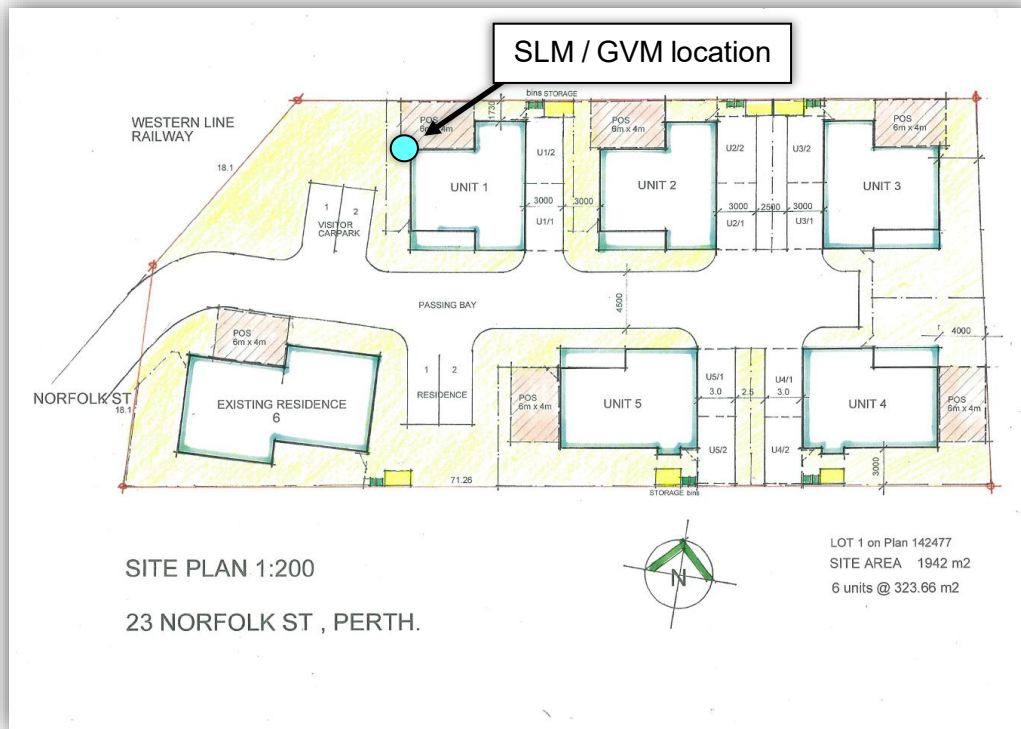


Figure 1-2: Site plan of 23 Norfolk St, provided by Abode Designer Homes.

## 2. MEASUREMENT PROCEDURE

A logging sound level meter (SLM) and ground vibration meter (GVM) were located at 23 Norfolk St for a period of approximately 6 days (see Figure 1-1 for approx. location) between 16 and 21 January 2026. The meters were positioned at the following approx. distances from the rail corridor track centreline:

- SLM: 16 m (9 m from the rail easement)
- GVM: 16 m (19 m from the rail easement)

Figure 2-1 shows the SLM and GVM geophone in place. The following instrumentation was utilised:

- Environmental noise analyser Larson Davis 831C, s/n 12838, measuring A-weighted  $L_n$  and  $L_{Aeq}$  statistics at 1-minute intervals.
- Instanetl Minimate Plus GVM, s/n BE12735, measuring peak particle velocity in mm/s at 1-minute intervals.



ADH – 23 Norfolk St, Perth, rail environmental noise and ground vibration assessment.

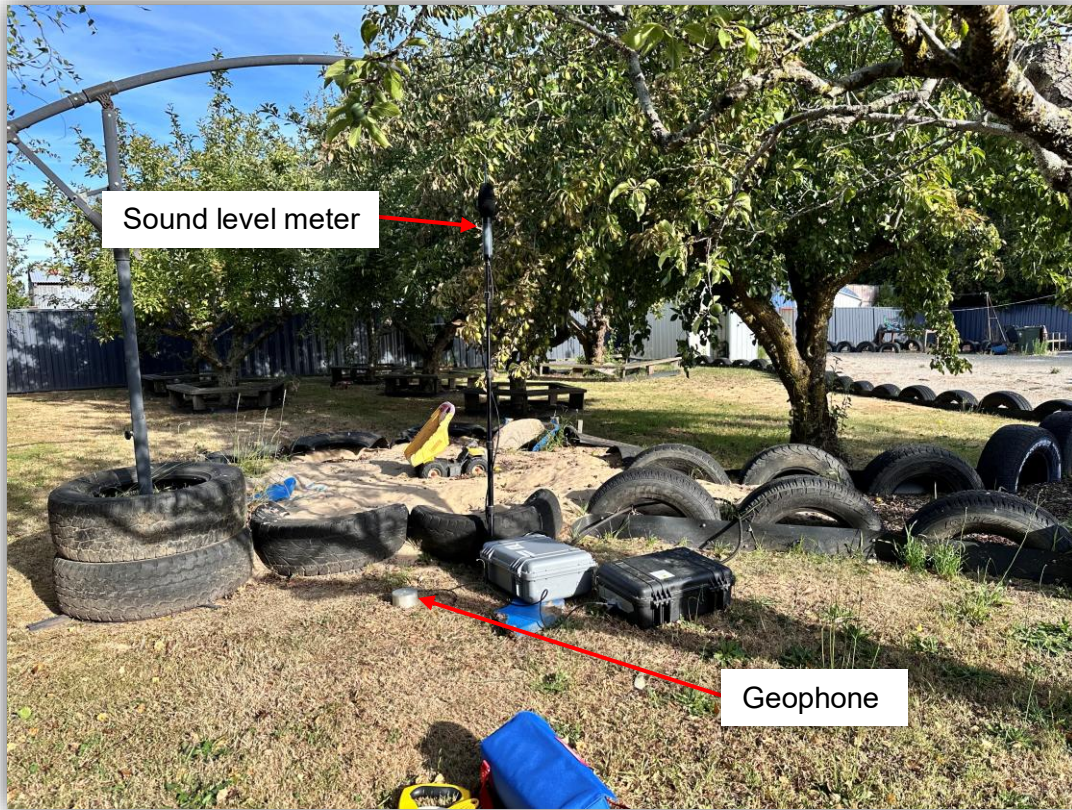


Figure 2-1: Photo of SLM and GVM location.

### 3. ENVIRONMENTAL NOISE

#### 3.1 Measured levels

Figure 3-1 below presents a time trace of the  $L_{Amax,1min}$  environmental noise statistics logged at the measurement position.



ADH – 23 Norfolk St, Perth, rail environmental noise and ground vibration assessment.

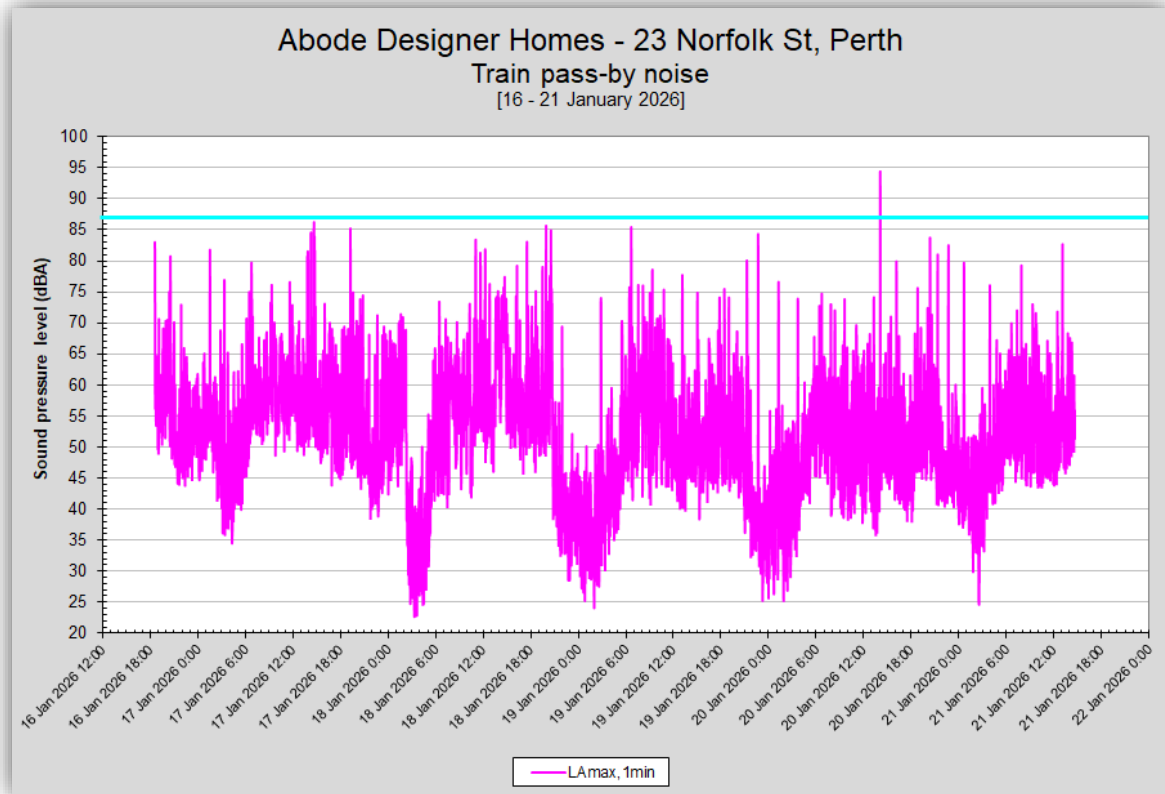


Figure 3-1: Time trace of and L<sub>Amax,1min</sub> sound pressure levels.

The measurement results show L<sub>Amax</sub> levels are typically below 87 dBA through the measurement period. This is considered acceptable, falling under the criterion of 87 dBA. The 94 dBA L<sub>Amax</sub> level on 20 January at 14:06 does not coincide with a peak in ground vibration that typically occur during a train pass-by event. Given this, it is likely this L<sub>Amax</sub> level was generated by a source external to the railway.

No recommendations are provided, due to the acceptable noise level of train pass-bys at the proposed site.

**NB:** The acceptable solution L<sub>Aeq</sub> criterion isn't considered here with train pass-bys not frequent enough to significantly impact L<sub>Aeq,24hr</sub> values.

## 4. GROUND VIBRATION

Under the *NSW EPA (2013) Rail Infrastructure Noise Guideline* for the assessment of vibration generated by train movements, assessors are redirected to the *NSW Department of Environment and Conservation (2006) Assessing Vibration: a technical guideline* and advised to consider rail generated vibration as intermittent. The frequency of train pass-bys on the Western Line is deemed not suitable for an intermittent assessment and the guideline's impulsive vibration exposure criteria for night are applied here. These are as follows:

- Preferred: 2.8 mm/s (peak velocity).
- Maximum: 5.6 mm/s (peak velocity).



ADH – 23 Norfolk St, Perth, rail environmental noise and ground vibration assessment.

Measured ground vibration levels were consistently below 1 mm/s (peak velocity), which is well below the criteria. Therefore, no recommendations are given. Vibration may be perceptible but highly unlikely to cause structural or cosmetic damage.

I hope this information meets your immediate requirements.

Please contact me directly if you have any questions concerning this work.

Yours faithfully,  
Tarkarri Engineering Pty Ltd

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20/4/2026

Evan Smith  
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Cleanaway Pty Ltd  
ABN: 79 000 164 938

37 Birch Avenue,  
Newstead TAS 7249  
Australia  
P +61 499 334 302

Dear Evan and Simon,

Services confirmed for site at 23 Norfolk St, Perth.

I would like to write to confirm that Cleanaway will service the sites waste management on a weekly basis for both the General and Comingle waste. The suggested size of the bins would be a minimum of 660 litres each.

Yours sincerely

*Kelly Locatelli*

Kelly Locatelli

Territory Manager - Tasmania

# Exhibited





**23 NORFOLK STREET, PERTH**

**UNIT DEVELOPMENT**

**TRAFFIC IMPACT ASSESSMENT**

**FEBRUARY 2026**





## 23 Norfolk Street, Perth Unit Development

### TRAFFIC IMPACT ASSESSMENT

- Final
- February 2026

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## Document history and status

Revision	Date issued	Reviewed by	Approved by	Date approved	Revision type
1	17 <sup>th</sup> Feb 2026	R Burk	R Burk	17 <sup>th</sup> Feb 2026	Draft
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# 1. Introduction

## 1.1 Background

This TIA reviews the proposed multiple dwelling development at 23 Norfolk Street, Perth. The review considers the adjacent road network, road safety, parking requirements and impact of traffic due to the proposal.

This Traffic Impact Assessment (TIA) should be submitted with the development application for the proposal and has been prepared based on Department of State Growth guidelines and provides details as follows:

- Anticipated additional traffic and pedestrian movements
- The significance of the impact of these movements on the existing road network
- Any changes required to accommodate the additional traffic

## 1.2 Objectives

A traffic impact assessment is a means for assisting in the planning and design of sustainable development proposals that consider:

- Safety and capacity
- Equity and social justice
- Economic efficiency and the environment and
- Future development with traffic projections for 10 years

## 1.3 Scope of Traffic Impact Assessment (TIA)

This TIA considers in detail the impact of the proposal on Norfolk Street.

## 1.4 References

- AS 1742.1 – 2014 – General introduction and index of signs
- AS /NZS 2890.1 - 2004 – Off-street carparking
- AS /NZS 2890.2 - 2004 – Off-street commercial vehicle facilities
- NSW Guide to Transport Impact Assessment 2024
- Tasmanian Planning Scheme – Northern Midlands
- Austroads Guidelines
  - Road Design Part 4A: Unsignalised & Signalised Intersections 2023
  - Traffic Management Part 6: Intersections, Interchanges & Crossings 2020.



## 1.5 Statement of Qualifications and Experience

This TIA has been prepared by Richard Burk, an experienced and qualified traffic engineer in accordance with the requirements of the Department of State Growth's guidelines and Council's requirements.

Richard Burk is an experienced and qualified traffic engineer with:

- 38 years professional experience in road and traffic engineering industry
  - Director Traffic and Civil Service Pty Ltd since May 2017.
  - Manager Traffic Engineering at the Department of State Growth until May 2017.
  - Previous National committee membership with Austroads Traffic Management Working Group and State Road Authorities Pavement Marking Working Group
- Certified Professional Engineer with Engineers Australia
- Master of Traffic, Monash University, 2004
- Post Graduate Diploma in Management, Deakin University, 1995
- Bachelor of Civil Engineering, University of Tasmania, 1987

A handwritten signature in blue ink, appearing to read 'R Burk', is positioned above the printed name.

Richard Burk

BE (Civil) M Traffic Dip Man. MIE Aust CPEng

Director Traffic and Civil Services Pty Ltd



## 1.6 Glossary of Terms

AADT	Annual Average Daily Traffic - The total number of vehicles travelling in both directions passing a point in a year divided by the number of days in a year.
Acceleration Lane	An auxiliary lane used to allow vehicles to increase speed without interfering with the main traffic stream. It is often used on the departure side of intersections.
Access	The driveway by which vehicles and/or pedestrians enter and/or leave the property adjacent to a road.
ADT	Average Daily Traffic – The average 24-hour volume being the total number of vehicles travelling in both directions passing a point in a stated period divided by the stated number of days in that period.
Austroads	The Association of Australian and New Zealand road transport and traffic authorities and includes the Australian Local Government Association.
Delay	The additional travel time experienced by a vehicle or pedestrian with reference to a base travel time (e.g. the free flow travel time).
DSG	Department of State Growth – The Tasmanian Government Department which manages the State Road Network.
GFA	Gross Floor Area
Intersection Kerb	The place at which two or more roads meet or cross. A raised border of rigid material formed at the edge of a carriageway, pavement or bridge.
km/h	Kilometres per hour
Level of Service	An index of the operational performance of traffic on a given traffic lane, carriageway or road when accommodating various traffic volumes under different combinations of operating conditions. It is usually defined in terms of the convenience of travel and safety performance.
m	Metres
Median	A strip of road, not normally intended for use by traffic, which separates carriageways for traffic in opposite directions. Usually formed by painted lines, kerbed and paved areas grassed areas, etc.
Movement	A stream of vehicles that enters from the same approach and departs from the same exit (i.e. with the same origin and destination).
Phase	The part of a signal cycle during which one or more movements receive right-of-way subject to resolution of any vehicle or pedestrian conflicts by priority rules. A phase is identified by at least one movement gaining right-of-way at the start of it and at least one movement losing right-of-way at the end of it.



Sight Distance	The distance, measured along the road over which visibility occurs between a driver and an object or between two drivers at specific heights above the carriageway in their lane of travel.
Signal Phasing	Sequential arrangement of separately controlled groups of vehicle and pedestrian movements within a signal cycle to allow all vehicle and pedestrian movements to proceed.
SISD	Safe Intersection Sight Distance – The sight distance provides sufficient distance for a driver of a vehicle on the major road to observe a vehicle on a minor road approach moving into a collision situation and to decelerate to a stop before reaching the collision point.
Speed	Distance travelled per unit time.
85th Percentile	The speed at which 85% of car drivers will travel slower and 15% will travel faster. A control method that allows a variable sequence and variable duration of signal displays depending on vehicle and pedestrian traffic demands.
Traffic-actuated Control	A control method that allows a variable sequence and variable duration of signal displays depending on vehicle and pedestrian traffic demands.
Traffic Growth Factor	A factor used to estimate the percentage annual increase in traffic volume.
Trip	A one-way vehicular movement from one point to another excluding the return journey. Therefore, a vehicle entering and leaving a land use is counted as two trips. (RTA Guide to Traffic generating Developments).
Turning Movement	The number of vehicles observed to make a particular turning movement (left or right turn, or through movement) at an intersection over a specified period.
Turning Movement Count	A traffic count at an intersection during which all turning movements are recorded.
Vehicle Actuated Traffic Signals	Traffic signals in which the phasing varies in accordance with the detected presence of vehicles on the signal approaches.
vpd	vehicles per day – The number of vehicles travelling in both directions passing a point during a day from midnight to midnight.
vph	vehicles per hour – The number of vehicles travelling in both directions passing a point during an hour.

## 1.7 Site Specific Glossary of Terms

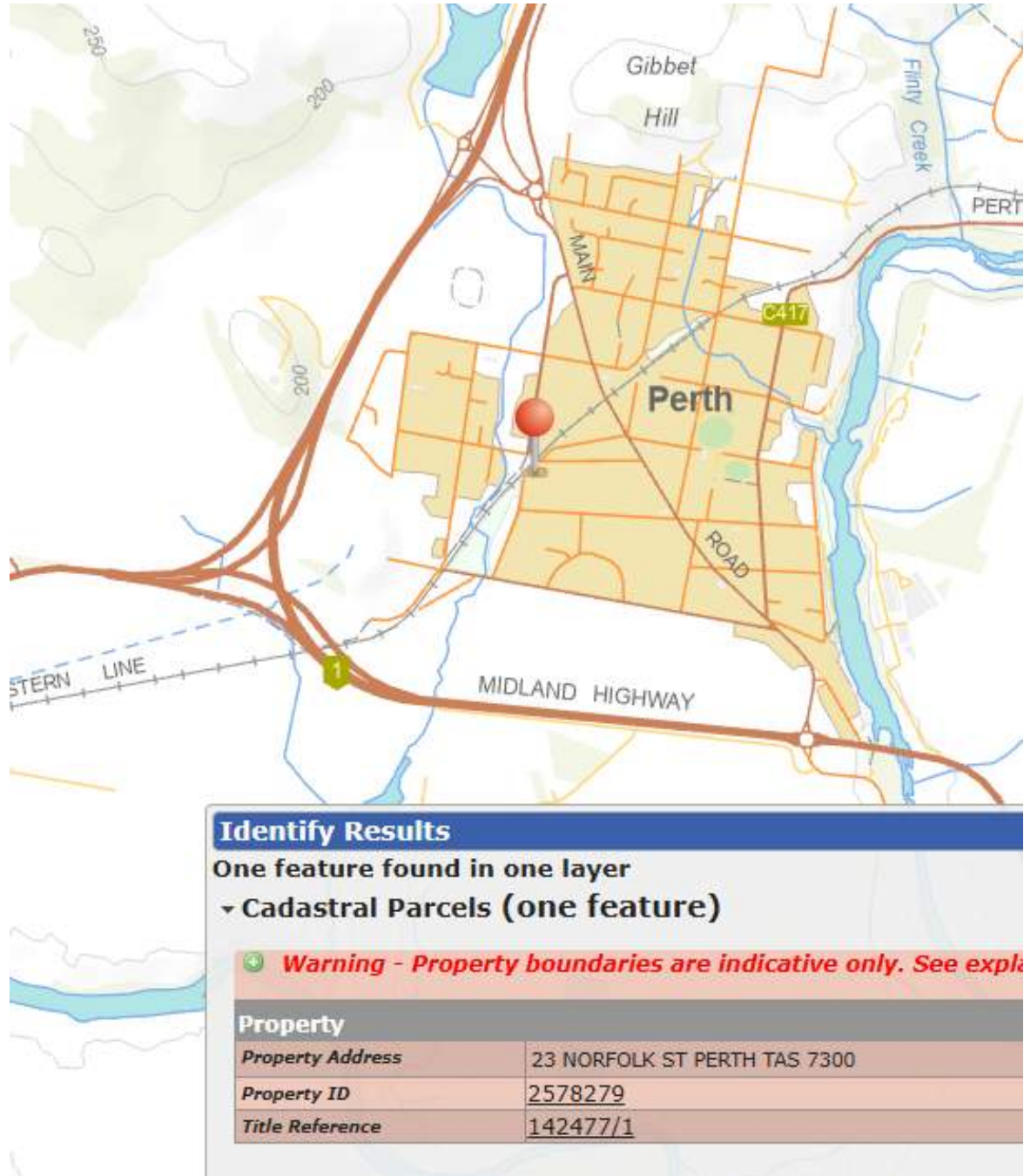
NMC	Northern Midlands Council
SSA	Safe System Assessment



## 2. Site Description

The 23 Norfolk Street development site is on the Northeast side of the road, see Figure 1&2. The topography is flat and within an urban residential setting.

Figure 1 - Location of proposed development



Source: LISTmap, DPIPWE



Figure 2 – Development site – 23 Norfolk Street, Perth



Source: LISTmap, DPIPWE

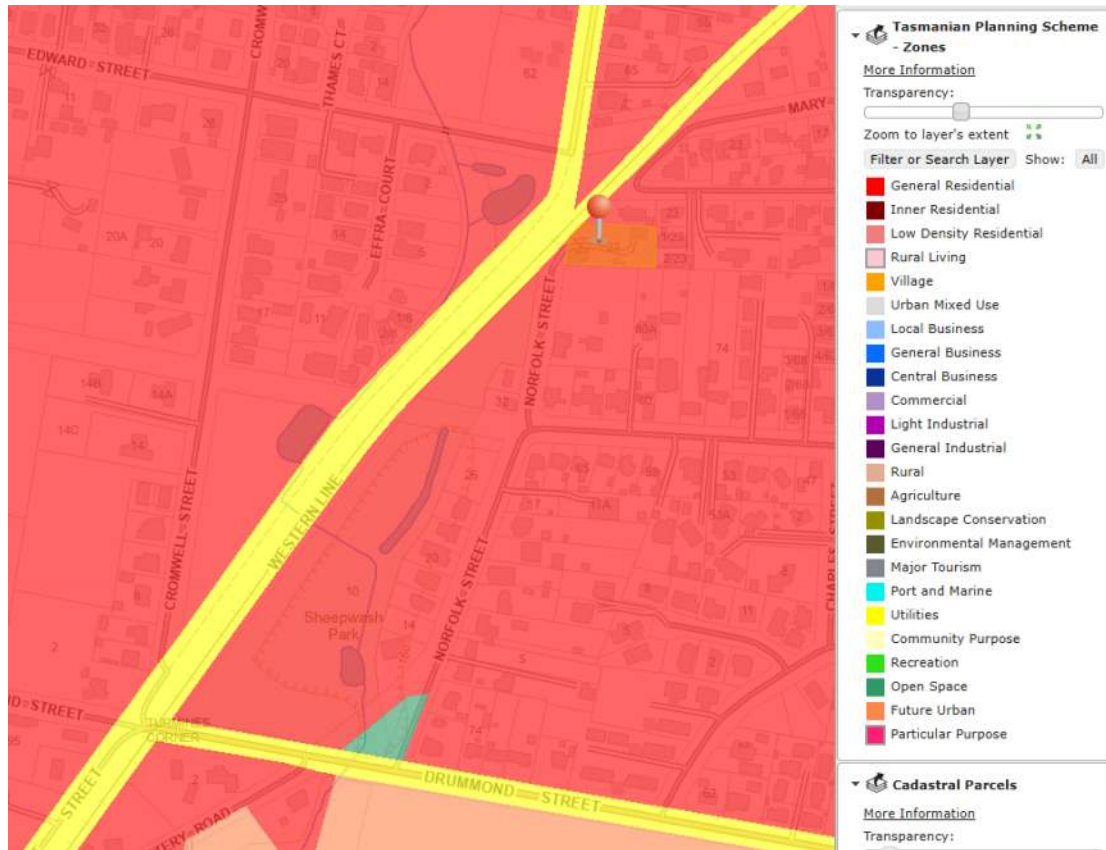




### 3.2 Council Planning Scheme

The proposed development involves land currently zoned in accordance with the Tasmanian Planning Scheme – Northern Midlands, see Figure 4.

**Figure 4 – Zoning for 23 Norfolk Street is General Residential**



Source: LISTmap, DPIPWE

### 3.3 Council Road Network Owner Objectives

Northern Midlands Council (NMC) is the manager of High Street. NMC objectives are to maintain traffic safety and transport efficiency.



## **4. Existing Conditions**

### **4.1 Transport Network**

The adjacent road network consists of Main Road, Frederick Street and Norfolk Street which are Council Roads.

#### **4.1.1 Main Road**

Main Road is a Collector Road in the Council Road Hierarchy with some 2,000 vpd (2026) through Perth, see Appendix B. The road has a 50km/h speed limit and is part of the Tasmanian 26m B Double Network, see Appendix C.

The road has a sealed width of 13m from face to face of kerb with on street parking and footpath both sides, is delineated with a Separation Line and has street lighting.

#### **4.1.2 Frederick Street**

Frederick Street functions as a residential street and connects Norfolk Street to Main Road.

The road is not part of the Tasmanian 26m B Double Network, see Appendix C.

The General Urban Speed Limit of 50km/h applies. The seal width is typically 12.5m from face to face of kerb. There is footpath along the Southern side of the road. AADT is estimated at 400vpd.

#### **4.1.3 Norfolk Street**

Norfolk Street links Drummond and Frederick Street and continues North of Frederick Street as a short No Through road some 130m in length.

The road has a sealed width of 5.4m and has no kerb and channel or footpath at the Northern end of the street. There is a streetlight midway of the section of the road North of Frederick Street.

The General Urban Speed Limit of 50km/h applies and estimated AADT is 40vpd as five residences access the street.

#### **4.1.4 # 23 Norfolk Street**

The existing access to 23 Norfolk Street is at the Northeast end of the street. Figures 5 – 9 show the nature of the access.



**Figure 5 – Aerial view of 23 Norfolk Street**



Source: LISTmap, DPIPWE

**Figure 6 – Elevation view of 23 Norfolk Street**



**Figure 7 – Looking left & right along Norfolk Street for the access to #23**



**Sight distance left  
& right is 130m.**



Figure 8 – Looking South along Norfolk Street and the Tasrail Western Line



Tasrail Western Line  
some 1.5m higher  
than Norfolk St  
surface level.

Figure 9 – Looking East along the Tasrail Western Line / 23 Norfolk St boundary



Tasrail Western Line  
some 1.5m higher  
than Norfolk St  
surface level.

## 4.2 Traffic Activity

Estimated traffic activity on the Council Roads is estimated as follows:

- Main Street – 2,000pd and 200vph at peak times, see Appendix B.
- Frederick Street – 200vpd and 20vph at peak times.
- Norfolk Street – 50vpd and 5vph at peak times.



### 4.3 Crash History

The Department of State Growth is supplied with reported crashes by Tasmania Police. The Department maintains a crash database from the crash reports which is used to monitor road safety, identify problem areas and develop improvement schemes. The 5-year reported crash history for Norfolk Street reveals no reported crashes as at 17<sup>th</sup> February 2026.

### 4.4 Road Safety Review

From site inspection of Norfolk St, an old farm fence exists across the Northern end of Norfolk Street, physically separating the road from the Tasrail Western Line, see Figure 10.

The fence helps define the end of the road and as such helps reduce the risk of vehicles driving onto the railway line.

An Obstruction Marker sign as depicted in Figure 10 would improve traffic safety during the day and at night.

**Figure 10 – Looking North on Norfolk Street approaching the Tasrail Western Line and the access to 23 Norfolk Street.**



**Obstruction marker sign D4-5(A)**



#### 4.5 Austroads Safe System Assessment

Norfolk Street (Frederick Street to end) has been assessed in accordance with the Austroads Safe System assessment framework. This framework involves consideration of exposure, likelihood and severity to yield a risk framework score. High risk crash types and vulnerable road user crash types are assessed for each site and aggregated to provide an overall crash risk. Crash risk is considered in terms of three components:

- Exposure (is low where low numbers of through and turning traffic) i.e. 1 out of 4
- Likelihood (is low where the infrastructure standard is high) i.e. 1 out of 4
- Severity (is low where the speed environment is low) i.e. 1 out of 4

The Austroads Safe System Assessment process enables the relative crash risk of an intersection or road link to be assessed. Vulnerable Road users are considered along with the most common crash types.

Crash risk score is an indication of how well infrastructure satisfies the *safe system objective for a forgiving road system where crashes do not result in death or serious injury*.

From safe system assessment, there is evidence that the Norfolk Street section North of Frederick Street has good alignment with the safe system objective with a crash risk score of 20/448, which indicates a very low crash risk, see Figures 11 and 12.

**Figure 11 – Austroads Safe System Assessment alignment between crash score and risk**







## 4.6 Sight Distance Review

Sight distance availability and requirements are summarised in Figure 13.

**Figure 13 – Sight Distance Summary**

Junction Major Rd - Minor Rd	Speed Limit (km/h)	Speed Environment (km/h)	Road frontage sight distance			
			Austroads SISD (m)	Available		AS 2890.1 SSD(m)
				Left(m)	Right(m)	
Frederick / Norfolk	50	50	97	200	200	NA
Existing access to 23 Norfolk	50	< 40	73	130	130	30

SISD compliant

## 4.7 Access Standard

There is no kerb and channel at the Northern end of Norfolk Street and the existing driveway has no culvert and is unsealed. The LGAT Urban Road property access standard shown in LGAT Standard Drawings TSD-R09 which are accessible online at.

[https://www.lgat.tas.gov.au/\\_data/assets/pdf\\_file/0027/813735/Tasmanian-Municipal-Standards-Drawings-v3-December-20202.pdf](https://www.lgat.tas.gov.au/_data/assets/pdf_file/0027/813735/Tasmanian-Municipal-Standards-Drawings-v3-December-20202.pdf)

To match with Norfolk Street and the LGAT property access standard, sealing of the driveway is required. A driveway culvert is not required at the ground surface drains to existing stormwater drains.



## 5. Traffic Generation and Assignment

This section of the report describes how traffic generated by the proposal is distributed within the adjacent road network now and in ten years (2036).

### 5.1 Traffic Growth

Compound annual traffic growth on Norfolk Street for projection purposes is assumed to be 0% as there is no potential for further infill development:

- AADT (2026) 30 vpd and 3 vph
- AADT (2036) 30 vpd and 3 vph

### 5.2 Trip Generation

For Medium Density Residential Dwellings, NSW Guide to Transport Impact Assessment 2024 – Section 5.6.2 Residential traffic generation guidelines are:

- 3.67 vehicle movements per day
- 0.41 vehicle movements during AM peak hour
- 0.60 vehicle movements during PM peak hour

For a 6 Lot multiple dwelling development as proposed estimated traffic generation is:

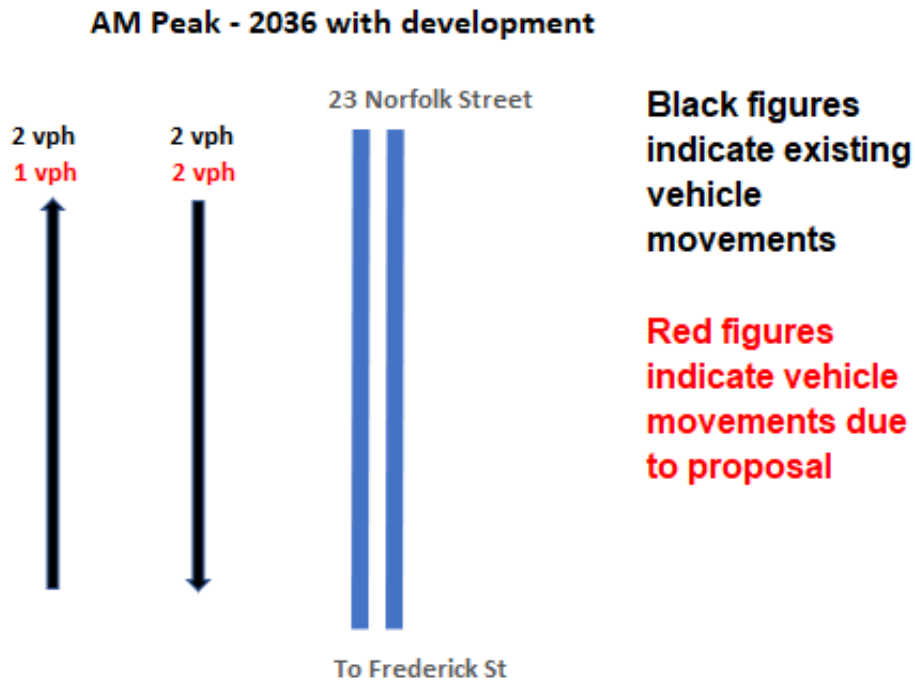
- 22 vpd
- 3 vph during AM peak hour
- 4 vph during PM peak hour

### 5.3 Trip Assignment

Figure 14 shows the traffic assignment for 2036 at the existing access to 23 Norfolk Street.



Figure 14 – Projected AM & PM traffic movements at 23 Norfolk St for 2036





## **6. Impact on Road Network**

### **6.1 Traffic impact on Norfolk Street**

2036 traffic flow on Norfolk Street is estimated at 50vpd without the proposal. The proposal will result in an additional 22 vpd to traffic flow on Norfolk Street which can easily be absorbed. These traffic activity levels are very low and, in the range, where there are no traffic capacity issues. Frederick Street and the junction with Norfolk Street is estimated to operate at LOS A by 2036. LOS descriptions are attached in Appendix D.

### **6.2 Proposed access and internal traffic management**

The proposed access and driveway provide suitably for 2-way traffic. Norfolk Street collection of waste bins is proposed, see design plans in Appendix A.

### **6.3 Other impacts**

#### **6.3.1 Environmental**

No environmental impacts were identified in relation to:

- Noise, Vibration and Visual Impact
- Community Severance and Pedestrian Amenity
- Hazardous Loads
- Air Pollution, Dust and Dirt and Ecological Impacts
- Heritage and Conservation values

#### **6.3.2 Street Lighting and Furniture**

The proposal does not require additional street lighting in Norfolk Street or justify further roadside furniture such as bus shelters, seats, direction signs, cycle racks, landscaping, street trees or fencing.

### **6.4 Tasmanian Subdivision Guideline Considerations**

No road and transport related issues with subdivision guidelines are identified.

### **6.5 Transport Planning Considerations**

The Environmental Noise and Ground Vibration report prepared by Tarkarri Engineering (February 2026) found no mitigations are necessary though close to the Tasrail Western Line.



## 6.6 Liveability, Safety and Amenity Guidelines

The basic requirements necessary for the safety and amenity of a residential area:

- Residential precincts need to be bounded by traffic routes and/or natural barriers to minimise conflict.
- Direct vehicular and pedestrian access should be avoided from single dwelling units onto road with over 2,000 vehicles per day.
- Effective street lengths should be less than 200-250m in order to achieve typical vehicle speeds of 40km/h.
- Cyclist and pedestrian demands should be catered for separately using path or cycle networks.

To maximise the liveability, safety and amenity of the local area, road and street network layout should be such that:

- A minimum of 60% of lots should abut residential streets with less than 300vpd passing traffic.
- A minimum of 80% of lots should abut residential streets with less than 600 vpd passing traffic.
- A maximum of 5% of single dwelling lots should abut residential streets with between 1,000-2,000 vpd passing traffic.
- A maximum of 1% of single dwelling lots should abut local streets or collectors with less than 3,000 vpd passing traffic, and
- No single dwelling lot should abut a route with more than 3,000 vpd passing traffic.

These guidelines are from *TE&M Chapter 2.2: Design of New Urban Networks*.

The proposal satisfies the liveability, safety and amenity targets described above.



## 7. Tas. Plan. Scheme – Northern Midlands

### 7.1.1 Parking and Sustainable Transport Code C2

#### C2.5.1 Car parking numbers

##### *Acceptable Solution A1*

The number of on-site car parking spaces must be no less than the number specified in Table C2.1, excluding if:

- (a) The site is subject to a parking plan for the area adopted by Council, in which case parking provision (spaces or cash in lieu) must be in accordance with that plan,
- (b) The site is contained within a parking precinct plan and subject to Clause C2.7,
- (c) The site is subject to Clause C2.5.5; or
- (d) It relates to an intensification of an existing use or development or a change of use where:
  - i. The number of onsite car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional onsite car parking is required; or
  - ii. The number of onsite car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows:

$$N=A+(C-B)$$

*N = Number of on-site car parking spaces required*

*A = Number of existing on-site car parking spaces*

*B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1.*

*C = Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1.*

Table C2.1 parking requirements for 2 or more-bedroom units are as follows:

- Resident parking – 2 spaces per unit, for 6 units 12 spaces are required.
- Visitor parking – 1 space per 3 units, for 6 units 2 spaces are required.

The proposal provides 2 spaces per unit and 2 visitor parking spaces, see Figure 3.

**A1 is satisfied.**



### **C2.5.2 Bicycle parking numbers**

No requirement.

### **C2.5.3 Motorcycle parking numbers**

#### ***Acceptable Solution A1***

*The number of on-site motorcycle parking spaces for all uses must:*

- (a) Be no less than the number specified in Table C2.4. and*
- (b) if an existing use or development is extended or intensified, the number of on-site motorcycle parking spaces must be based on the proposed extension or intensification, provided the existing number of motorcycle spaces is maintained.*

Table C2.5.3 has no requirement where the number of car parking spaces required is 0-20.

### **C2.5.4 Loading Bays**

#### ***Acceptable Solution A1***

*A loading bay must be provided for uses with a floor area of more than 1000m<sup>2</sup> in a single occupancy.*

Dwelling floor areas are less than 1000m<sup>2</sup>. **A1 is not applicable.**

### **C2.6.1 Construction of parking areas**

#### ***Acceptable Solution A1***

*All parking, access ways, manoeuvring and circulation spaces must:*

- (a) be constructed with a durable all-weather pavement,*
- (b) be drained to the public stormwater system, or contain stormwater on the site; and*
- (c) excluding all uses in the Rural Zone, Agricultural Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Public Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement.*

Concrete parking spaces and driveway is proposed and to be drained to the public stormwater system, see Appendix A, **A1 is satisfied.**



## C2.6.2 Design and layout of parking areas

### *Acceptable Solution A1.1*

*Parking, accessways, manoeuvring and circulation spaces must All parking, access ways, manoeuvring and circulation spaces must either:*

- (a) *comply with the following:*
- i. *have a gradient in accordance with Australian Standard AS 2890 Parking facilities, Parts 1-6. Driveway gradient is less than 5% and satisfied.*
  - ii. *Provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces. Satisfied as standard cars can park and manoeuvre within the site so that they can enter and exit in a forward direction.*
  - iii. *Have an access width not less than the requirements in Table C2.2.*  
 Proposal provides an access width of 4.5m which satisfies Table C2.2 where 6 to 20 parking spaces are proposed and 6m wide driveway internally satisfying Table C2.2
  - iv. *Have car parking space dimensions which satisfy the requirements in Table C2.3.*  
 90-degree parking spaces are proposed 3.0m wide and 5.4m long satisfying the 2.6m wide\* 5.4m long requirement of Table C2.3.
  - v. *Have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces.*  
 Minimum manoeuvre space required for 3m wide 90 degrees car parking spaces is 5.2m from Table C2.3. Proposal provides 6.6m of manoeuvre space.
  - vi. *Have a vertical clearance of not less than 2.1 metres above the parking surface level, Satisfied.*
  - vii. *Excluding a single dwelling, be delineated by line marking or other clear physical means. Satisfied.*

(b) *Comply with Australian Standard AS 2890 Parking facilities, Parts 1-6. Satisfied.*

**A1.1 is satisfied.**

### *Acceptable Solution A1.2*

*Parking spaces provided for use by persons with a disability must satisfy the following:*

- (a) *Be located as close as practical to the main entry point to the building. Satisfied.*
- (b) *be incorporated into the overall car park design. Satisfied.*
- (c) *be designed and constructed in accordance with Australian/ New Zealand Standard AS/NZS 2890.6-2009 Parking facilities - Off-street parking for people with disabilities.*

**Not Applicable.**



### **C2.6.3 Number of accesses for vehicles**

#### ***Acceptable Solution A1***

The number of accesses provided for each frontage must:

- (a) be no more than 1; or
- (b) no more than the existing number of accesses whichever is greater.

Existing two-way access is proposed. **A1 is satisfied.**

### **C2.6.5 Pedestrian access**

#### ***Acceptable Solution A1.1***

Applies to uses that require 10 or more car parking space must:

- (a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:
  - i. a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or
  - ii. protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and

(b) be signed and line marked at points where pedestrians cross access ways or parking aisles.

The proposal requires 14 car parking spaces and has no proposed footpath due to site constraints i.e lack of width.

**A1.1 is not satisfied.**

#### ***Performance Criteria P1***

Safe and convenient pedestrian access must be provided within parking areas, regarding:

- (a) the characteristics of the site.
- (b) the nature of the use
- (c) the number of parking spaces
- (d) the frequency of vehicle movements
- (e) the needs of persons with a disability
- (f) the location and number of footpath crossings
- (g) vehicle and pedestrian traffic safety
- (h) the location of any access ways or parking aisles
- (i) any protective devices proposed for pedestrian safety.

The site has insufficient width to provide footpath and 2.5m separation to the driveway.



The use is residential in a low-speed environment < 30km/h.

Adequate off-street parking is proposed.

Vehicle activity is low with peak vehicle movements at some 4 vph.

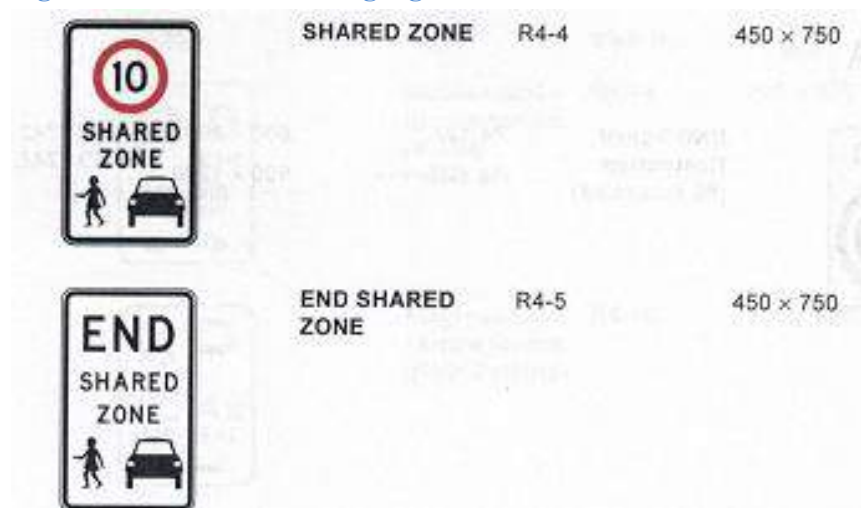
Sealed surfaces are provided in all areas conducive to safe pedestrian use.

From Austroads Safe System Assessment crash risk is considered very low:

- Crash exposure is low as traffic activity levels are low and pedestrian activity in the vicinity of vehicles is low i.e low crash exposure.
- Crash likelihood is low as Australian Standard off street parking is provided, sight lines are open and pedestrian activity and parking areas are reasonable separate and at low activity levels.
- Crash severity is low as the vehicle speed environment is low < 30km/h.

Formal signage of shared zones is a recognised pedestrian safety improvement where there is a mix of pedestrian, local access traffic only and situation where this is no kerb separation between pedestrians and vehicles. This is because Shared Zone signage includes provision of a regulator speed limit to keep speed to an appropriate level. In the case of the proposed driveway a 10 km/hr speed limit is considered normal. The proposed development is in keeping with this kind of situation. Figure 15 shows Shared Zone signage standards.

**Figure 15 – Shared Zone signage standards, AS1742.1-2014**



Accordingly, TCS recommends acceptance of the proposal with provision of 10km/hr Shared and End Shared Zone signage at the entry and exit to the development to limit speeds to a safe level. **P1 is satisfied.**

**Acceptable Solution A1.2**

*In parking areas containing accessible car parking spaces for uses by persons with a disability, a footpath having a width not less than 1.5m and a gradient not steeper than 1 in 14 is required from those spaces to the main entry point to the building.*

**A1.2 is not applicable.**

**C2.6.6 Loading bays****Acceptable Solution A1**

*The area and dimensions of loading bays and access way areas must be designed in accordance with Australian Standard AS 2890.2-2002, Parking facilities, Part 2: Off-street commercial vehicle facilities, for the type of vehicles likely to use the site.*

**A1 is not applicable.**

**7.1.2 Road and Railway Assets Code C3****C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction**

*Acceptable Solution A1.1 – For a category 1 road or a limited access road, vehicular traffic to and from the site will not require:*

- (a) A new junction*
- (b) A new vehicle crossing*
- (c) A new level crossing*

**Not applicable** as the roads are not Category 1.

*Acceptable Solution A1.2 – For a road, excluding a Category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority.*

**A1.2 is not satisfied** as no written consent has been issued by the road authority, see response to Performance Criteria P1.

*Acceptable Solution A1.3 – For the rail network, written consent for a new private level crossing to serve the use and development has been issued by the rail authority.*

**Not applicable** as no new rail network is involved.



**Acceptable solution A1.4:**

*Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing will not increase by more than:*

- (a) *The amounts in Table C3.1*
- (b) *Allowed by a licence issued under Part IVA of the Roads and Jetties Act 1935 in respect to a limited access road; and*

From Table C3.1 for vehicle crossings on *other roads*, the acceptable increase in AADT at the site is 20% or 40vpd whichever is greater. The proposal is estimated to generate 22vpd on Norfolk Street which is classed *other road*. **A1.4 is satisfied.**

**A1.5: Vehicular traffic must be able to enter and leave a major road in a forward direction.**

**A1.5 is satisfied.**

**C3.6.1 Habitable buildings for sensitive uses within a road or railway attenuation area**

**Acceptable Solution A1**

Unless within a building area on a sealed plan approved under this planning scheme, habitable buildings for a sensitive use within a road or railway attenuation area, must be:

- (a) within a row of existing habitable buildings for sensitive uses and no closer to the existing or future major road or rail network than the adjoining habitable building;
- (b) an extension which extends no closer to the existing or future major road or rail network than:
  - (i) the existing habitable building; or
  - (ii) an adjoining habitable building for a sensitive use; or
- (c) located or designed so that external noise levels are not more than the level in Table C3.2 measured in accordance with Part D of the *Noise Measurement Procedures Manual, 2nd edition, July 2008*.

**Table C3.2 Acceptable noise levels within a road or railway attenuation area**

<b>Roads</b>
The arithmetic average of the A-weighted L10 sound pressure levels for each of the one-hour periods between 6:00am and midnight on any day [L10 (18-hour)] of 63 dB(A).

Habitable buildings (sensitive uses) are proposed within 50m of the Tasrail Western Line (the rail attenuation area), see Figure 16. **A1 is not satisfied.**



Figure 16 – Shared Zone signage standards, AS1742.1-2014



### Performance Criteria P1

Habitable buildings for sensitive uses within a road or railway attenuation area, must be sited, designed or screened to minimise adverse effects of noise, vibration, light and air emissions from the existing or future major road or rail network, having regard to:

- (a) the topography of the site;
- (b) the proposed setback;
- (c) any buffers created by natural or other features;
- (d) the location of existing or proposed buildings on the site;
- (e) the frequency of use of the rail network;
- (f) the speed limit and traffic volume of the road;
- (g) any noise, vibration, light and air emissions from the rail network or road;
- (h) the nature of the road;
- (i) the nature of the development;
- (j) the need for the development;
- (k) any traffic impact assessment;
- (l) any mitigating measures proposed;
- (m) any recommendations from a suitably qualified person for mitigation of noise; and
- (n) any advice received from the rail or road authority.



All proposed dwellings are within 50m of the Tasrail Western line, see Figure 19.

- a. The topography of the site is flat however the Tasrail Western Line level is some 1.5m higher than Norfolk St surface level, see Figures 8 & 9.
- b. The development site is on the Southeast side of the Tasrail Western Line, all the proposed dwellings are within the 50m attenuation zone.
- c. There is no natural buffer between the rail line and development site.
- d. The building locations are shown in Figure 3 & 16.
- e. The Tasrail Western Line is a primary freight route.
- f. Norfolk Street AADT is estimated at 50 vpd (2026) and a 10km/h Shared zone is proposed within the development site. The General Urban Default Speed Limit of 50km/h applies on Norfolk Street.
- g. The rail noise level is within acceptable noise levels as reported in the Rail Environmental Noise and Ground Assessment report prepared by Tarkarri Engineering (9<sup>th</sup> February 2026).
- h. Norfolk Street is in reasonable condition with a 5.4m sealed width with lawn verges and a street lighting. The road is considered fit for purpose.
- i. The proposed development increases the number of dwellings within 50m of the Western Line railway boundary by 5 dwellings.
- j. The development is justified on commercial grounds.
- k. This TIA determines that subject to the recommendations contained in this report, the proposal will allow for continued safe and efficient operation of Norfolk Street.
- l. No noise mitigations are required, see Rail Environmental Noise and Ground Assessment report prepared by Tarkarri Engineering (9<sup>th</sup> February 2026).
- m. No noise mitigations are required, see Rail Environmental Noise and Ground Assessment report prepared by Tarkarri Engineering (9<sup>th</sup> February 2026).
- n. No advice has been received from Northern Midlands or Tasrail.

**P1 is satisfied.**



### **C3.7.1 Subdivision for sensitive uses within a road or railway attenuation area**

#### ***Acceptable Solution A1***

A lot, or a lot proposed in a plan of subdivision, intended for a sensitive use must have a building area for the sensitive use that is not within a road or railway attenuation area.

The proposal is within the General Residential Zone and within 50m of the Tasrail Western Rail Line (the road attenuation area), see Figure 16. **A1 is not satisfied.**

#### ***Performance Criteria P1***

A lot, or a lot proposed in a plan of subdivision, intended for sensitive uses within a road or railway attenuation area, must be sited, designed or screened to minimise the effects of noise, vibration, light and air emissions from the existing or future major road or rail network, having regard to:

- (a) the topography of the site;
- (b) any buffers created by natural or other features;
- (c) the location of existing or proposed buildings on the site;
- (d) the frequency of use of the rail network;
- (e) the speed limit and traffic volume of the road;
- (f) any noise, vibration, light and air emissions from the rail network or road;
- (g) the nature of the road;
- (h) the nature of the intended uses;
- (i) the layout of the subdivision;
- (j) the need for the subdivision;
- (k) any traffic impact assessment;
- (l) any mitigating measures proposed;
- (m) any recommendations from a suitably qualified person for mitigation of noise; and
- (n) any advice received from the rail or road authority.

- a. The topography of the site is flat however the Tasrail Western Line level is some 1.5m higher than Norfolk St surface level, see Figures 8 & 9.
- b. There is no natural buffer.



- c. Proposed buildings would be on the Southeast side of the Tasrail Western Line and within the attenuation area.
- d. There Tasrail Western Line is regularly used.
- e. Norfolk Street traffic volume is estimated at 50 vpd (2036) in an 50km/h zone.
- f. The rail noise level is within acceptable noise levels as reported in the Rail Environmental Noise and Ground Assessment report prepared by Tarkarri Engineering (9<sup>th</sup> February 2026).
- g. The road is flat and straight and some 1.5m lower in surface level to the Tasrail Western Line, see Figures 8 & 9.
- h. The proposed development is for residential dwellings consistent with the Tasmanian Planning Scheme Land Use Zoning – Northern Midlands.
- i. The dwelling layout suits the site contours.
- j. The proposed multiple dwelling development is justified on a commercial basis.
- k. This traffic impact assessment determines that subject to the recommendations contained in this report, the subdivision proposal will allow continued safe and efficient operation of Norfolk Street and is supported on traffic grounds.
- l. No noise mitigations are required, see Rail Environmental Noise and Ground Assessment report prepared by Tarkarri Engineering (9<sup>th</sup> February 2026).
- m. No noise mitigations are required, see Rail Environmental Noise and Ground Assessment report prepared by Tarkarri Engineering (9<sup>th</sup> February 2026).
- n. Northern Midlands or Tasrail.

**P1 is satisfied.**



## 8. Recommendations and Conclusions

This traffic impact assessment has been prepared to consider the proposed 5 residential units and house i.e 6 dwelling proposal at 23 Norfolk Street, Perth.

2036 traffic flow on Norfolk Street is estimated at 50vpd without the proposal.

The proposal will result in an additional 22 vpd on Norfolk Street which can easily be absorbed. These traffic activity levels are very low and, in the range, where there are no traffic capacity issues. Frederick Street and the junction with Norfolk Street is estimated to operate at LOS A by 2036 which is a very high LOS.

The assessment has reviewed the existing road conditions, crash history and road safety including an Austroads Safe System assessment.

No serious traffic safety issues were apparent in the vicinity of the proposal and the five -year reported crash history reports provides no evidence of a crash propensity in the vicinity of the proposal. Safe System Assessment of Norfolk Street indicates the existing situation near the access has a very low crash risk. No noise mitigations are required, see Rail Environmental Noise and Ground Assessment report prepared by Tarkarri Engineering (9<sup>th</sup> February 2026).

Evidence is provided that demonstrates the proposal satisfies the Road & Railway Assets Code C3 and Car Parking & Sustainable Transport Code C2 requirements of the Tasmanian Planning Scheme – Northern Midlands.

### **Recommendations:**

- *Ensure the concrete driveway meets the sealed edge of Norfolk Street consistent with LGAT Standard Drawing TSD-R09. The driveway will not require a culvert as there is drainage to the stormwater system.*
- *Developer install 10km/h Shared Zoned signage for traffic entering the driveway & End Shared Zone signage for traffic exiting the driveway onto Norfolk Street, see Figure 15.*

### **Suggestion for Council consideration:**

- *Council install Obstruction Marker D4-5(A) sign facing North bound traffic on Norfolk Street approaching the railway line beside the access to 23 Norfolk Street, see Figure 10.*

Overall, it has been concluded that the proposed development will not create any traffic issues and traffic will continue to operate safely and efficiently along Norfolk Street.

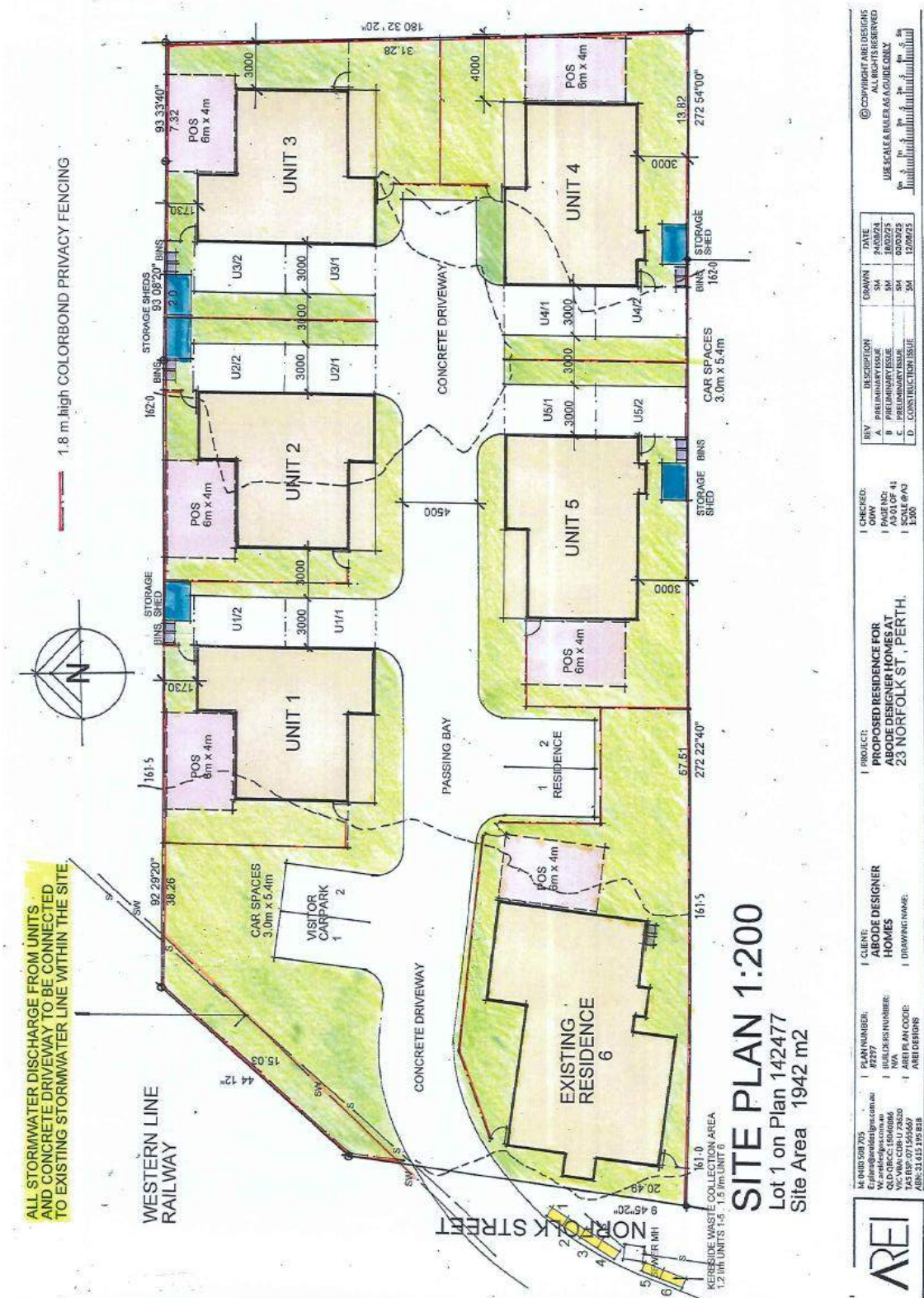
Based on the findings of this report and subject to the recommendation above, the proposed development is supported on traffic grounds.

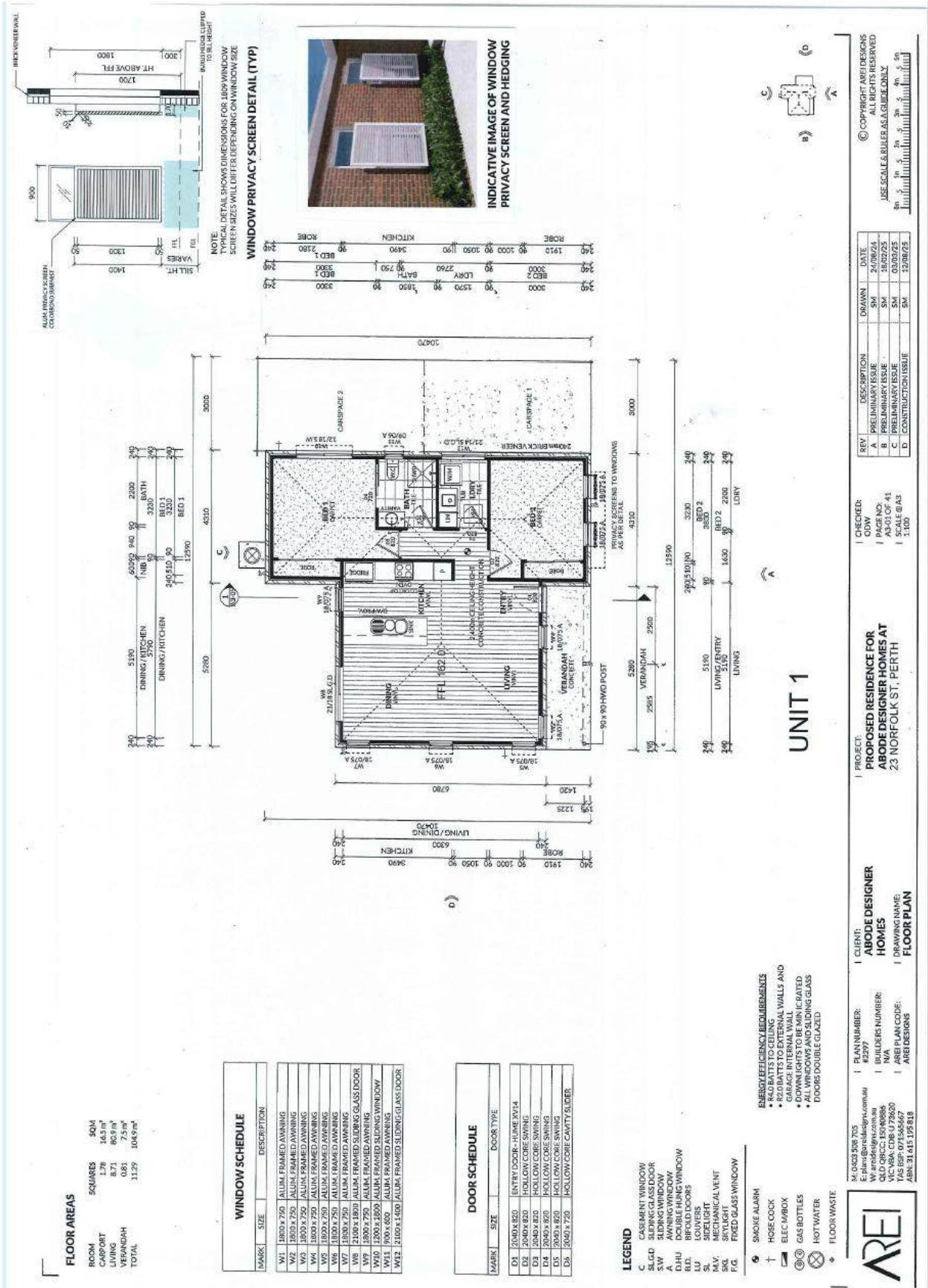


## **Appendices**



# Appendix A – Proposal Design Plans







**FLOOR AREAS**

ROOM	SQUARES	SQM
CARPORT	1.78	16.51 m <sup>2</sup>
LIVING	8.71	80.9 m <sup>2</sup>
VERANDAH	0.81	7.51 m <sup>2</sup>
TOTAL	11.29	104.9 m <sup>2</sup>

MARK	SIZE	DESCRIPTION
W1	1800 x 750	ALUM. FRAMED AWNING
W2	1800 x 750	ALUM. FRAMED AWNING
W3	1800 x 750	ALUM. FRAMED AWNING
W4	1800 x 750	ALUM. FRAMED AWNING
W5	1800 x 750	ALUM. FRAMED AWNING
W6	1800 x 750	ALUM. FRAMED AWNING
W7	2100 x 1800	ALUM. FRAMED SLIDING GLASS DOOR
W8	1800 x 750	ALUM. FRAMED AWNING
W9	1800 x 750	ALUM. FRAMED AWNING
W10	1200 x 3000	ALUM. FRAMED SLIDING WINDOW
W11	1000 x 600	ALUM. FRAMED AWNING
W12	1400 x 400	ALUM. FRAMED SLIDING GLASS DOOR

MARK	SIZE	DOOR TYPE
D1	2400 x 800	ENTRY DOOR - H.M.E. X114
D2	2400 x 800	HOLLOW CORE SWING
D3	2400 x 800	HOLLOW CORE SWING
D4	2400 x 800	HOLLOW CORE SWING
D5	2400 x 800	HOLLOW CORE SWING
D6	2400 x 720	HOLLOW CORE CAVITY SLIDER

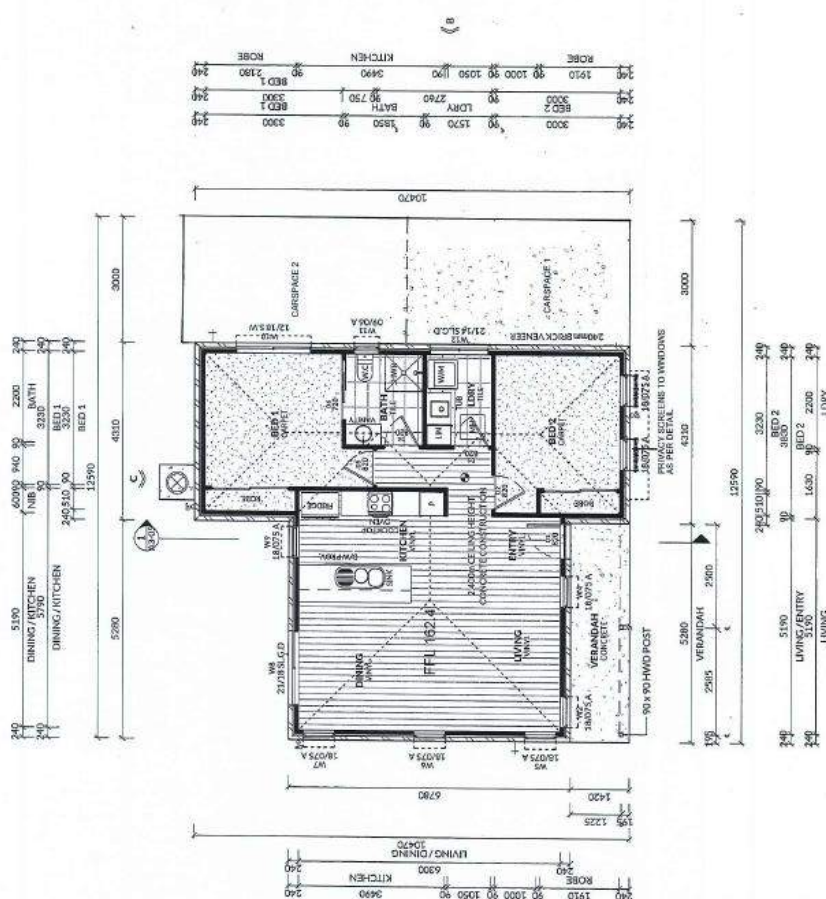
**LEGEND**

- C CEMENT WINDOW
- SLGD SLIDING GLASS DOOR
- SW SLIDING WINDOW
- AW AWNING WINDOW
- ALU ALUMINUM WINDOW
- BLD BRICK DOOR
- LU LOUVERS
- SL SIDELIGHT
- M.V. MECHANICAL VENT
- M.V. MECHANICAL VENT
- F.G. FIXED GLASS WINDOW

- ENERGY EFFICIENCY REQUIREMENTS
- R4 CABETS TO CEILING
- R2 CABETS TO EXTERNAL WALLS AND GARAGE INTERNAL WALL
- DOWNLIGHTS TO BE MINICATED
- DOUBLE GLAZING
- DOORS DOUBLE GLAZED



PLAN NUMBER: K14083-008-705  
 CLIENT: ABODE DESIGNER HOMES  
 BUILDERS NUMBER: N/A  
 AREI PLAN CODE: AREI DESIGNS  
 AREI: 31.615.195.818



**UNIT 2**

PROJECT: PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK ST, PERTH

CLIENT: ABODE DESIGNER HOMES  
 DRAWING NAME: FLOOR PLAN

PLAN NUMBER: K14083-008-705  
 CLIENT: ABODE DESIGNER HOMES  
 BUILDERS NUMBER: N/A  
 AREI PLAN CODE: AREI DESIGNS  
 AREI: 31.615.195.818



REV	DESCRIPTION	DRAWN	DATE
A	PRELIMINARY ISSUE	SM	24/06/24
B	PRELIMINARY ISSUE	SM	18/02/25
C	PRELIMINARY ISSUE	SM	03/03/25
D	CONSTRUCTION ISSUE	SM	22/08/25

CHECKED: [Signature]  
 PROJECT: PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK ST, PERTH

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**WINDOW SCHEDULE**

MARK	SIZE	DESCRIPTION
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W6	1800 x 750	ALUM. FRAMED AWNING
W7	2100 x 1800	ALUM. FRAMED SLIDING GLASS DOOR
W9	1800 x 750	ALUM. FRAMED AWNING
W10	1800 x 1800	ALUM. FRAMED AWNING
W11	1800 x 600	ALUM. FRAMED AWNING
W12	2100 x 1400	ALUM. FRAMED SLIDING GLASS DOOR

**DOOR SCHEDULE**

MARK	SIZE	DOOR TYPE
D1	2040 x 820	ENTRY DOOR - HLMC NVG4
D2	2040 x 820	HOLLOW CORE SWING
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D4	2040 x 820	HOLLOW CORE SWING
D5	2040 x 820	HOLLOW CORE SWING
D6	2040 x 820	HOLLOW CORE SWING
D7	2040 x 820	HOLLOW CORE SWING
D8	2040 x 820	HOLLOW CORE SWING
D9	2040 x 820	HOLLOW CORE SWING
D10	2040 x 820	HOLLOW CORE SWING
D11	2040 x 820	HOLLOW CORE SWING
D12	2040 x 820	HOLLOW CORE SWING
D13	2040 x 820	HOLLOW CORE SWING
D14	2040 x 820	HOLLOW CORE SWING
D15	2040 x 820	HOLLOW CORE SWING
D16	2040 x 820	HOLLOW CORE SWING
D17	2040 x 820	HOLLOW CORE SWING
D18	2040 x 820	HOLLOW CORE SWING
D19	2040 x 820	HOLLOW CORE SWING
D20	2040 x 820	HOLLOW CORE SWING
D21	2040 x 820	HOLLOW CORE SWING
D22	2040 x 820	HOLLOW CORE SWING
D23	2040 x 820	HOLLOW CORE SWING
D24	2040 x 820	HOLLOW CORE SWING
D25	2040 x 820	HOLLOW CORE SWING
D26	2040 x 820	HOLLOW CORE SWING
D27	2040 x 820	HOLLOW CORE SWING
D28	2040 x 820	HOLLOW CORE SWING
D29	2040 x 820	HOLLOW CORE SWING
D30	2040 x 820	HOLLOW CORE SWING
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D60	2040 x 820	HOLLOW CORE SWING
D61	2040 x 820	HOLLOW CORE SWING
D62	2040 x 820	HOLLOW CORE SWING
D63	2040 x 820	HOLLOW CORE SWING
D64	2040 x 820	HOLLOW CORE SWING
D65	2040 x 820	HOLLOW CORE SWING
D66	2040 x 820	HOLLOW CORE SWING
D67	2040 x 820	HOLLOW CORE SWING
D68	2040 x 820	HOLLOW CORE SWING
D69	2040 x 820	HOLLOW CORE SWING
D70	2040 x 820	HOLLOW CORE SWING
D71	2040 x 820	HOLLOW CORE SWING
D72	2040 x 820	HOLLOW CORE SWING
D73	2040 x 820	HOLLOW CORE SWING
D74	2040 x 820	HOLLOW CORE SWING
D75	2040 x 820	HOLLOW CORE SWING
D76	2040 x 820	HOLLOW CORE SWING
D77	2040 x 820	HOLLOW CORE SWING
D78	2040 x 820	HOLLOW CORE SWING
D79	2040 x 820	HOLLOW CORE SWING
D80	2040 x 820	HOLLOW CORE SWING
D81	2040 x 820	HOLLOW CORE SWING
D82	2040 x 820	HOLLOW CORE SWING
D83	2040 x 820	HOLLOW CORE SWING
D84	2040 x 820	HOLLOW CORE SWING
D85	2040 x 820	HOLLOW CORE SWING
D86	2040 x 820	HOLLOW CORE SWING
D87	2040 x 820	HOLLOW CORE SWING
D88	2040 x 820	HOLLOW CORE SWING
D89	2040 x 820	HOLLOW CORE SWING
D90	2040 x 820	HOLLOW CORE SWING
D91	2040 x 820	HOLLOW CORE SWING
D92	2040 x 820	HOLLOW CORE SWING
D93	2040 x 820	HOLLOW CORE SWING
D94	2040 x 820	HOLLOW CORE SWING
D95	2040 x 820	HOLLOW CORE SWING
D96	2040 x 820	HOLLOW CORE SWING
D97	2040 x 820	HOLLOW CORE SWING
D98	2040 x 820	HOLLOW CORE SWING
D99	2040 x 820	HOLLOW CORE SWING
D100	2040 x 820	HOLLOW CORE SWING

**LEGEND**

- C CASEMENT WINDOW
- SLD SLIDING GLASS DOOR
- AW AWNING WINDOW
- A AWNING WINDOW
- D1/D2 DOUBLE HUNG WINDOW
- BD BIFOLD DOORS
- LD LIFT UP & SLIDE DOOR
- LU LIFT UP & SLIDE DOOR
- M.V. MECHANICAL VENT
- SKL SKYLIGHT
- FIG. FIXED GLASS WINDOW
- SMOKE ALARM
- HOSE COCK
- ELEC W/BOX
- GAS BOTTLES
- HOT WATER
- FLOOR WASTE

- ENERGY EFFICIENCY REQUIREMENTS**
- R40 RATIOS TO CEILING
  - R40 RATIOS TO EXTERNAL WALLS AND ROOFS
  - DOWNLIGHTS TO BE MINICRATED
  - ALL WINDOWS AND SLIDING GLASS DOORS DOUBLE GLAZED



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TAS RFR-071561667  
ABN: 31 615 195 818

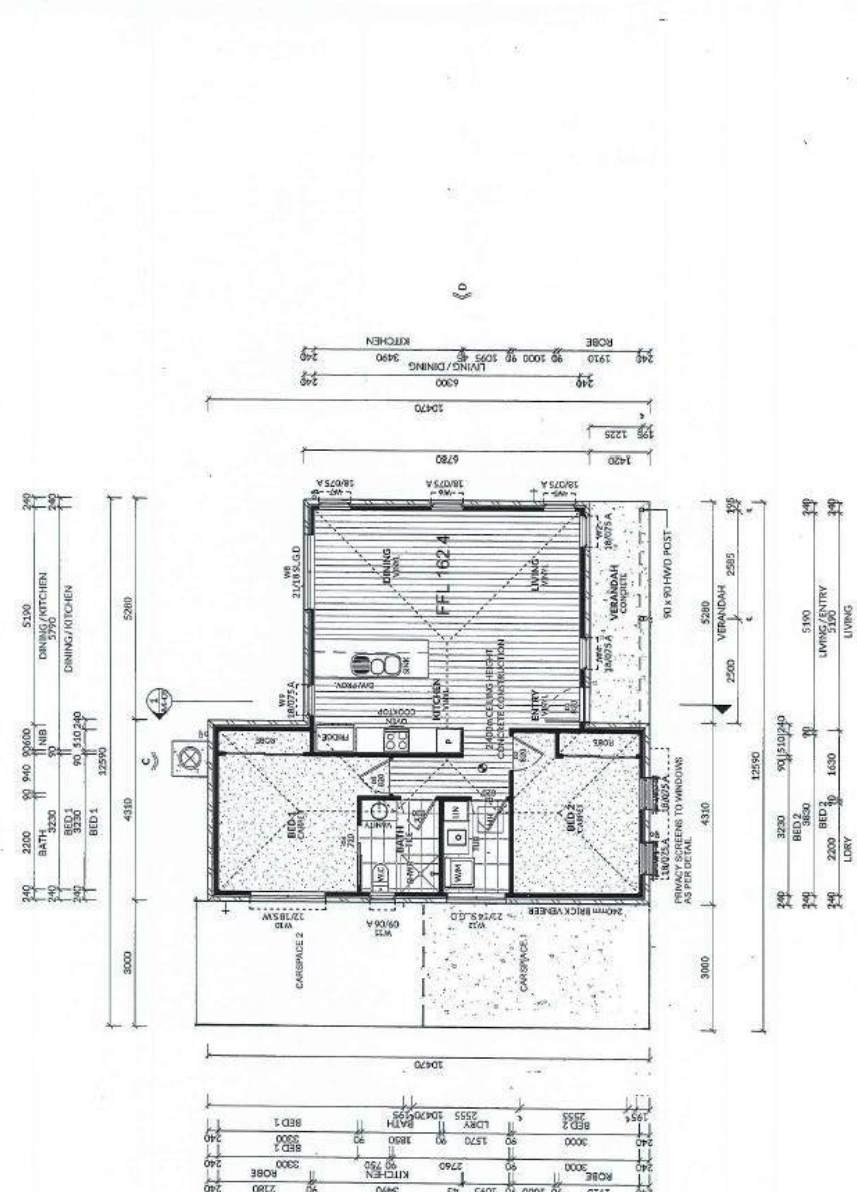
CLIENT:  
**ABODE DESIGNER HOMES**

DRAWING NAME:  
**FLOOR PLAN**

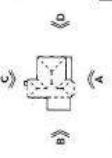
PROJECT:  
**PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK ST, PERTH**

REV	DESCRIPTION	DRAWN	DATE
A	PRELIMINARY ISSUE	SM	20/02/24
B	PRELIMINARY ISSUE	SM	03/02/25
C	PRELIMINARY ISSUE	SM	03/02/25
D	CONSTRUCTION ISSUE	SM	12/08/25

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SCALE: 1:100



**UNIT 3**





**FLOOR AREAS**

ROOM	SQMS	SQM
CORRIDOR	16.27	16.27
LIVING	8.23	85.21
VERANDA/DEK	0.81	7.51
<b>TOTAL</b>	<b>11.80</b>	<b>107.99</b>

**WINDOW SCHEDULE**

MARK	SIZE	DESCRIPTION
W1	1800 x 750	ALUM. FRAMED AWNING
W2	1800 x 750	ALUM. FRAMED AWNING
W3	1800 x 750	ALUM. FRAMED AWNING
W4	1800 x 750	ALUM. FRAMED AWNING
W5	1800 x 750	ALUM. FRAMED AWNING
W6	1800 x 750	ALUM. FRAMED AWNING
W7	1800 x 750	ALUM. FRAMED AWNING
W8	1800 x 750	ALUM. FRAMED AWNING
W9	1800 x 750	ALUM. FRAMED AWNING
W10	1800 x 750	ALUM. FRAMED AWNING
W11	1800 x 750	ALUM. FRAMED AWNING
W12	1800 x 750	ALUM. FRAMED AWNING
W13	1800 x 750	ALUM. FRAMED AWNING

**DOOR SCHEDULE**

MARK	SIZE	DOOR TYPE
D1	2040 x 820	ENTRY DOOR - HULME XV14
D2	2040 x 820	HOLLOW CORE SWING
D3	2040 x 720	HOLLOW CORE CAVITY SLIDER
D4	2040 x 820	HOLLOW CORE SWING
D5	2040 x 720	HOLLOW CORE SWING
D6	2040 x 720	HOLLOW CORE SWING

**LEGEND**

- C CASMENT WINDOW
- SLD SLIDING GLASS DOOR
- AW AWNING WINDOW
- A AWNING WINDOW
- D.HU DOUBLE HUNG WINDOW
- B.D. BICOLOR DOORS
- LU LOUVERS
- M.V. MECHANICAL VENT
- SKL SKYLIGHT
- F.G. FIXED GLASS WINDOW

- ENERGY EFFICIENCY REQUIREMENTS
- R4.0 BATS TO CEILING
- R2.0 BATS TO EXTERNAL WALLS AND ROOF
- DOWNLIGHTS TO BE MIN. CEATED
- ALL WINDOWS AND SLIDING GLASS DOORS DOUBLE GLAZED



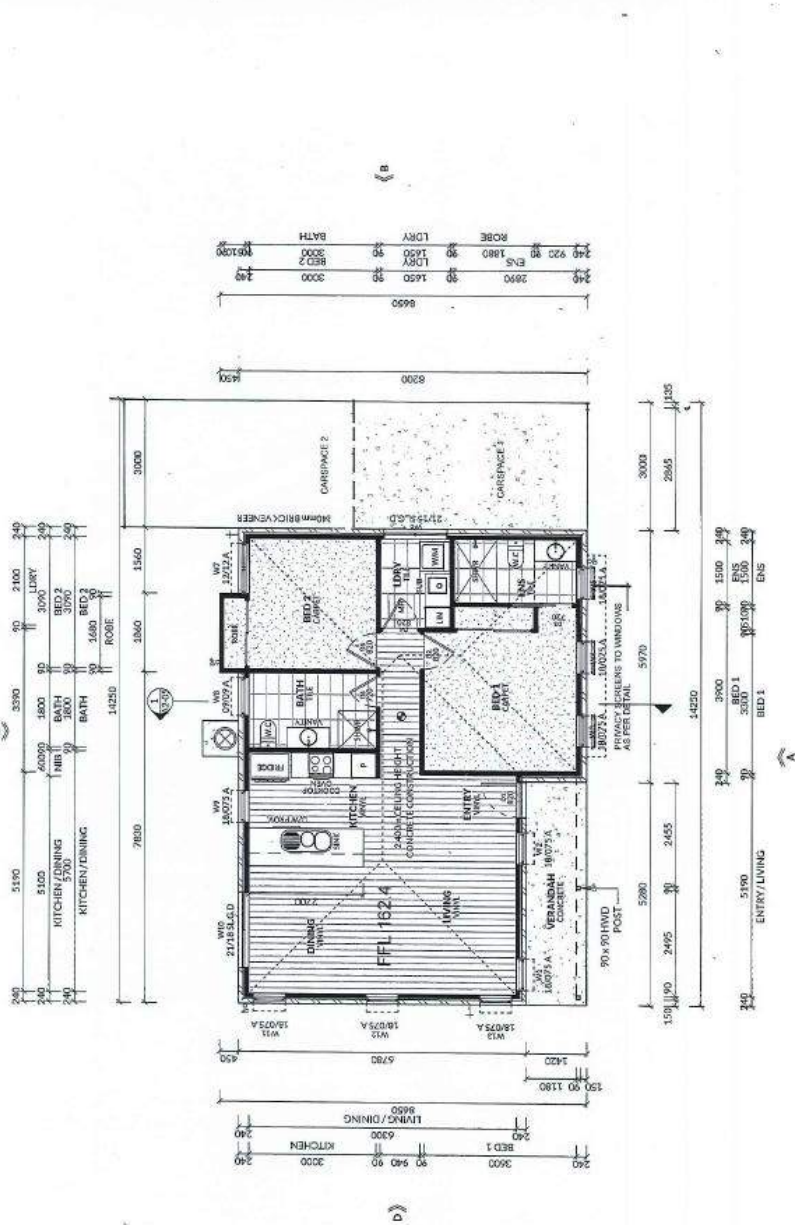
M 14003 508 705  
E: jhans@areidesigns.com.au  
W: areidesigns.com.au  
VIC 3048, CDLU 72620  
TAS ISP: 071 565667  
ABN: 51 645 195 818

PLAN NUMBER: #2297  
BUILDER NUM: N/A  
AREI PLAN COC: AREI DESIGNS

Summarize this document

Ask AI Assistant

By using AI Assistant, you agree to Generative AI User Guidelines.



**UNIT 4**

IN	DATE
1	24/08/24
2	19/02/25
3	10/06/25
4	15/06/25

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**FLOOR AREAS**

ROOM	SQUARES	SQM
CARPORT	1.78	16.5 m <sup>2</sup>
LIVING	9.21	85.6 m <sup>2</sup>
VEHICULAR	7.3	67.9 m <sup>2</sup>
TOTAL	11.50	109.9 m <sup>2</sup>

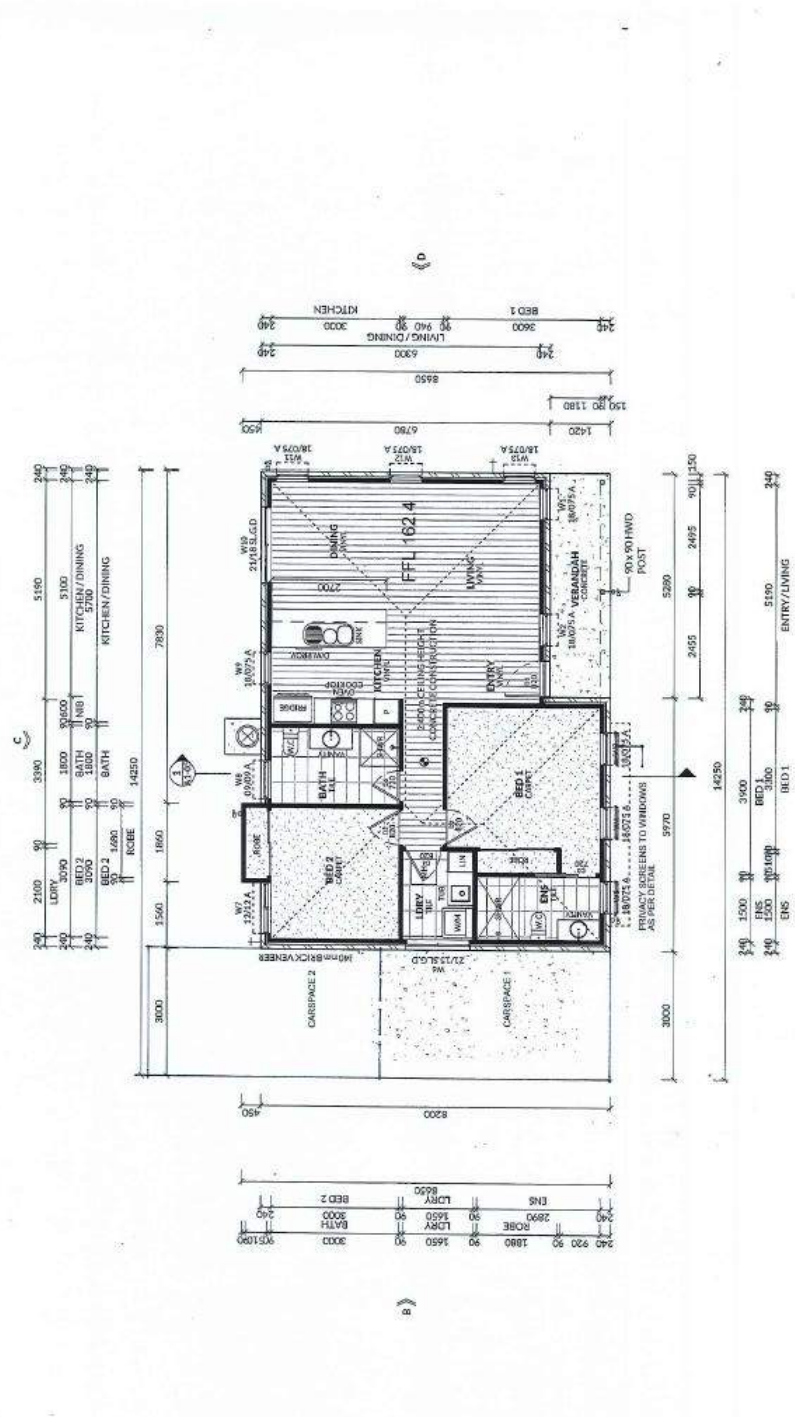
MARK	SIZE	DESCRIPTION
W1	1800x750	ALUMI FRAMED AWNING
W2	1800x750	ALUMI FRAMED AWNING
W3	1800x750	ALUMI FRAMED AWNING
W4	1800x750	ALUMI FRAMED AWNING
W5	1800x750	ALUMI FRAMED AWNING
W6	2100x1500	ALUMI FRAMED SLIDING GLASS DOOR
W7	1500x750	ALUMI FRAMED AWNING
W8	1500x750	ALUMI FRAMED AWNING
W9	1500x750	ALUMI FRAMED AWNING
W10	1500x750	ALUMI FRAMED AWNING
W11	1800x750	ALUMI FRAMED AWNING
W12	1800x750	ALUMI FRAMED AWNING
W13	1800x750	ALUMI FRAMED AWNING

MARK	SIZE	DOOR TYPE
D1	2040x820	ENTRY DOOR - HALLWAY
D2	2040x820	HOLLOW CORE SWING
D3	2040x770	HOLLOW CORE CAVITY SLIDER
D4	2040x820	HOLLOW CORE SWING
D5	2040x820	HOLLOW CORE SWING
D6	2040x720	HOLLOW CORE SWING

**LEGEND**

- C CASEMENT WINDOW
- SL GLD SLIDING GLASS DOOR
- SW SLIDING WINDOW
- A AWNING WINDOW
- W WINDOW
- R.D. REINFORCED CONCRETE DOOR
- LU LOUVERS
- SL SIDELIGHT
- M.V. MECHANICAL VENT
- M.V. MECHANICAL VENT
- F.G. FIXED GLASS WINDOW
- SMOKE ALARM
- HORE COOK
- ELEC/BOX
- GAS BOTTLES
- HOT WATER
- FLOOR WASTE

- ENERGY EFFICIENCY REQUIREMENTS**
- \* R20 BATTS TO EXTERNAL WALLS AND GARAGE INTERNAL WALL
  - \* DOWNLIGHTS TO BE MAIN GRADED
  - \* ALL WINDOWS AND SLIDING GLASS DOORS DOUBLE GLAZED



**UNIT 5**

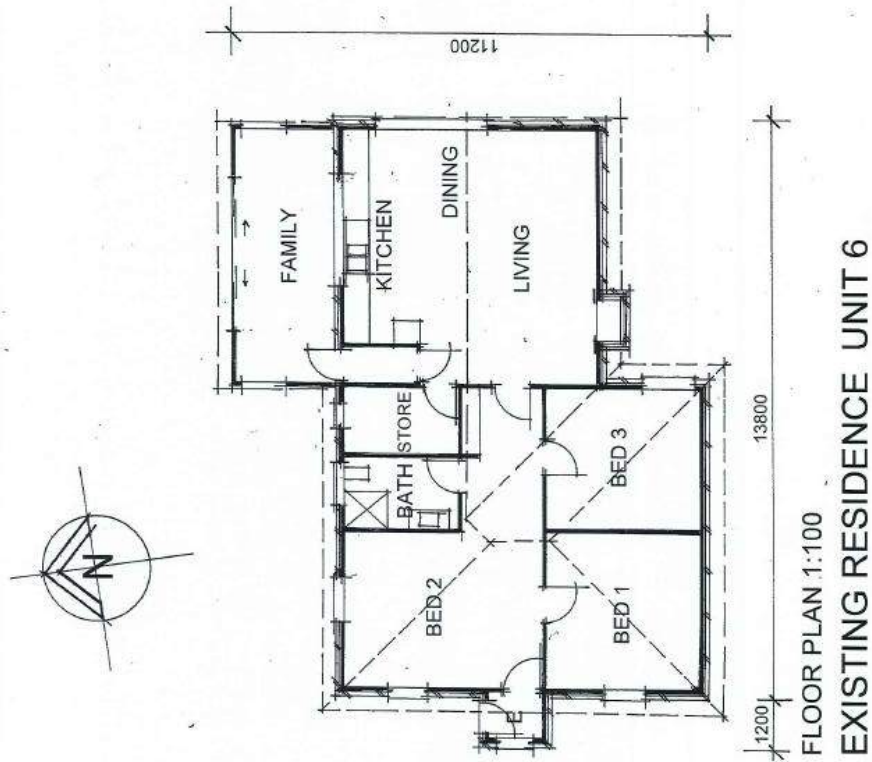
REV	DESCRIPTION	DATE
A	PRELIMINARY ISSUE	24/02/24
B	PRELIMINARY ISSUE	11/02/25
C	CONSTRUCTION ISSUE	11/02/25
D	CONSTRUCTION ISSUE	15/02/25

CHECKED:	DDW
PROJECT:	PROPOSED RESIDENCE FOR
PAGE NO:	ABODE DESIGNER HOMES AT
ALLOT OF 41	23 NORFOLK ST, PERTH
DATE:	11/02/25
SCALE:	1:100

CLIENT:	ABODE DESIGNER HOMES
PLAN NUMBER:	#2297
BUILDERS NUMBER:	TAS: CC 7461
INSULATION CODE:	VIC/VBA: COB-U 78620
AREI PLAN CODE:	TAS 059-07355667
AREI DESIGNS:	AREI 31 613 175 818

MARK	SIZE	DESCRIPTION
D1	2040x820	ENTRY DOOR - HALLWAY
D2	2040x820	HOLLOW CORE SWING
D3	2040x770	HOLLOW CORE CAVITY SLIDER
D4	2040x820	HOLLOW CORE SWING
D5	2040x820	HOLLOW CORE SWING
D6	2040x720	HOLLOW CORE SWING





FLOOR PLAN 1:100  
EXISTING RESIDENCE UNIT 6

PLAN NUMBER: #2297 MILLERS NUMBER: VIC VBA-CDB-U73620 AREI PLAN CODE: AREI-015-195318		CLIENT: ABODE DESIGNER HOMES		PROJECT: PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK ST, PERTH		CHECKER: DDW SCALE: 1:100		REV. DESCRIPTION DRAWN DATE A. PRELIMINARY ISSUE SM 24/08/24 B. PRELIMINARY ISSUE SM 10/02/25 C. CONSTRUCTION ISSUE SM 13/08/25 D. CONSTRUCTION ISSUE SM 15/08/25	
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## Appendix B – Main Rd Perth Traffic Data

**A0087590** - Midland Highway 80m N Of Illawarra MR

City: Perth

Route number: A0087

### Site Data

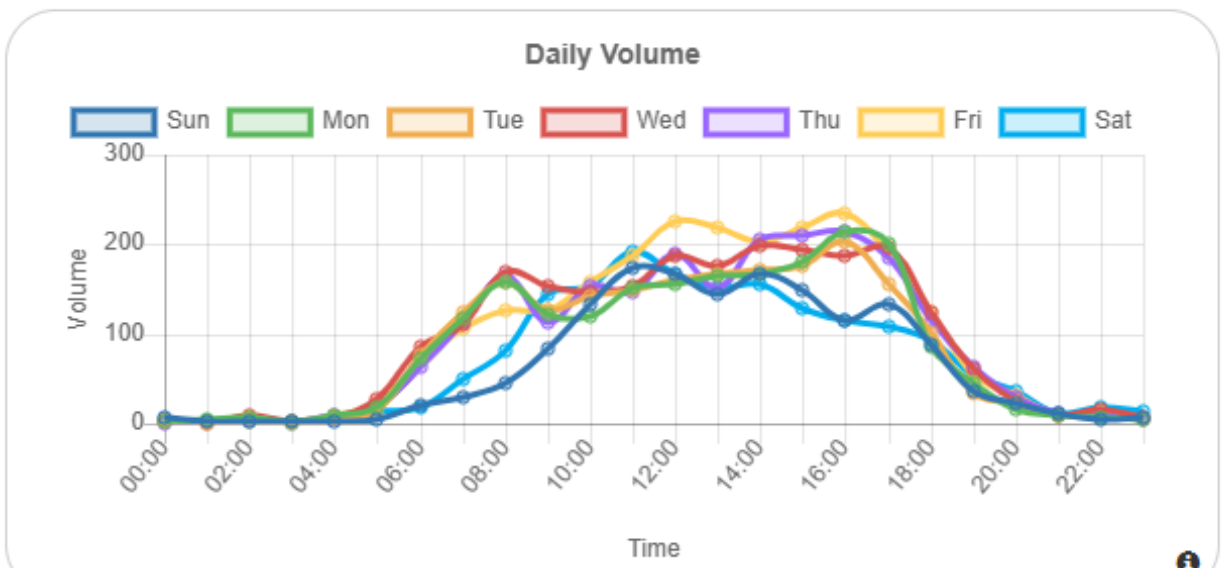
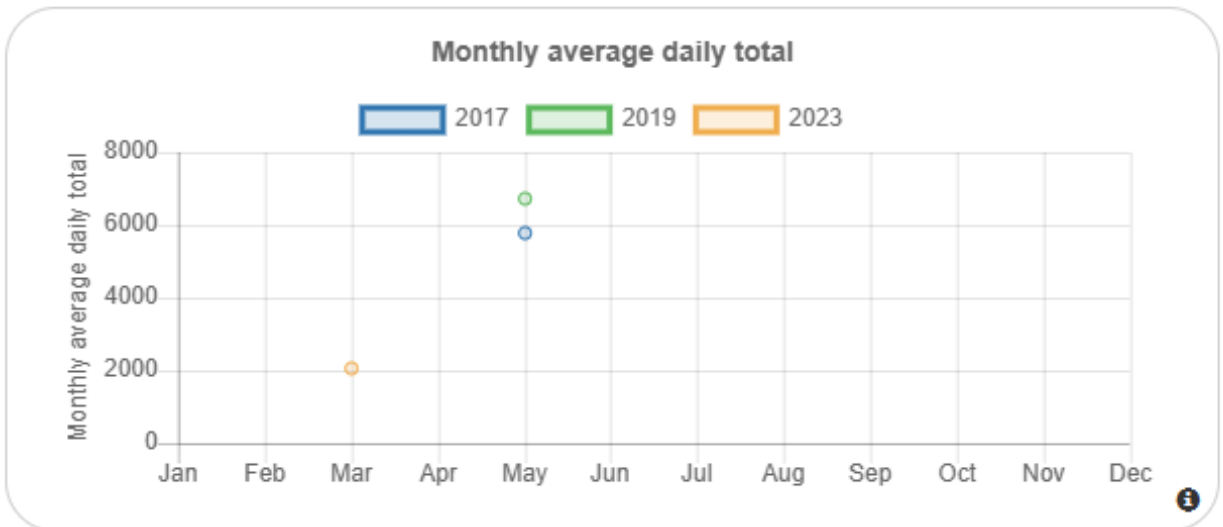
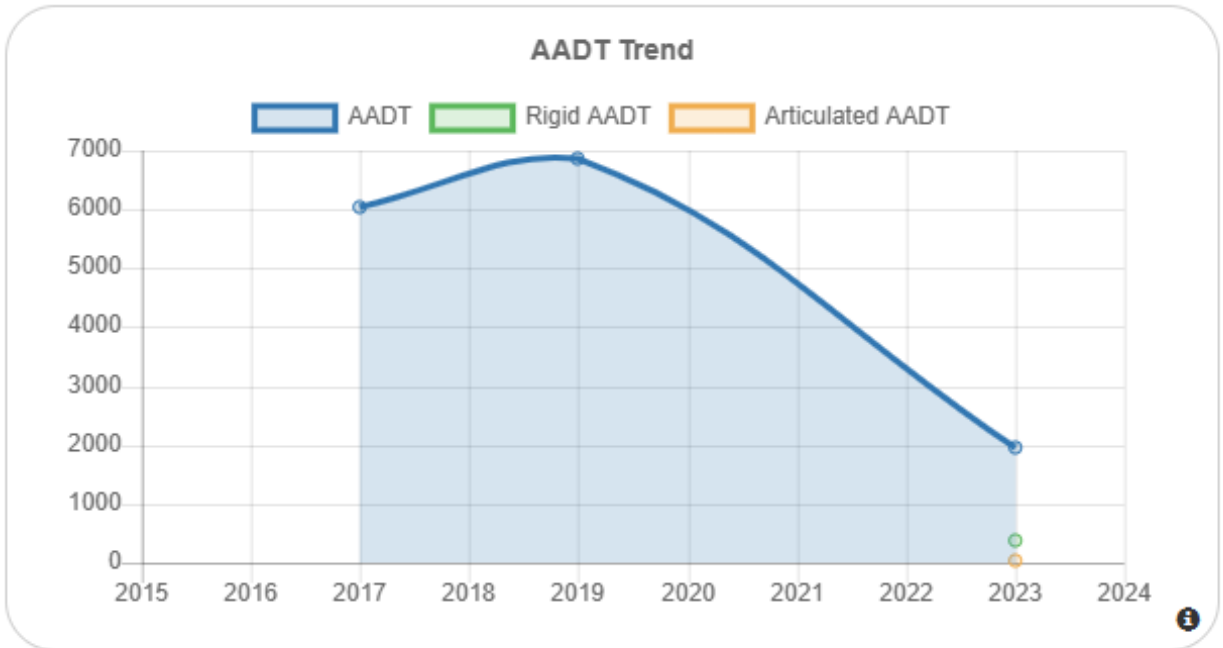


### Traffic Statistics by Direction

Direction	Weekday average total traffic	7-day average traffic	Weekly traffic total
North	985	932	6,512
South	1,181	1,081	7,554
Total	2,165	2,013	14,066

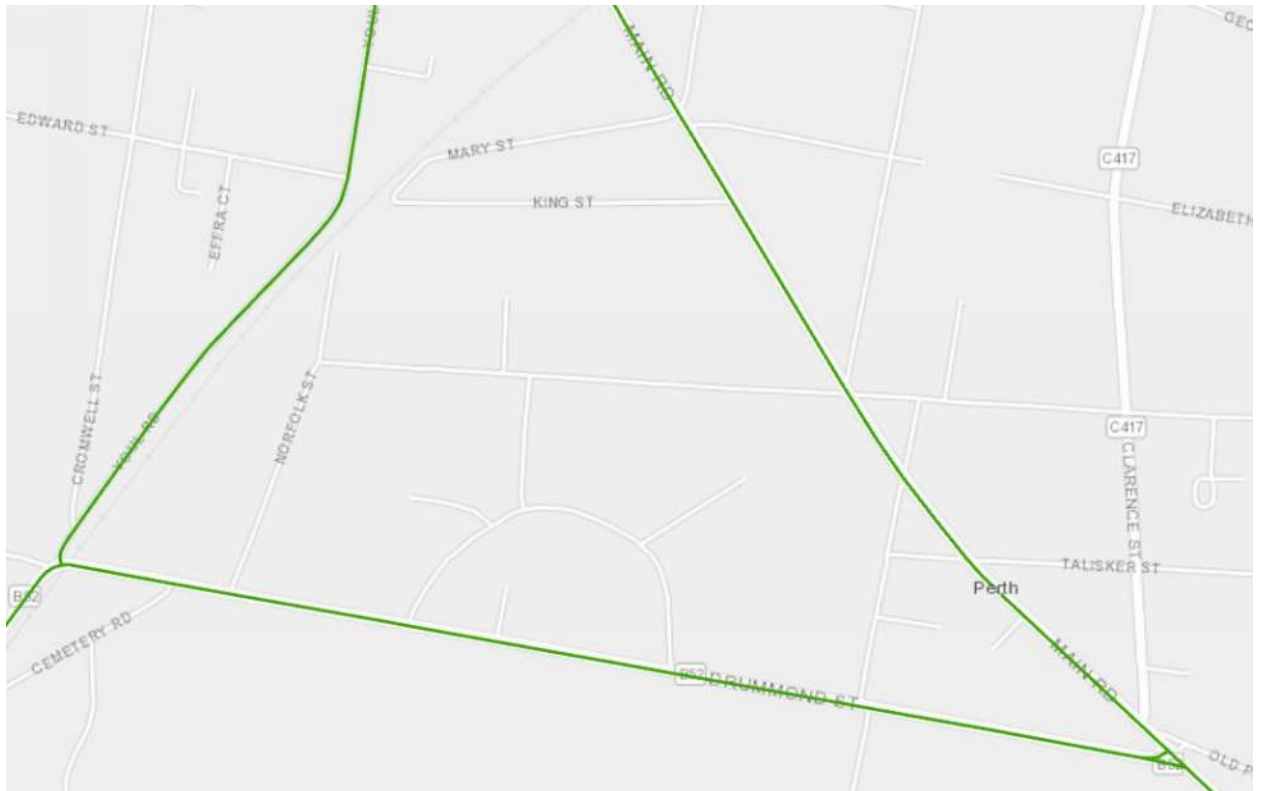
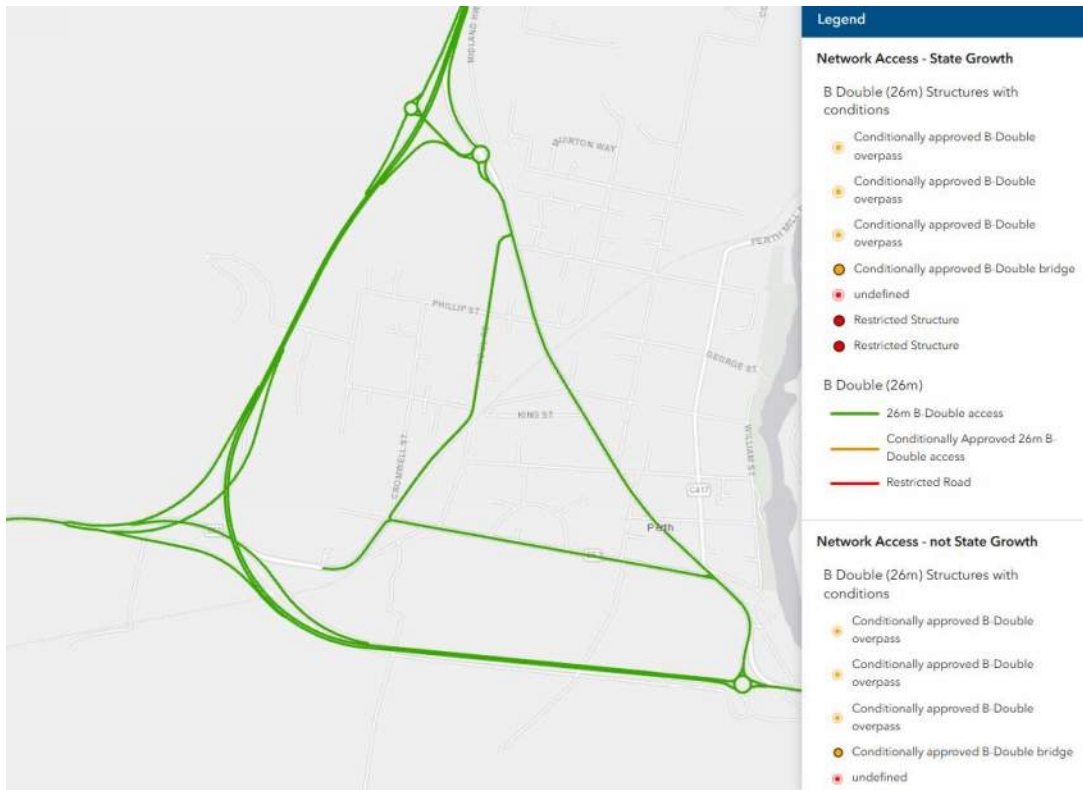
### Annual Statistics

Data Item	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024
AADT	-	-	6,033	-	6,869	-	-	-	1,950	-
% HV	-	-	16.4%	-	15.8%	-	-	-	20.9%	-





# Appendix C – Tas 26m B Double Network





## Appendix D – Level of Service Descriptions

<b>Level of service A</b>	A condition of free-flow in which individual drivers are virtually unaffected by the presence of others in the traffic stream. Freedom to select desired speeds and to manoeuvre within the traffic stream is extremely high, and the general level of comfort and convenience provided is excellent.
<b>Level of service B</b>	In the zone of stable flow where drivers still have reasonable freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience is a little less than with level of service A.
<b>Level of service C</b>	Also in the zone of stable flow, but most drivers are restricted to some extent in their freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience declines noticeably at this level.
<b>Level of service D</b>	Close to the limit of stable flow and approaching unstable flow. All drivers are severely restricted in their freedom to select their desired speed and to manoeuvre within the traffic stream. The general level of comfort and convenience is poor, and small increases in traffic flow will generally cause operational problems.
<b>Level of service E</b>	Traffic volumes are at or close to capacity, and there is virtually no freedom to select desired speeds or to manoeuvre within the traffic stream. Flow is unstable and minor disturbances within the traffic stream will cause breakdown.
<b>Level of service F</b>	In the zone of forced flow, where the amount of traffic approaching the point under consideration exceeds that which can pass it. Flow breakdown occurs, and queuing and delays result.

6.5.2026

# PRELIMINARY ISSUE

## REVISION C

PROPOSED RESIDENCE FOR

### ABODE DESIGNER HOMES

AT

23 NORFOLK STREET  
PERTH TAS 7300



UNIT 1 & 2



UNIT 3



UNIT 4



UNIT 5

PAGE LIST	
PAGE NO	PAGE TITLE
01	COVER
02	SITE PLAN
03	DRIVEWAY PLAN
04	LANDSCAPE PLAN
05	SITE OVERSHADOWING PLAN
A1-01	UNIT 1 & 2 FLOOR PLAN
A1-02	UNIT 1 & 2 3D VIEWS
A1-03	UNIT 1 & 2 ELEVATIONS A & B
A1-04	UNIT 1 & 2 ELEVATIONS C & D
A2-01	UNIT 3 FLOOR PLAN
A2-02	UNIT 3 3D VIEWS

PAGE LIST	
PAGE NO	PAGE TITLE
A2-03	UNIT 3 ELEVATIONS A & B
A2-04	UNIT 3 ELEVATIONS C & D
A3-01	UNIT 5 FLOOR PLAN
A3-02	UNIT 5 3D VIEWS
A3-03	UNIT 5 ELEVATIONS A & B
A3-04	UNIT 5 ELEVATIONS C & D
A3-09	LIGHTING CALCULATOR
A4-01	UNIT 4 FLOOR PLAN
A4-02	UNIT 4 3D VIEWS
A4-03	UNIT 4 ELEVATIONS A & B
A4-04	UNIT 4 ELEVATIONS C & D

#### GENERAL NOTES:

- ALL DESIGN, CONSTRUCTION METHODS AND MATERIALS TO BE IN ACCORDANCE WITH:
  - THE CURRENT NATIONAL CONSTRUCTION CODES (NCC)
  - THE STATE DEVELOPMENT CODE
  - BUILDING REGULATIONS
  - CURRENT ISSUES OF AUSTRALIAN STANDARDS & MANUFACTURERS SPECIFICATIONS & INSTALLATION DETAILS FOR MATERIALS USED
- THESE PLANS ARE TO BE READ IN CONJUNCTION WITH CONTRACT DOCUMENTS AND ENGINEERS DRAWINGS AND SPECIFICATIONS. RESPONSIBLE PARTIES ARE TO BE NOTIFIED OF ANY DISCREPANCIES.
- SUBSTITUTION OF ANY STRUCTURAL MEMBERS & OR VARIATIONS TO ANY PART OF THE DESIGN WILL VOID ANY RESPONSIBILITIES OF THE BUILDING DESIGNER FOR THE STRUCTURAL INTEGRITY & PERFORMANCE OF THE BUILDING.
- 3D VIEWS, PERSPECTIVES AND ILLUSTRATIONS ARE INTENDED TO BE A VISUAL AID ONLY, THEY ARE NOT PRESCRIPTIVE BUT INDICATIVE ONLY. THE IMAGES ARE NOT TO BE RELIED UPON IN ANY WAY FOR FINAL CONSTRUCTION FINISHES AND RESULTS.
- ALL DIMENSIONS IN MILLIMETERS.
- DIMENSIONS TAKE PREFERENCE TO SCALE AND ARE TO STRUCTURE NOT FINISH ON NEW WORK. EXISTING WALLS MAY BE NOMINALLY DIMENSIONED.
- ALL DIMENSIONS, DETAILS, SITE LEVELS AND FINISHED FLOOR LEVELS TO BE CONFIRMED BY CONTRACTOR BEFORE COMMENCEMENT OF ANY CONSTRUCTION AND RESPONSIBLE PEOPLE NOTIFIED OF ANY DISCREPANCIES.
- MANUFACTURER'S SPECIFICATION MEANS A CURRENT APPROVED SPECIFICATION FOR USE UNDER THE CONDITIONS APPLICABLE THESE DRAWINGS ARE AVAILABLE DIGITALLY, IF REQUIRED.
- ANY DATA SUPPLIED BY OTHERS AND SHOWN ON THESE DRAWINGS ARE NOT THE RESPONSIBILITY OF THIS DESIGNER. ALL USERS OF THESE DRAWINGS ARE ADVISED TO CHECK OTHER SUPPLIED DATA.
- OWNER REMAINS RESPONSIBLE FOR ONGOING MAINTENANCE OF BUILDING. STRUCTURAL ELEMENTS IN PARTICULAR ARE TO REMAIN PROTECTED BY THE METHODS SHOWN AND LISTED IN THESE DRAWINGS.
- ALL WINDOW AND DOOR DIMENSIONS ARE NOMINAL.

#### SITE WORKS NOTES:

- POSITION OF DWELLING TO BE CONFIRMED BY SURVEYOR & CLIENT PRIOR TO ANY SITE WORKS.
- ALL STORMWATER, DOWN PIPES, RAIN WATER TANKS & SITE DRAINAGE TO BE SIZED & LOCATED BY THE HYDRAULIC CONSULTANT/ PLUMBER IN ACCORDANCE WITH THE CURRENT **ABCB HOUSING PROVISIONS PART 3.3** AND STATE LEGISLATION/ LOCAL PLANNING SCHEME HOUSE CODE AND AS 3500 ALL PARTS.
- BUILDER TO ENSURE THAT ACTUAL SEWER LINE AND MANHOLE POSITIONS MATCH THOSE AS SHOWN AS BASED ON LOCAL AUTHORITY DOCUMENTS. ANY DISCREPANCIES MUST BE BROUGHT TO ATTENTION AND RESOLVED PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- BUILDER TO DETERMINE APPROPRIATE PLATFORMING METHOD ON SITE PRIOR TO COMMENCEMENT OF ANY WORKS. FINISHED FLOOR LEVEL IS TO BE ABOVE THE MINIMUM LEVEL AS PER LOCAL AUTHORITIES REQUIREMENTS & TO COMPLY WITH THE CURRENT **ABCB HOUSING PROVISIONS PART 3.3.3**
- FALL OF LAND UNKNOWN AND IS TO BE CONFIRMED ON SITE BEFORE COMMENCEMENT OF CONSTRUCTION. ANY REQUIRED EARTHWORKS INCLUDING CUT, FILL, BATTERS AND RETAINING MUST COMPLY WITH THE CURRENT **ABCB HOUSING PROVISIONS PART 4.2.2** AS 3798, AS4200 & **AS 4678**
- THE FINISHED SURFACE IMMEDIATELY SURROUNDING THE DWELLING, 1000mm WIDE, IS TO FALL AWAY FROM THE DWELLING AT A SLOPE OF 1 IN 20 MINIMUM.
- STORMWATER MUST BE CONNECTED TO A LEGAL POINT OF DISCHARGE -
  - STORMWATER KERB ADAPTERS TO STREET (2 MAX.)
  - ROOFWATER/STORMWATER PIPE
  - BUBBLERS TO COUNCIL SPECIFICATION.
  - RAINWATER TANK, OVERFLOW MUST CONNECT TO STORMWATER SYSTEM.
- SURFACE DRAINAGE IS TO DISCHARGE EVENLY WITHIN THE SITE AND WITHOUT NUISANCE TO ADJOINING PROPERTIES.
- ALL SUB-FLOOR AREAS MUST BE GRADED TO AVOID THE PONDING OF WATER.
- THE HEIGHT OF FENCES, INCLUDING THE HEIGHT OF RETAINING WALLS ARE NOT TO EXCEED 2.0m ABOVE FINISHED GROUND LEVEL **UNLESS** INDICATED ON THE PLANS AND TO LOCAL AUTHORITY APPROVAL.
- WHERE SERVICES / PIPEWORK ARE LOCATED UNDER DRIVEWAYS AND SLABS CONTRACTORS ARE TO ENSURE ADEQUATE COMPACTION TO TRENCH BACKFILL IS ACHIEVED TO SUPPORT CONCRETE.

#### PATH/DRIVEWAY NOTES:

- DRIVEWAY SLOPE NOT TO EXCEED 1:4. CHECK WITH LOCAL AUTHORITY REQUIREMENTS PRIOR TO CONSTRUCTING ANY DRIVEWAYS, PATHWAYS OR CROSSOVERS BETWEEN THE PROPERTY BOUNDARY AND ROAD KERB.
- PROVIDE A LAYER OF SAND A MINIMUM OF 20mm THICK UNDER THE SLAB, COMPACTED AND LEVELED.
- SLAB THICKNESS, MESH TO ENGINEERS DESIGN.

#### 3D VIEW NOTES:

- GROUND LINE OR SLOPE OF SITE IS NOT REPRESENTED ON 3D VIEWS.
- FURNITURE AND FIXTURES ARE INDICATIVE ONLY AND ARE NOT PRESCRIPTIVE.
- 3D VIEWS, PERSPECTIVES AND ILLUSTRATIONS ARE INTENDED TO BE A VISUAL AID ONLY, THEY ARE NOT PRESCRIPTIVE BUT INDICATIVE ONLY. THE IMAGES ARE NOT TO BE RELIED UPON IN ANY WAY FOR FINAL CONSTRUCTION FINISHES AND RESULTS.

#### ELECTRICAL NOTES:

- SMOKE ALARMS TO BE INSTALLED IN ACCORDANCE WITH THE CURRENT **ABCB HOUSING PROVISIONS PART 9.5** SMOKE ALARMS MUST COMPLY WITH AS 3786.
  - ONLY USE PHOTOELECTRIC TYPE SMOKE ALARMS
  - ALL SMOKE ALARMS TO BE INTERCONNECTED
  - INSTALL LOCATIONS:
    - ON EACH LEVEL OF LIVING SPACE
    - OUTSIDE EACH BEDROOM AREA
    - IN EVERY BEDROOM (**QLD ONLY**)
- THIS PLAN IS INDICATIVE ONLY AND IS TO BE USED ONLY AS AN EXAMPLE. OWNERS TO NOMINATE FINAL POSITIONS OF ELECTRICAL APPLIANCES, LIGHTING AND ELECTRICAL FITTINGS.

#### ELEVATION NOTES:

- WALL FINISHES AND WINDOW TYPES ARE INDICATIVE ONLY AND ARE NOT PRESCRIPTIVE. REFER TO BUILDERS SPECIFICATIONS FOR DETAILS.
- GROUND LINE SHOWN ON ELEVATIONS DOES NOT RELATE TO ACTUAL SLOPE OF SITE.
- FURNITURE AND FIXTURES ARE INDICATIVE ONLY AND ARE NOT PRESCRIPTIVE.
- ELEVATIONS ARE INTENDED TO BE A VISUAL AID ONLY, THEY ARE NOT PRESCRIPTIVE BUT INDICATIVE ONLY. THE IMAGES ARE NOT TO BE RELIED UPON IN ANY WAY FOR FINAL CONSTRUCTION FINISHES AND RESULTS.

#### SECTION NOTES:

- TRUSS DESIGN IS INDICATIVE ONLY AND IS NOT PRESCRIPTIVE. FINAL DESIGN TO TRUSS MANUFACTURER SPECIFICATIONS.
- ALL PINE TO BE JD4 MIN.
- ALL HWD. TO BE F14 MIN.
- GROUND LINE SHOWN DOES NOT RELATE TO ACTUAL SLOPE OF SITE.
- FURNITURE AND FIXTURES ARE INDICATIVE ONLY AND ARE NOT PRESCRIPTIVE.
- SECTIONS ARE INTENDED TO BE A VISUAL AID ONLY, THEY ARE NOT PRESCRIPTIVE BUT INDICATIVE ONLY. THE IMAGES ARE NOT TO BE RELIED UPON IN ANY WAY FOR FINAL CONSTRUCTION FINISHES AND RESULTS.

#### FOUNDATION NOTES:

- THESE PLANS ARE TO BE READ IN CONJUNCTION WITH CONTRACT DOCUMENTS AND ENGINEERS DRAWINGS AND SPECIFICATIONS. RESPONSIBLE PARTIES ARE TO BE NOTIFIED OF ANY DISCREPANCIES. SITE CLASSIFICATION IS TO BE CONFIRMED BY INSPECTION OF FOOTING EXCAVATIONS.
- PLUMBER RESPONSIBLE TO LOCATE AND CONFIRM SEWER HOUSE CONNECTION LOCATION ACCURATELY PRIOR TO COMMENCEMENT. PLUMBER IS TO VERIFY WITH SITE SUPERVISOR PRIOR TO SETTING OUT FIXTURE DRAINAGE POINTS. NO AMENDMENTS OR SPECIAL FIXTURES HAVE BEEN NOMINATED.
- WHERE SERVICES / PIPEWORK ARE LOCATED UNDER DRIVEWAYS AND SLABS CONTRACTORS ARE TO ENSURE ADEQUATE COMPACTION TO TRENCH BACKFILL ACHIEVED TO SUPPORT CONCRETE.
- REBATE GARAGE DOORS & SLIDING GLASS DOORS 20mm, AND SHOWER RECESSES 50mm IN LOCATIONS SHOWN.
- ACCORDING TO MANUF' SPEC. OR BUILDERS DIRECTIONS.
- MINIMUM COVER TO GROUND - 50mm.
- TOP COVER TO SLAB REINFORCEMENT - 30mm.
- GRADE FINISHED GROUND SURFACE TO DIVERT WATER AWAY FROM BUILDING.
- WATERPROOF MEMBRANE IS 0.2mm POLYETHYLENE. JOINTS ARE TO BE LAPPED 300mm AND TAPED.
- REINFORCEMENT TO BE SUPPORTED ON PLASTIC CHAIRS AT 1000mm CRS.
- ALL CONCRETE IS TO BE MECHANICALLY VIBRATED DURING PLACING.
- FILL MATERIAL AND SAND UNDER SLABS IS TO BE COMPACTED TO 95% OF MAX. DRY DENSITY.
- FLOORS TO ALL WET AREAS TO HAVE A FALL TO A FLOOR WASTE.

#### SITE INFORMATION:

LAND TITLE REFERENCE: 142477/1  
SUBURB: PERTH  
LOCALITY: PERTH  
LOCAL AUTHORITY: NORTHERN MIDLANDS  
ZONING: GENERAL RESIDENTIAL

LAND SIZE: 1942m<sup>2</sup>  
DWELLING FLOOR AREA: ???m<sup>2</sup>  
SITE COVERAGE: ???%  
PORCH/PATIO AREA: ???m<sup>2</sup>

#### OVERLAYS:

AIRPORT OBSTACLE LIMITATION AREA

WIND CLASSIFICATION: N- (-m/s)  
TERRAIN CATEGORY: TC ???  
SHIELDING: ???  
BAL LEVEL: ???  
TOPOGRAPHIC: ???  
CLIMATE ZONE: ???  
CORROSION ENVIRONMENT: ???  
SOIL TYPE: CLASS ???

#### FLOOR PLAN NOTES:

- SMOKE ALARMS TO BE INSTALLED IN ACCORDANCE WITH THE CURRENT **ABCB HOUSING PROVISIONS PART 9.5** SMOKE ALARMS MUST COMPLY WITH AS3786.
  - ONLY USE PHOTOELECTRIC TYPE SMOKE ALARMS
  - ALL SMOKE ALARMS TO BE INTERCONNECTED
  - INSTALL LOCATIONS:
    - ON EACH LEVEL OF LIVING SPACE
    - OUTSIDE EACH BEDROOM AREA
    - IN EVERY BEDROOM (**QLD ONLY**)
- WALL FINISHES AND WINDOW TYPES ON 3D VIEWS ARE INDICATIVE ONLY AND ARE NOT PRESCRIPTIVE.
- ALL GLAZING TO BE IN ACCORDANCE WITH AS1288. WINDOWS SIZES MAY VARY DUE TO MANUFACTURER'S SPECIFICATIONS.
- BUILDER TO CONFIRM ALL DIMENSIONS PRIOR TO CONSTRUCTION. DIMENSIONS ARE TO FRAME ONLY AND DO NOT INCLUDE CLADDING/LININGS (UNO).
- S.S. BALUSTRADING TO COMPLY WITH THE CURRENT **ABCB HOUSING PROVISIONS PART 11.3.6**
- DOORS TO W.C.'S TO HAVE LIFT OFF HINGES (ONLY IF THE DOORS SWING IN TOWARDS THE W.C.).
- MASONRY CONSTRUCTION TO AS 3700.
- REFER ENGINEERS DRAWINGS & SPECIFICATIONS FOR ALL STRUCTURAL DETAILS, FRAMING, BRACING, TIE DOWN AND SLAB/FOOTING DETAILS.
- SEAL WET AREAS IN ACCORDANCE WITH AS3740 & THE CURRENT **ABCB HOUSING PROVISIONS PART 10.2**.
- PROVIDE FLOOR WASTE TO ALL WET AREAS.

#### ROOF DRAINAGE NOTES:

- ALL GUTTER AND DOWNPIPE WORKS TO AS/NZS 3500.3 AND THE CURRENT **ABCB HOUSING PROVISIONS PART 7.4**.
- DOWNPIPES (DP) TO BE 90mmØ UPVC.
- TEMPORARY DOWNPIPES TO BE PROVIDED AT DP LOCATIONS DURING CONSTRUCTION DRAINING ROOFWATER ONTO GROUND, 2M MIN AWAY FROM BUILDING.
- ALL STORMWATER, DOWN PIPES, RAIN WATER TANKS & SITE DRAINAGE TO BE SIZED & LOCATED BY THE HYDRAULIC CONSULTANT/ PLUMBER IN ACCORDANCE WITH **THE CURRENT NCC VOL. 3 PART B6 AND B7**, THE CURRENT **ABCB HOUSING PROVISIONS PART 7.4**, STATE LEGISLATION/ LOCAL PLANNING SCHEME HOUSE CODE AND AS 3500 ALL PARTS.
- THE ROOF DRAINAGE SYSTEM MUST BE PROVIDED WITH AN OVERFLOW TO PREVENT THE BACKFLOW OF WATER INTO THE BUILDING.
- THE AREA SPECIFIC RAINFALL INTENSITY **FOR GUTTERING SELECTION, OVERFLOW MEASURES & DOWNPIPES MUST BE SELECTED FROM THE RELEVANT TABLES IN THE CURRENT ABCB HOUSING PROVISIONS PART 7.4** OR FROM AS/NZ3500.
- EAVES GUTTERS MUST BE INSTALLED AT A FALL NOT LESS THAN 1 IN 500 WITH SUPPORT BRACKETS AT 1.2m MAXIMUM CENTRES.
- BOX GUTTERS MUST BE INSTALLED AT A FALL NOT LESS THAN 1 IN 100 IN ACCORDANCE WITH AS/NZ3500.3.
- DOWNPIPES MUST SERVE NOT MORE THAN 12 METERS OF GUTTER LENGTH FOR EACH DOWNPIPE WHICH MUST BE LOCATED AS CLOSE AS POSSIBLE TO VALLEY GUTTERS. EAVES GUTTERS MUST BE PROVIDED WITH AN OVERFLOW SYSTEM WHERE DOWNPIPES ARE LOCATED MORE THAN 1.2 METRES FROM A VALLEY GUTTER.



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ABN: 31 615 195 818

PLAN NUMBER:  
#0000  
BUILDERS NUMBER:  
N/A  
AREI PLAN CODE:  
AREI DESIGNS

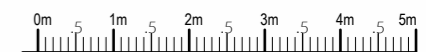
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**ABODE DESIGNER HOMES**  
DRAWING NAME:  
**COVER**

PROJECT:  
**PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK STREET PERTH TAS 7300**

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CHK  
PAGE NO:  
01 OF 05  
SCALE @ A3  
1:100

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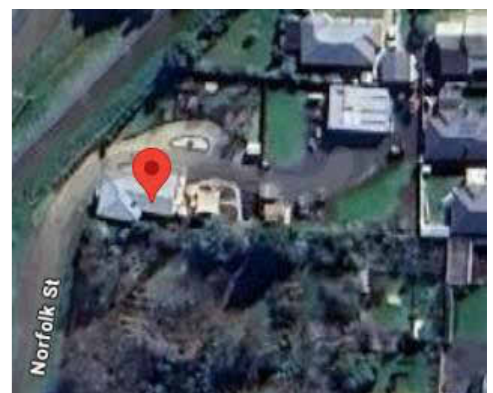
6.5.2026



NORFOLK STREET

### LEGEND

- UNDERGROUND GAS MARKER
- HYDRANT
- STORM WATER PIT
- WATER CONNECTION
- ELECTRICAL TURRET
- TELSTRA PIT
- MAN HOLE
- 100mm DOWN PIPE
- POWER POLE
- STREET LIGHT
- SITE BENCH MARK
- SEWER LINE
- CONTOUR LINE
- EXISTING RETAINING
- NEW RETAINING
- ROOF LINE
- EXISTING FENCE
- PROPOSED FENCE
- ELECTRICAL (UNDERGROUND)
- ELECTRICAL (OVERHEAD)
- TELSTRA COMMUNICATIONS
- WATER LINE
- DOWNPIPE (DP) STORM WATER LINE
- CONCRETE DRIVEWAY / PARKING / PATHWAY
- VISITOR CARPORT PARKING 5.4 x 3.0
- PRIVATE OPEN SPACE 6m x 4m
- UNCOVERED CAR PARKING SPACES 5.4 x 3.0
- SECURE STORAGE SPACE 2.2m x 1.5m



COUNCIL ONLINE MAPPING AERIAL VIEW

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 ABN: 31 615 195 818

PLAN NUMBER:  
#0000

BUILDERS NUMBER:  
N/A

AREI PLAN CODE:  
AREI DESIGNS

CLIENT:  
**ABODE DESIGNER HOMES**

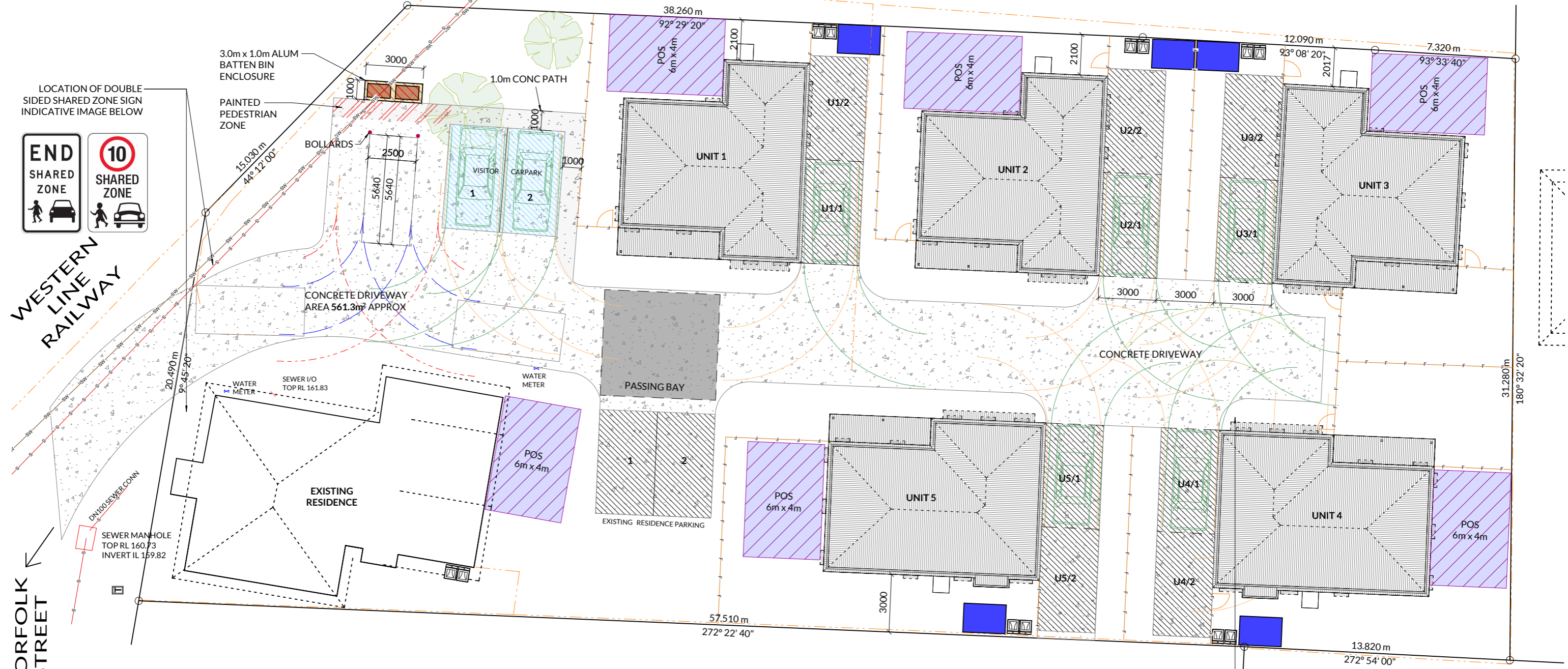
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**SITE PLAN**

PROJECT:  
**PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK STREET PERTH TAS 7300**

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02 OF 05

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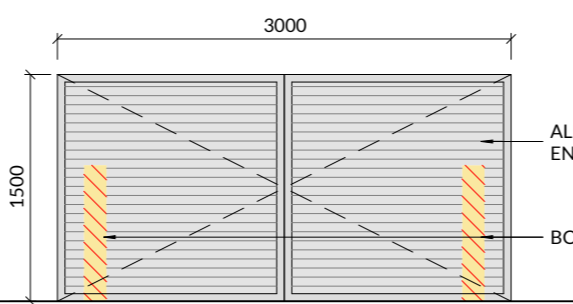


WESTERN LINE RAILWAY

NORFOLK STREET

- CONCRETE DRIVEWAY / PARKING / PATHWAY
- VISITOR CARPORT PARKING 5.4 x 3.0
- UNCOVERED CAR PARKING SPACES 5.4 x 3.0
- SECURE STORAGE SPACE 2.2m x 1.5m

PROVIDED PARKING SPACES	
VISITOR PARKS (3m x 5.4m MIN.)	2
PRIVATE UNCOVERED PARKS (3m x 5.4m MIN.)	12
<b>TOTAL PROVIDED PARKING SPACES</b>	<b>14</b>



**BIN ENCLOSURE ELEVATION**  
1:50

LOCATION OF DOUBLE SIDED SHARED ZONE SIGN INDICATIVE IMAGE BELOW



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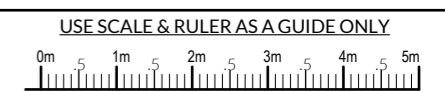
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BUILDERS NUMBER: N/A  
AREI PLAN CODE: AREI DESIGNS

CLIENT: **ABODE DESIGNER HOMES**  
DRAWING NAME: **DRIVEWAY PLAN**

PROJECT: **PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK STREET PERTH TAS 7300**

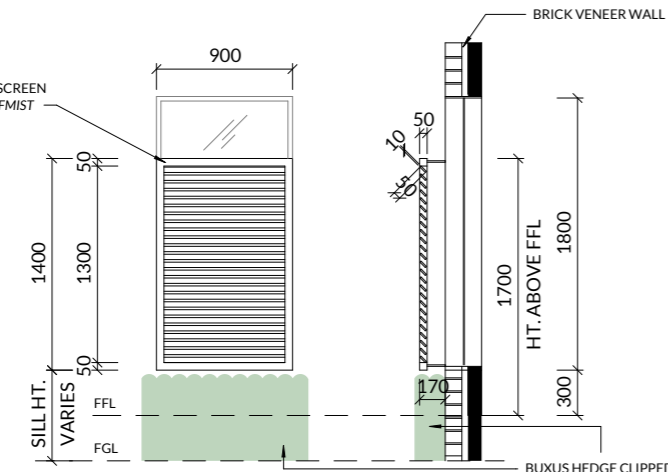
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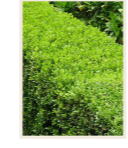


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INDICATIVE IMAGE OF WINDOW PRIVACY SCREEN AND HEDGING



INDICATIVE HEDGE PLANTING TO REDUCE VEHICLE LIGHT IMPACT TO HABITABLE ROOMS ADJOINING COMMON DRIVEWAY

ENGLISH BOX - BUXUS SEMPERVIRENS CLIPPED TO HEIGHT OF 0.6m x 0.6m WIDE PLANTED IN 1m WIDE GARDEN BEDS FRONT OF / BELOW HABITABLE WINDOWS ADJOINING COMMON DRIVEWAY

- INDICATIVE LOCATION OF COUNCIL RUBBISH BIN STORAGE AREAS
- INDICATIVE LOCATION OF ALUMINIUM WINDOW PRIVACY SCREENS
- PRIVATE OPEN SPACE 6m x 4m
- INDICATIVE BOUNDARIES OF LAWN / PRIVATE GARDEN AREAS
- INDICATIVE LOCATION OF GARDEN BEDS / COMMUNAL LANDSCAPED AREAS

WINDOW PRIVACY SCREEN DETAIL (TYP) 1:50

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PLAN NUMBER: #0000  
BUILDERS NUMBER: N/A  
AREI PLAN CODE: AREI DESIGNS

CLIENT: **ABODE DESIGNER HOMES**  
DRAWING NAME: **LANDSCAPE PLAN**

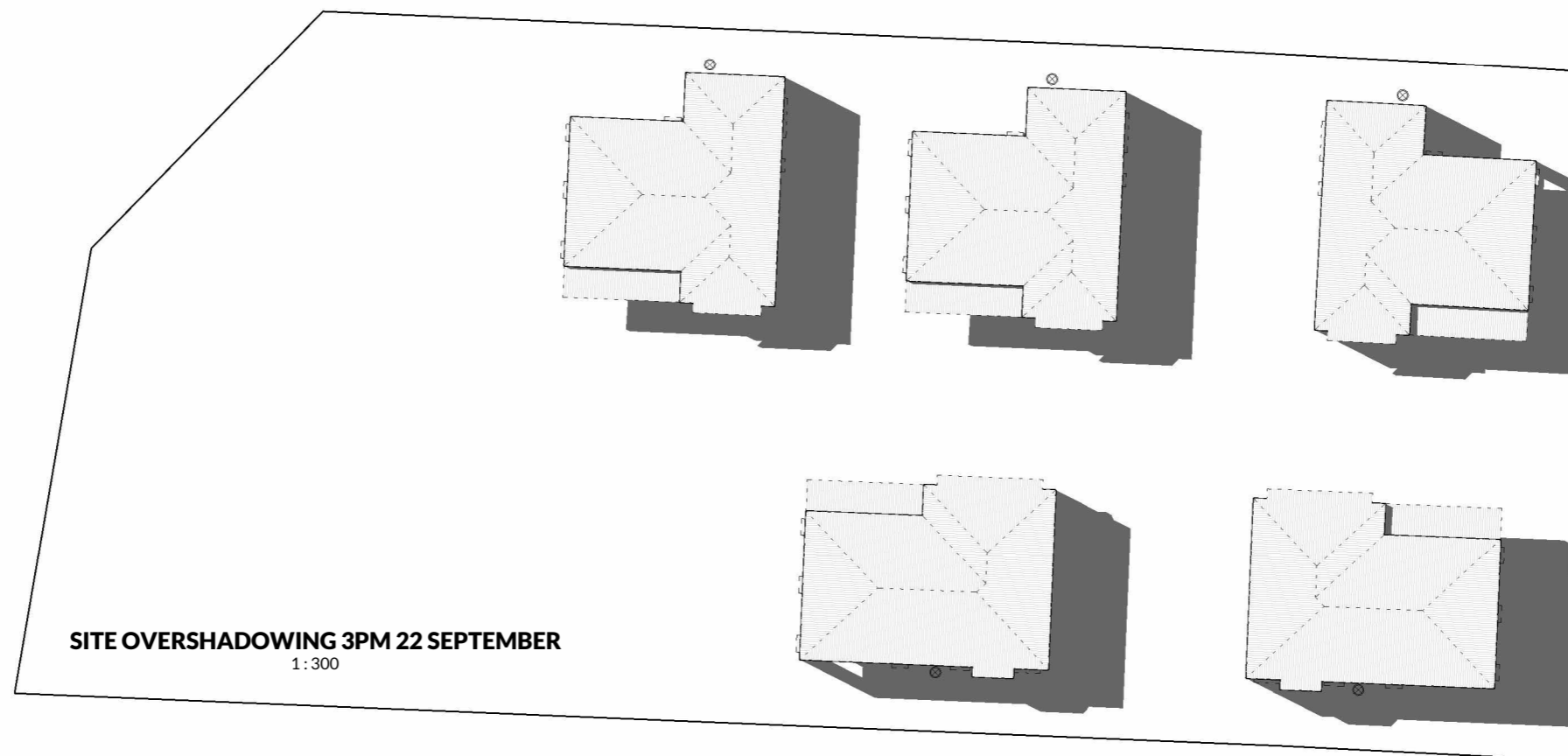
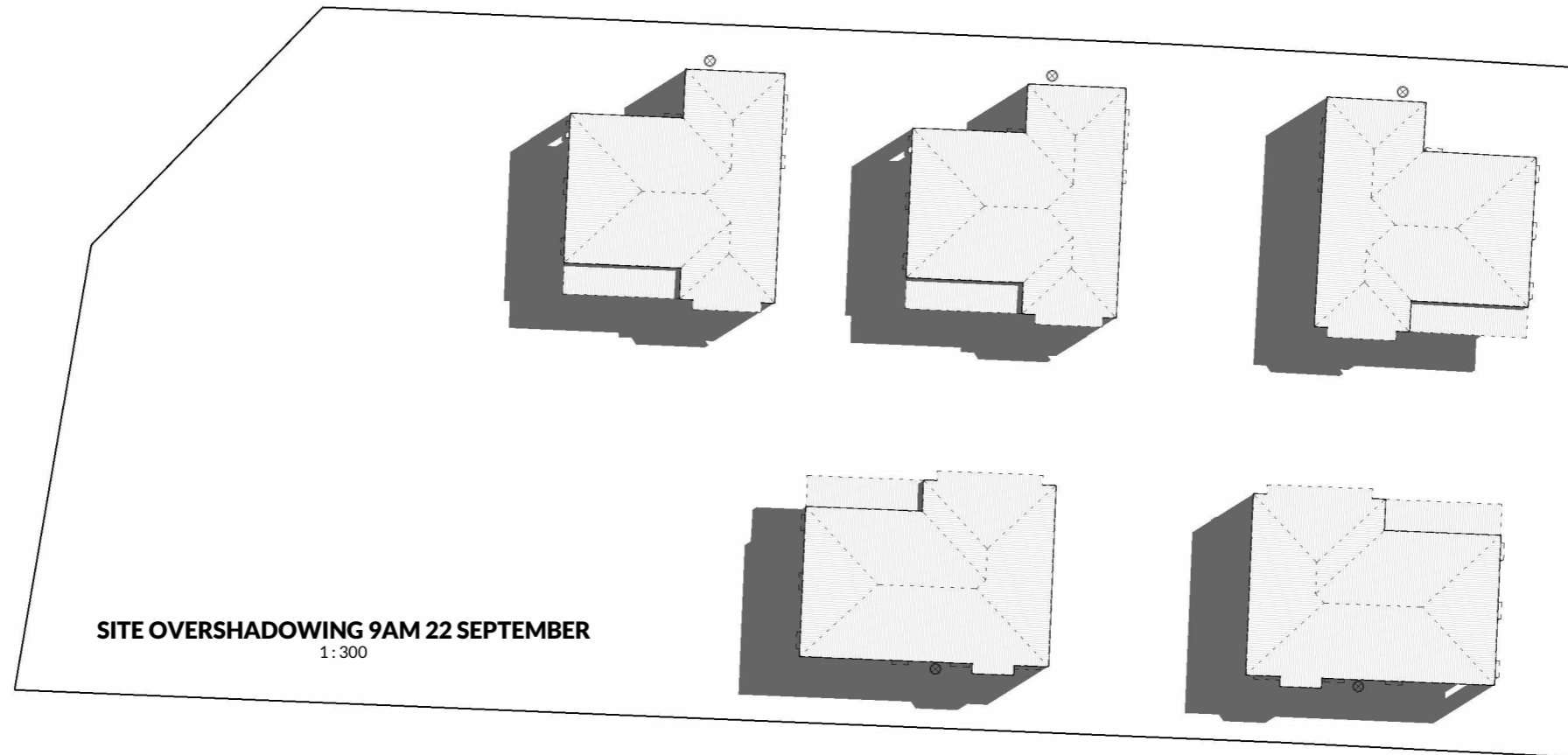
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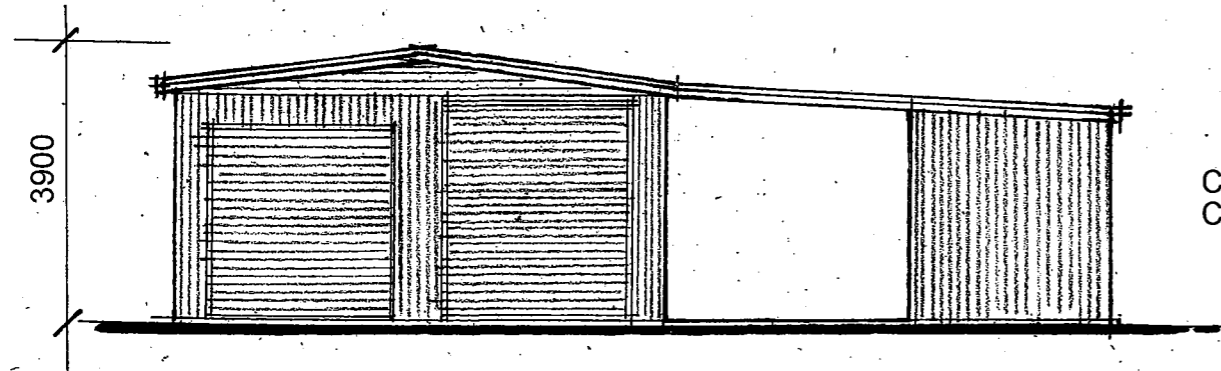
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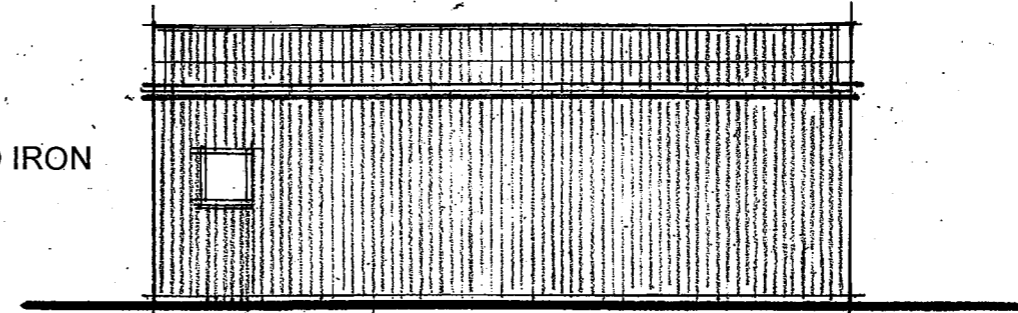
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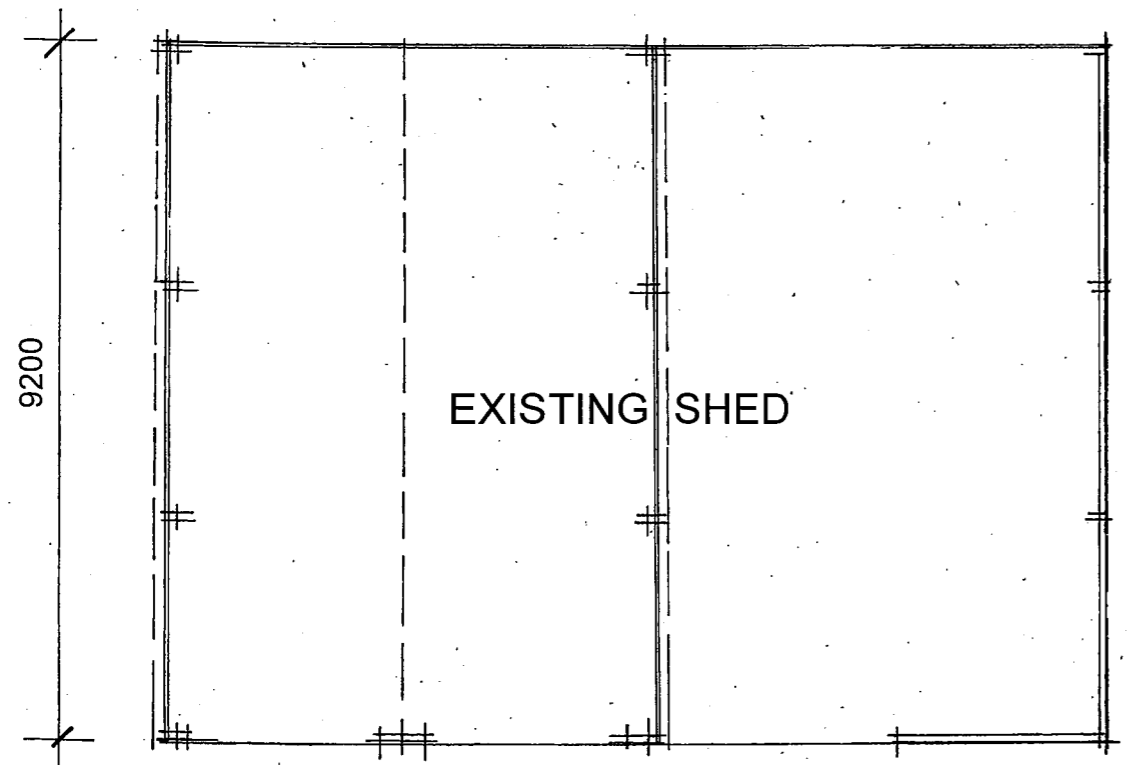


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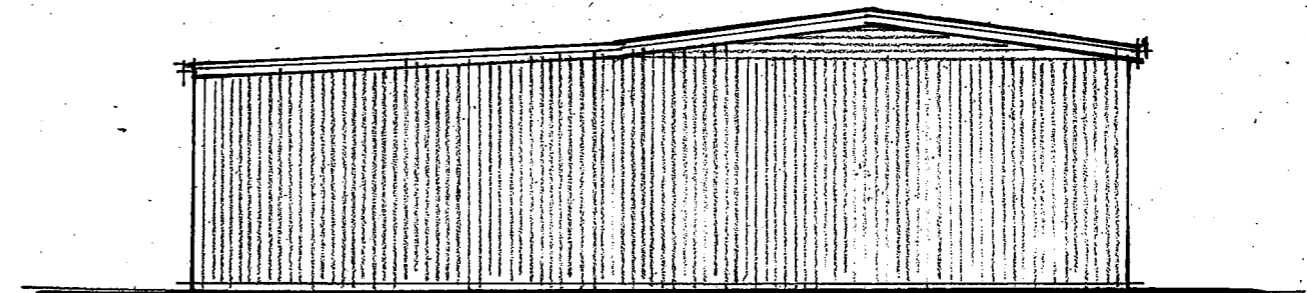
CORRAGATED IRON CLADDING



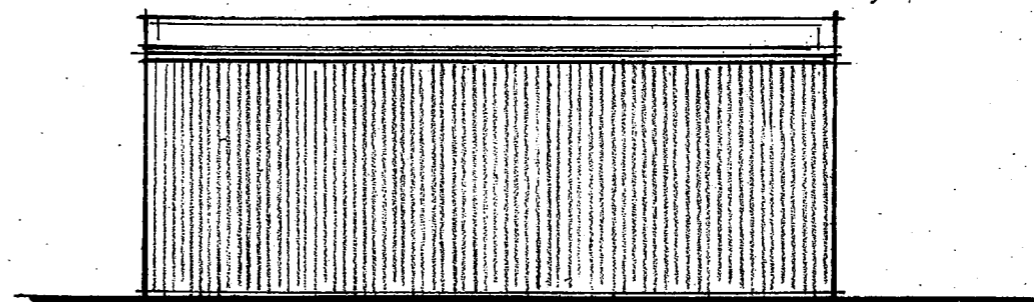
EAST ELEVATION



PLAN 1:100  
EXISTING SHED TO BE DEMOLISHED



NORTH ELEVATION



WEST ELEVATION



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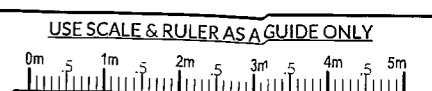
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AREI PLAN CODE:  
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CLIENT:  
**ABODE DESIGNER  
HOMES**  
DRAWING NAME:

PROJECT:  
**PROPOSED RESIDENCE FOR  
ABODE DESIGNER HOMES AT  
23 NORFOLK STREET  
PERTH TAS 7300**

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### FLOOR AREAS

ROOM	SQUARES	SQM
CARPORT	1.78	16.5 m <sup>2</sup>
LIVING	8.71	80.9 m <sup>2</sup>
VERANDAH	0.81	7.5 m <sup>2</sup>
TOTAL	11.29	104.9 m <sup>2</sup>

### WINDOW SCHEDULE

MARK	SIZE	DESCRIPTION
W1	1800 x 750	ALUM. FRAMED AWNING
W2	1800 x 750	ALUM. FRAMED AWNING
W3	1800 x 750	ALUM. FRAMED AWNING
W4	1800 x 750	ALUM. FRAMED AWNING
W5	1800 x 750	ALUM. FRAMED AWNING
W6	1800 x 750	ALUM. FRAMED AWNING
W7	1800 x 750	ALUM. FRAMED AWNING
W8	2100 x 1800	ALUM. FRAMED SLIDING GLASS DOOR
W9	1800 x 750	ALUM. FRAMED AWNING
W10	1200 x 1800	ALUM. FRAMED SLIDING WINDOW
W11	900 x 600	ALUM. FRAMED AWNING
W12	2100 x 1400	ALUM. FRAMED SLIDING GLASS DOOR

### DOOR SCHEDULE

MARK	SIZE	DOOR TYPE
D1	2040 x 820	ENTRY DOOR - HUME XV14
D2	2040 x 820	HOLLOW CORE SWING
D3	2040 x 820	HOLLOW CORE SWING
D4	2040 x 820	HOLLOW CORE SWING
D5	2040 x 820	HOLLOW CORE SWING
D6	2040 x 720	HOLLOW CORE CAVITY SLIDER

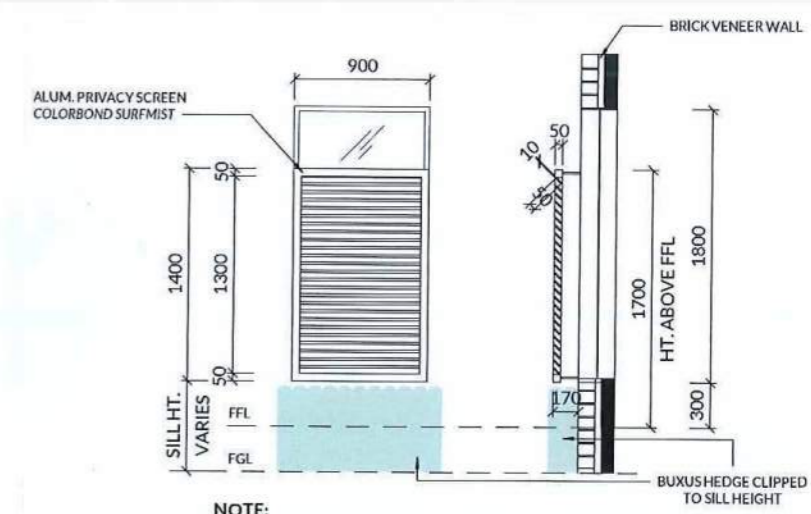
### LEGEND

- C CASEMENT WINDOW
- SL.G.D SLIDING GLASS DOOR
- S.W SLIDING WINDOW
- A AWNING WINDOW
- D.HU DOUBLE HUNG WINDOW
- B.D. BIFOLD DOORS
- LU LOUVERS
- SL SIDELIGHT
- M.V. MECHANICAL VENT
- SKL SKYLIGHT
- F.G. FIXED GLASS WINDOW

- ☼ SMOKE ALARM
- † HOSE COCK
- ⚡ ELEC M/BOX
- ⊕ GAS BOTTLES
- ⊗ HOT WATER
- ⊕ FLOOR WASTE

### ENERGY EFFICIENCY REQUIREMENTS

- R4.0 BATTS TO CEILING
- R2.0 BATTS TO EXTERNAL WALLS AND GARAGE INTERNAL WALL
- DOWNLIGHTS TO BE MIN IC RATED
- ALL WINDOWS AND SLIDING GLASS DOORS DOUBLE GLAZED

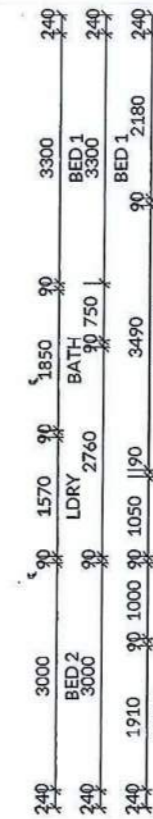
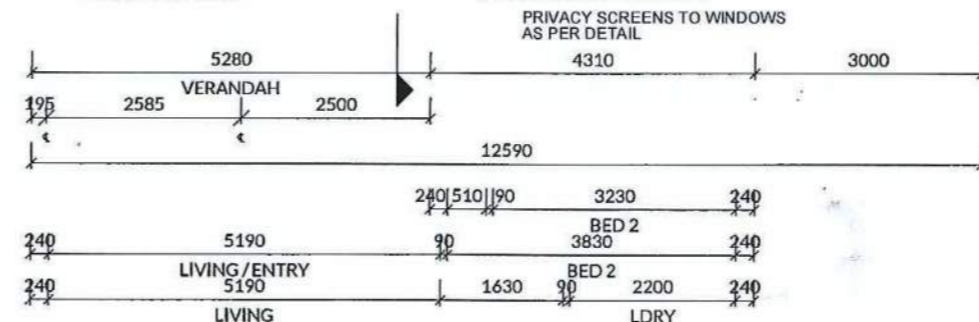
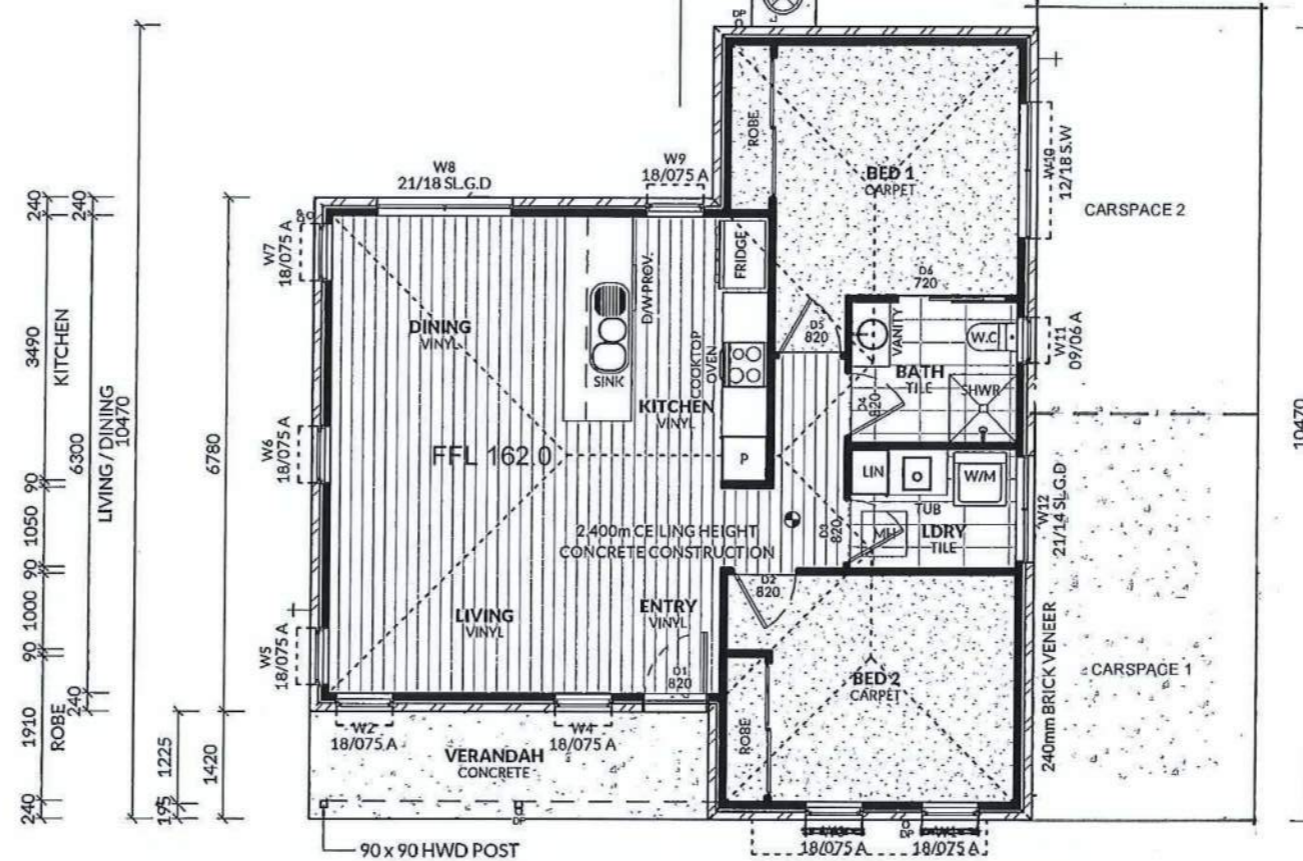
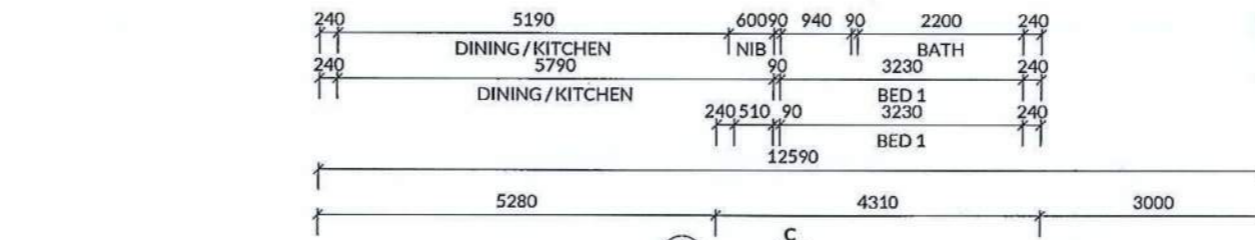


NOTE: TYPICAL DETAIL SHOWS DIMENSIONS FOR 1809 WINDOW SCREEN SIZES WILL DIFFER DEPENDING ON WINDOW SIZE

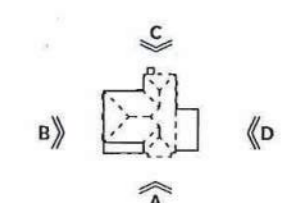
### WINDOW PRIVACY SCREEN DETAIL (TYP)



INDICATIVE IMAGE OF WINDOW PRIVACY SCREEN AND HEDGING



## UNIT 1



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 ABN: 31 615 195 818

PLAN NUMBER: #2297  
 BUILDERS NUMBER: N/A  
 AREI PLAN CODE: AREI DESIGNS  
 CLIENT: **ABODE DESIGNER HOMES**  
 DRAWING NAME: **FLOOR PLAN**

PROJECT: **PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK ST, PERTH**  
 CHECKED: ODW  
 PAGE NO: A3-01 OF 41  
 SCALE @ A3: 1:100

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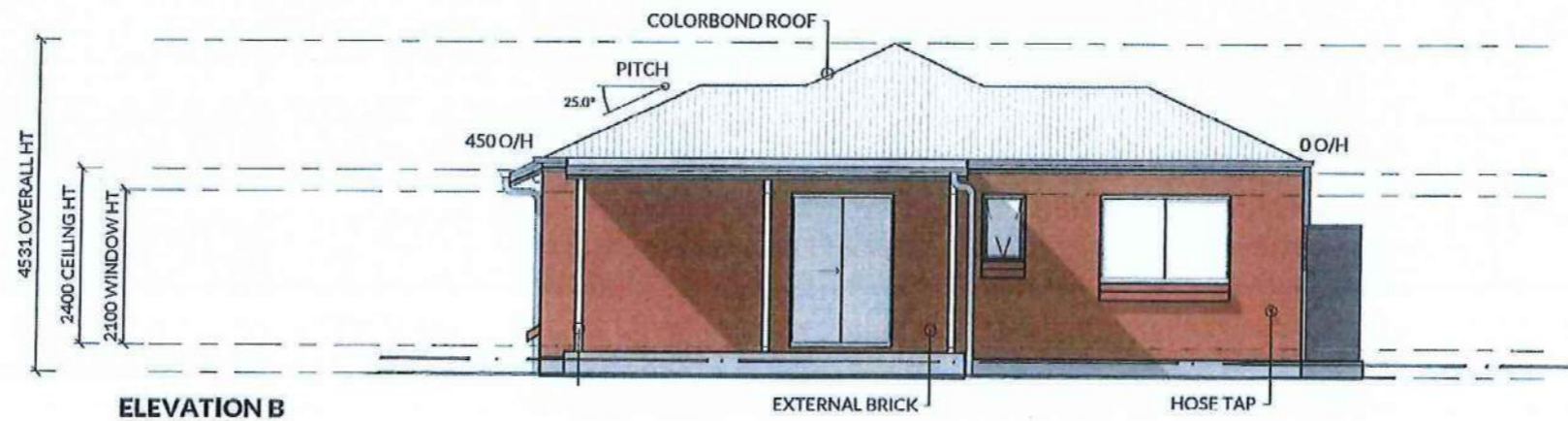
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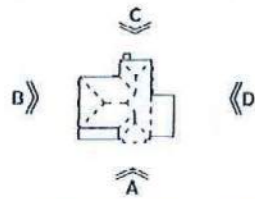


ELEVATION A



ELEVATION B

UNIT 1



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**ELEVATIONS A & B**

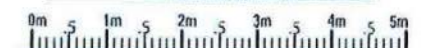
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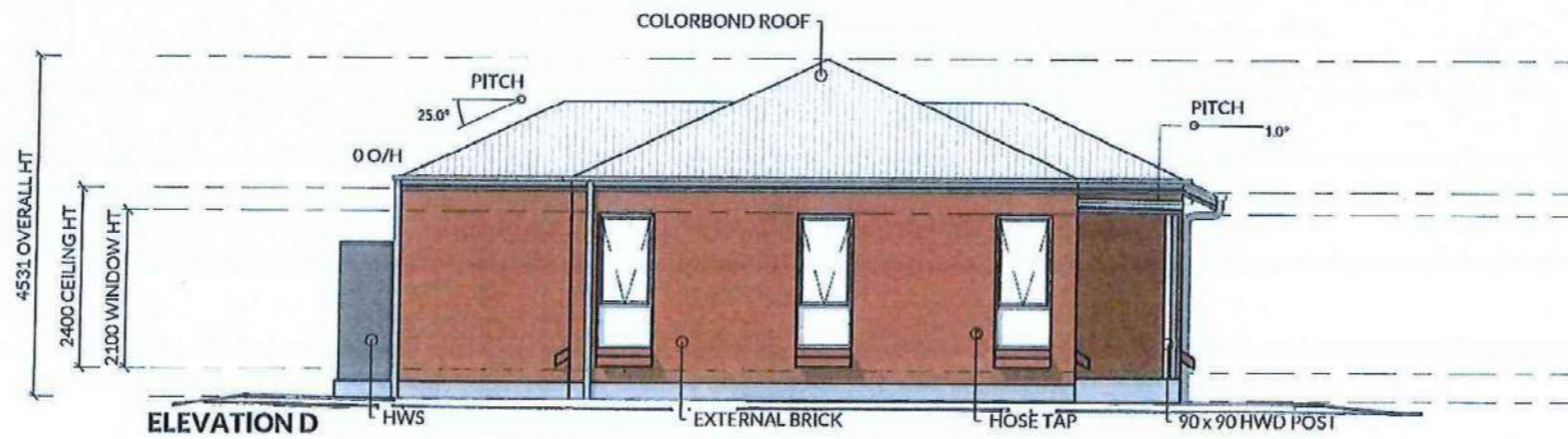
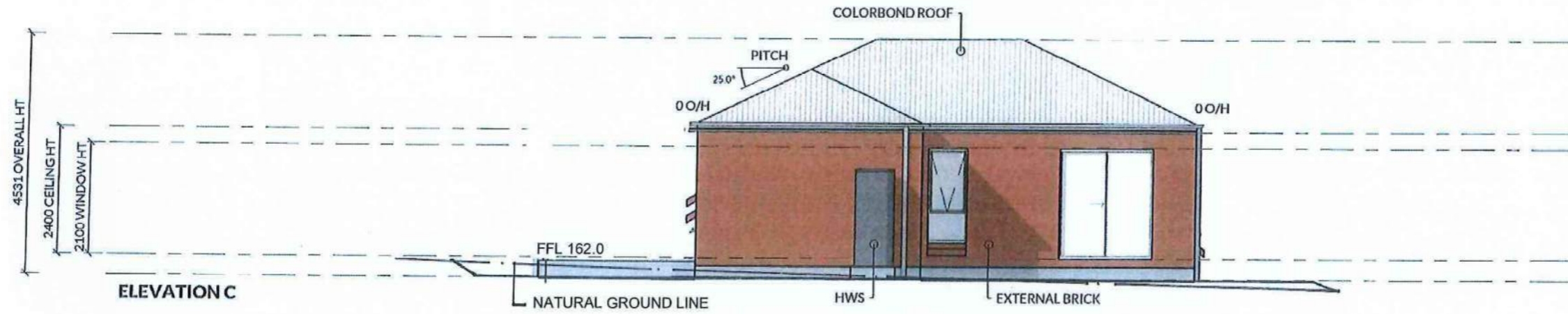
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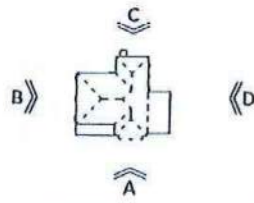
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 AREI PLAN CODE:  
AREI DESIGNS

CLIENT:  
**ABODE DESIGNER HOMES**  
 DRAWING NAME:  
**ELEVATIONS C & D**

PROJECT:  
**PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK ST, PERTH**

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A3-04 OF 41  
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### FLOOR AREAS

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LIVING	8.71	80.9 m <sup>2</sup>
VERANDAH	0.81	7.5 m <sup>2</sup>
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### WINDOW SCHEDULE

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W12	2100 x 1400	ALUM. FRAMED SLIDING GLASS DOOR

### DOOR SCHEDULE

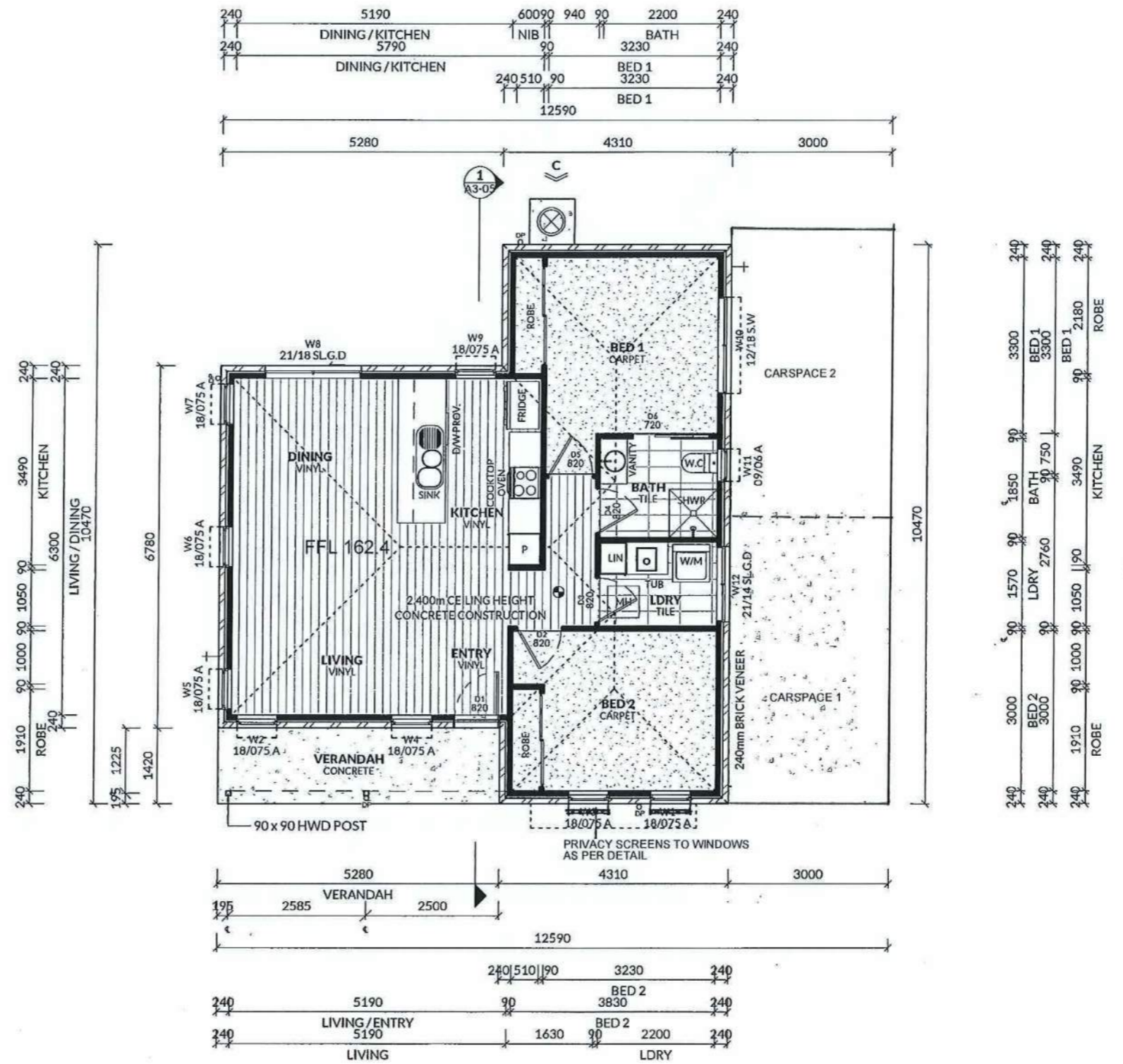
MARK	SIZE	DOOR TYPE
D1	2040 x 820	ENTRY DOOR - HUMEXV14
D2	2040 x 820	HOLLOW CORE SWING
D3	2040 x 820	HOLLOW CORE SWING
D4	2040 x 820	HOLLOW CORE SWING
D5	2040 x 820	HOLLOW CORE SWING
D6	2040 x 720	HOLLOW CORE CAVITY SLIDER

### LEGEND

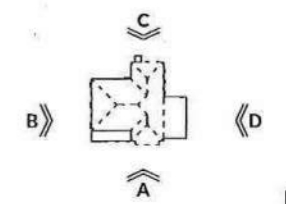
- C CASEMENT WINDOW
- SL.G.D SLIDING GLASS DOOR
- S.W SLIDING WINDOW
- A AWNING WINDOW
- D.HU DOUBLE HUNG WINDOW
- B.D. BIFOLD DOORS
- LU LOUVERS
- SL SIDELIGHT
- M.V. MECHANICAL VENT
- SKL SKYLIGHT
- F.G. FIXED GLASS WINDOW

- ☼ SMOKE ALARM
- † HOSE COCK
- ⊞ ELEC M/BOX
- ⊕ GAS BOTTLES
- ⊗ HOT WATER
- ⊕ FLOOR WASTE

- #### ENERGY EFFICIENCY REQUIREMENTS
- R4.0 BATTS TO CEILING
  - R2.0 BATTS TO EXTERNAL WALLS AND GARAGE INTERNAL WALL
  - DOWNLIGHTS TO BE MIN I.C RATED
  - ALL WINDOWS AND SLIDING GLASS DOORS DOUBLE GLAZED



## UNIT 2



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 VIC VBA: CDB-U 73620  
 TAS BSP: 071565667  
 ABN: 31 615 195 818

PLAN NUMBER:  
#2297  
 BUILDERS NUMBER:  
N/A  
 AREI PLAN CODE:  
AREI DESIGNS

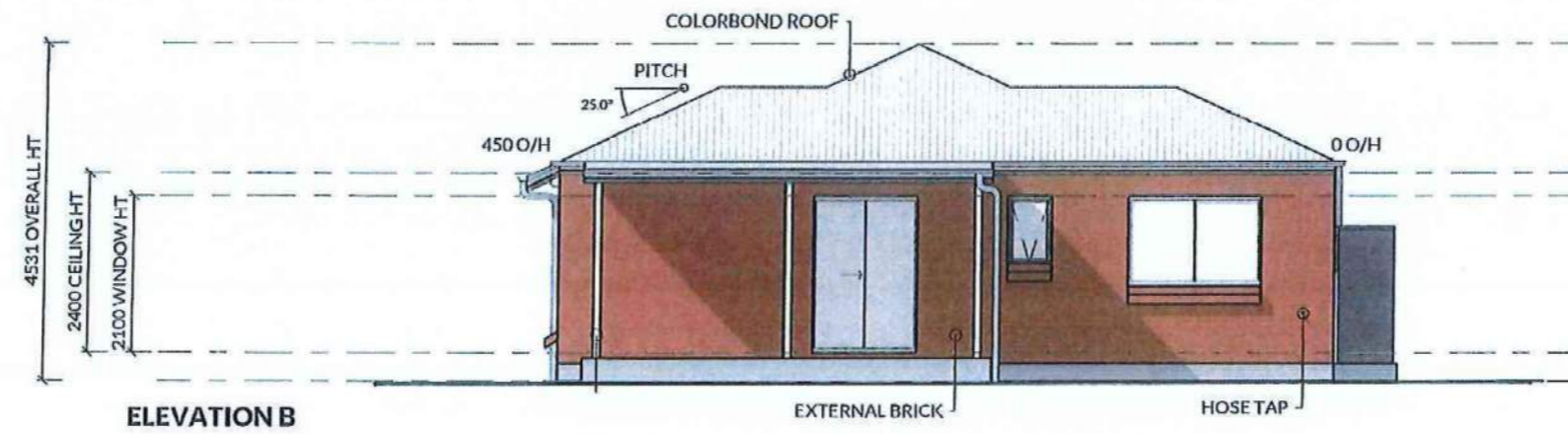
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 DRAWING NAME:  
**FLOOR PLAN**

PROJECT:  
**PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK ST, PERTH**

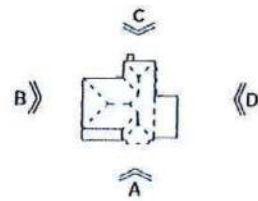
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 AREI DESIGNS

CLIENT:  
**ABODE DESIGNER  
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 DRAWING NAME:  
**ELEVATIONS A & B**

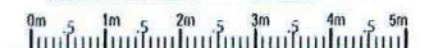
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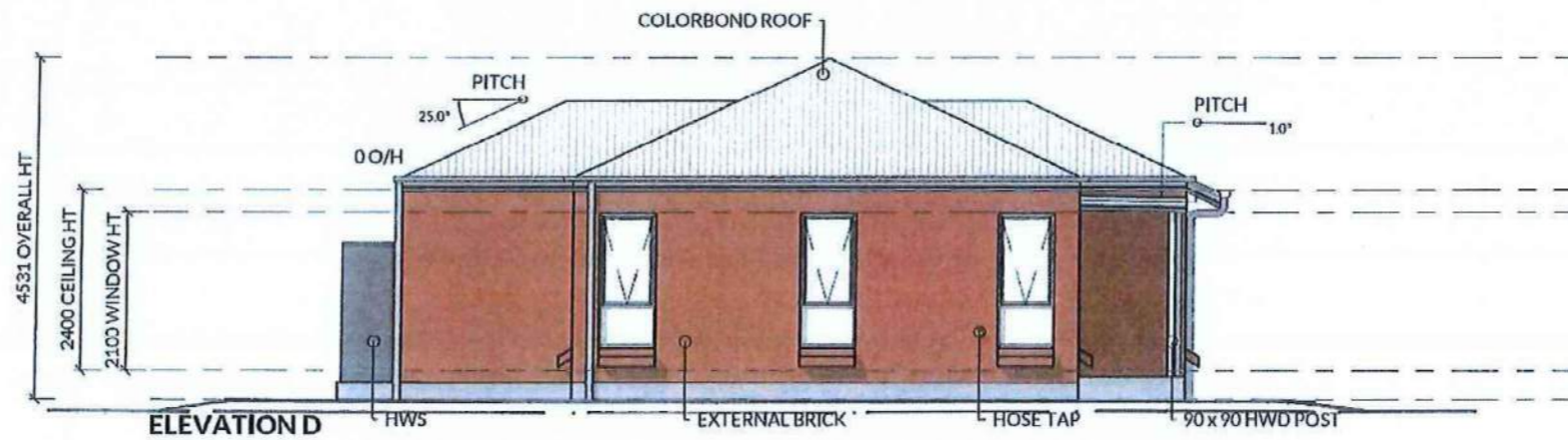
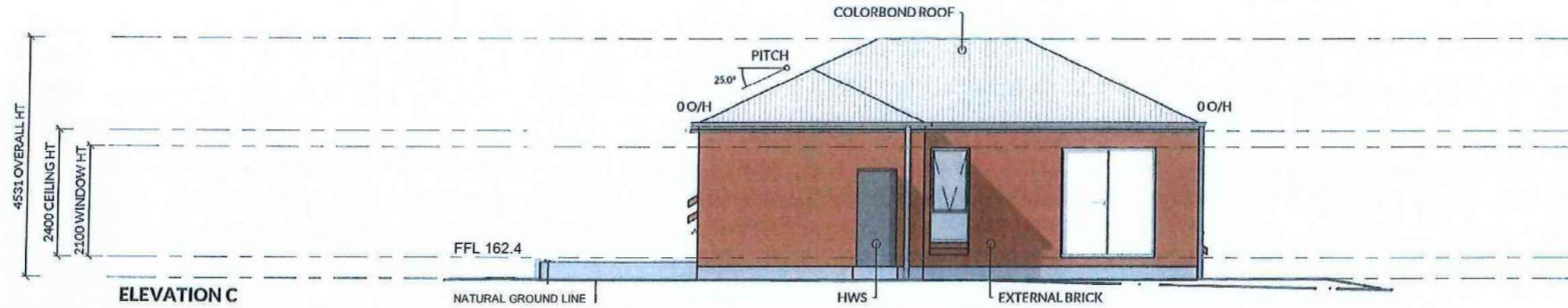
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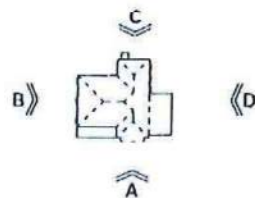
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### FLOOR AREAS

ROOM	SQUARES	SQM
CARPORT	1.78	16.5 m <sup>2</sup>
LIVING	8.71	80.9 m <sup>2</sup>
VERANDAH	0.81	7.5 m <sup>2</sup>
TOTAL	11.29	104.9 m <sup>2</sup>

### WINDOW SCHEDULE

MARK	SIZE	DESCRIPTION
W1	1800 x 750	ALUM. FRAMED AWNING
W2	1800 x 750	ALUM. FRAMED AWNING
W3	1800 x 750	ALUM. FRAMED AWNING
W4	1800 x 750	ALUM. FRAMED AWNING
W5	1800 x 750	ALUM. FRAMED AWNING
W6	1800 x 750	ALUM. FRAMED AWNING
W7	1800 x 750	ALUM. FRAMED AWNING
W8	2100 x 1800	ALUM. FRAMED SLIDING GLASS DOOR
W9	1800 x 750	ALUM. FRAMED AWNING
W10	1200 x 1800	ALUM. FRAMED SLIDING WINDOW
W11	900 x 600	ALUM. FRAMED AWNING
W12	2100 x 1400	ALUM. FRAMED SLIDING GLASS DOOR

### DOOR SCHEDULE

MARK	SIZE	DOOR TYPE
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D4	2040 x 820	HOLLOW CORE SWING
D5	2040 x 820	HOLLOW CORE SWING
D6	2040 x 720	HOLLOW CORE CAVITY SLIDER

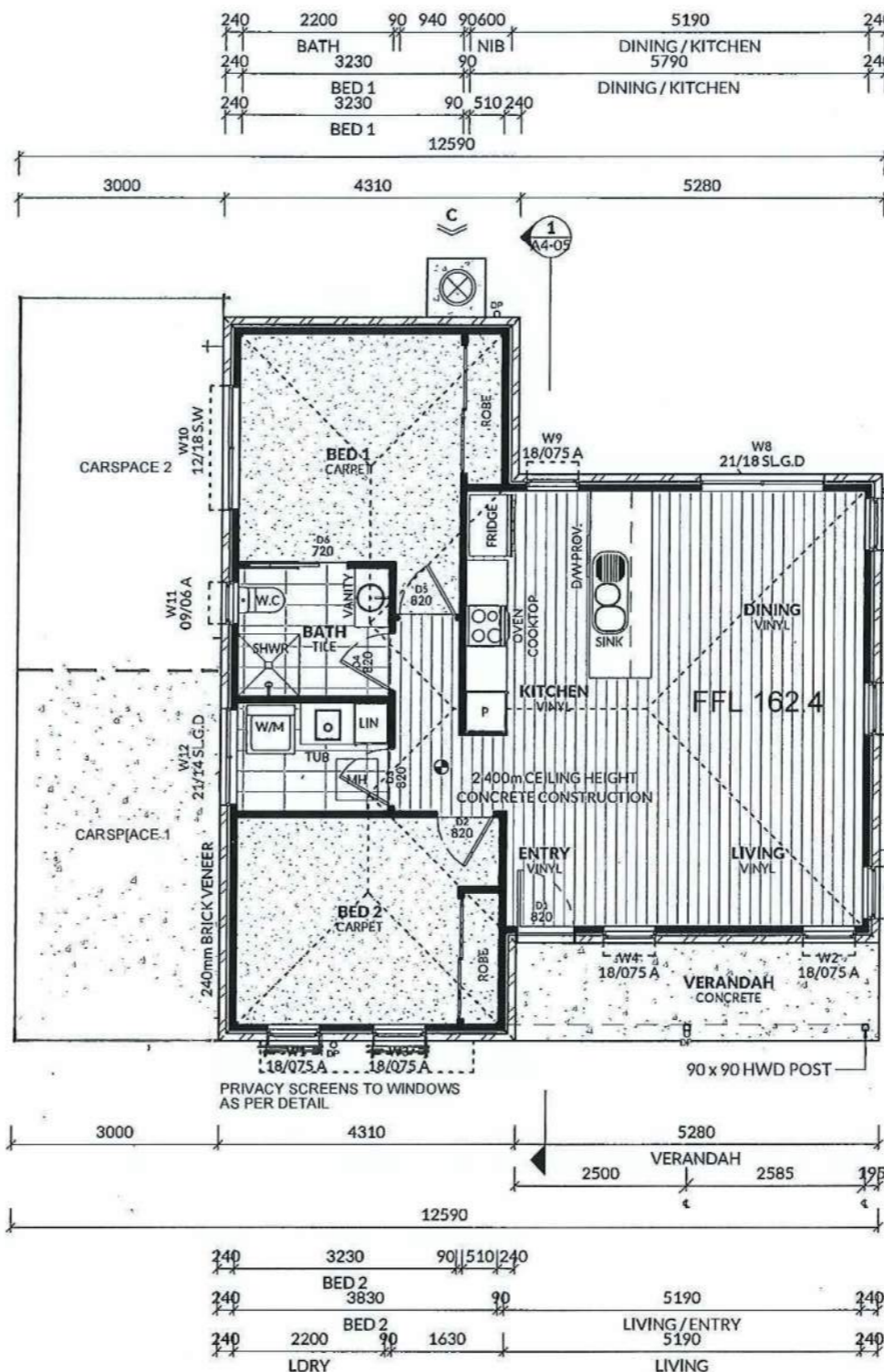
### LEGEND

- C CASEMENT WINDOW
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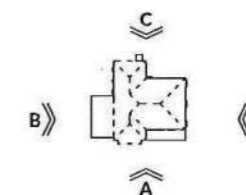
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- † HOSE COCK
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- ⊙ GAS BOTTLES
- ⊗ HOT WATER
- ⊕ FLOOR WASTE

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## UNIT 3



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PLAN NUMBER:  
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N/A  
 AREI PLAN CODE:  
AREI DESIGNS

CLIENT:  
**ABODE DESIGNER HOMES**  
 DRAWING NAME:  
**FLOOR PLAN**

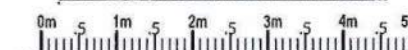
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**PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK ST, PERTH**

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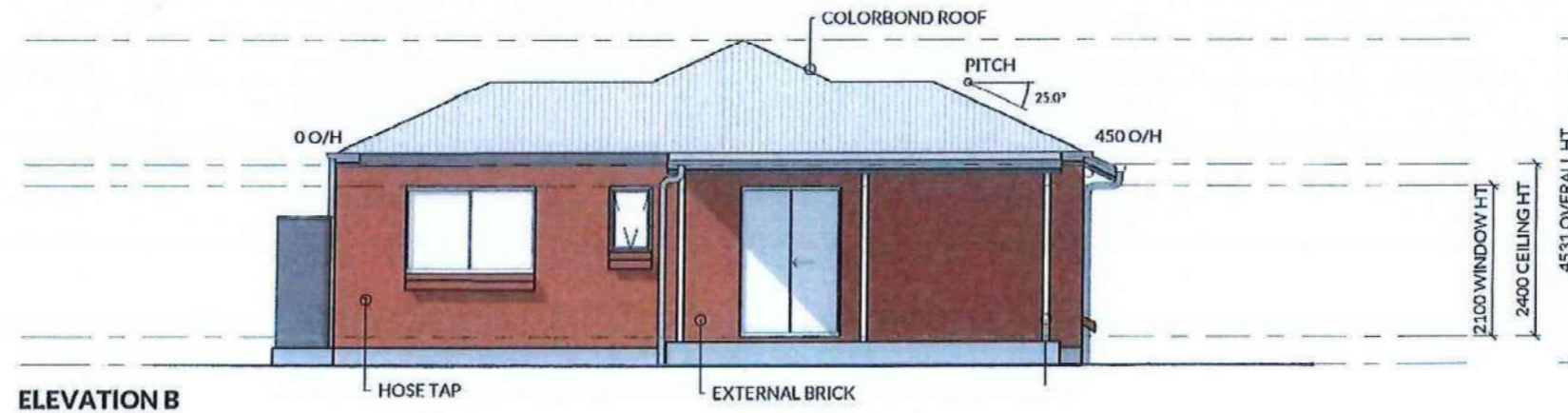
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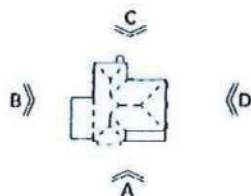
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NOTE:  
• ALL WINDOWS AND SLIDING GLASS DOORS DOUBLE GLAZED



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BUILDERS NUMBER:  
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AREI PLAN CODE:  
AREI DESIGNS

CLIENT:  
**ABODE DESIGNER HOMES**  
DRAWING NAME:  
**ELEVATIONS A & B**

PROJECT:  
**PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK ST, PERTH**

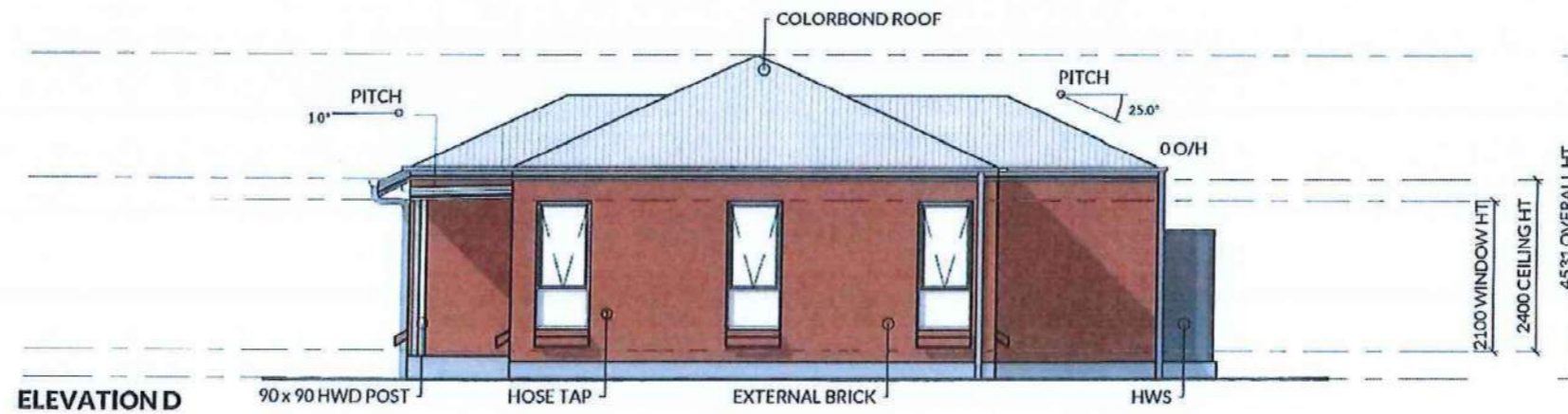
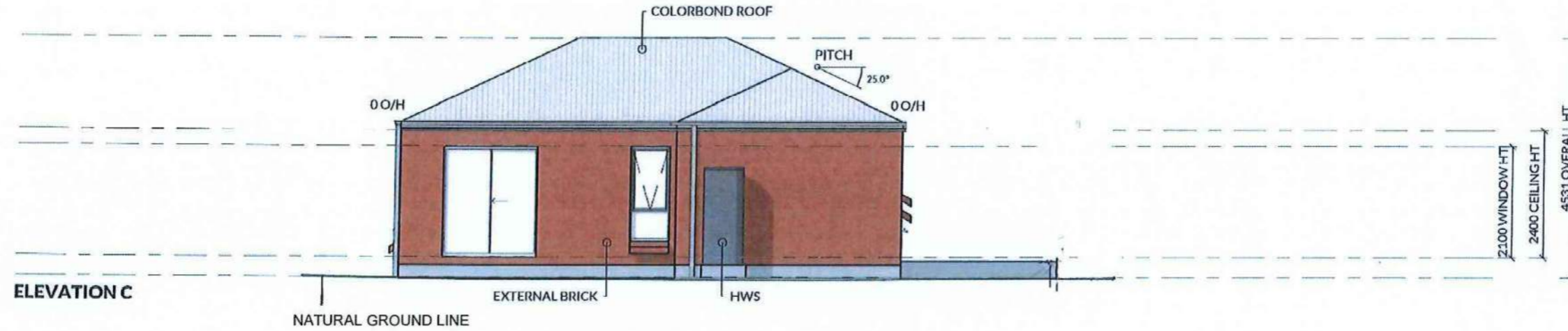
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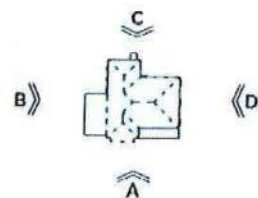
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### FLOOR AREAS

ROOM	SQUARES	SQM
CARPORT	1.78	16.5 m <sup>2</sup>
LIVING	9.21	85.6 m <sup>2</sup>
VERANDAH	0.81	7.5 m <sup>2</sup>
TOTAL	11.80	109.6 m <sup>2</sup>

### WINDOW SCHEDULE

MARK	SIZE	DESCRIPTION
W1	1800 x 750	ALUM. FRAMED AWNING
W2	1800 x 750	ALUM. FRAMED AWNING
W3	1800 x 750	ALUM. FRAMED AWNING
W4	1800 x 750	ALUM. FRAMED AWNING
W5	1800 x 750	ALUM. FRAMED AWNING
W6	2100 x 1500	ALUM. FRAMED SLIDING GLASS DOOR
W7	1200 x 1200	ALUM. FRAMED AWNING
W8	900 x 900	ALUM. FRAMED AWNING
W9	1800 x 750	ALUM. FRAMED AWNING
W10	2100 x 1800	ALUM. FRAMED SLIDING GLASS DOOR
W11	1800 x 750	ALUM. FRAMED AWNING
W12	1800 x 750	ALUM. FRAMED AWNING
W13	1800 x 750	ALUM. FRAMED AWNING

### DOOR SCHEDULE

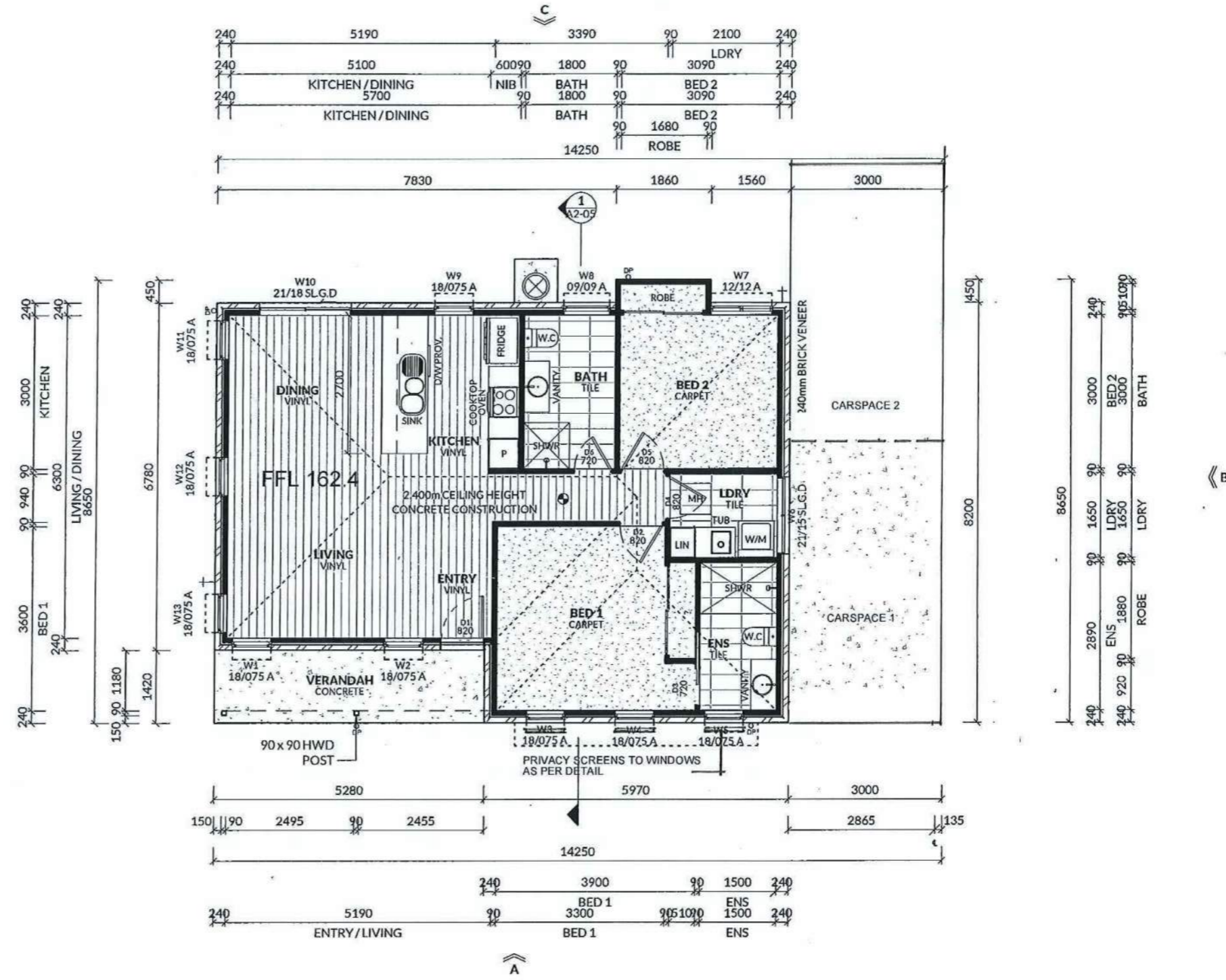
MARK	SIZE	DOOR TYPE
D1	2040 x 820	ENTRY DOOR - HUME XV14
D2	2040 x 820	HOLLOW CORE SWING
D3	2040 x 720	HOLLOW CORE CAVITY SLIDER
D4	2040 x 820	HOLLOW CORE SWING
D5	2040 x 820	HOLLOW CORE SWING
D6	2040 x 720	HOLLOW CORE SWING

### LEGEND

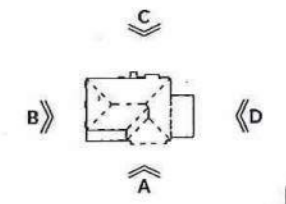
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- SL.G.D SLIDING GLASS DOOR
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- † HOSE COCK
- ⊞ ELEC M/BOX
- ⊕ GAS BOTTLES
- ⊗ HOT WATER
- ⊕ FLOOR WASTE

- #### ENERGY EFFICIENCY REQUIREMENTS
- R4.0 BATTS TO CEILING
  - R2.0 BATTS TO EXTERNAL WALLS AND GARAGE INTERNAL WALL
  - DOWNLIGHTS TO BE MINICRATED
  - ALL WINDOWS AND SLIDING GLASS DOORS DOUBLE GLAZED



## UNIT 4



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 TAS BSP: 071565667  
 ABN: 31 615 195 818

PLAN NUMBER:  
#2297  
 BUILDERS NUMBER:  
N/A  
 AREI PLAN CODE:  
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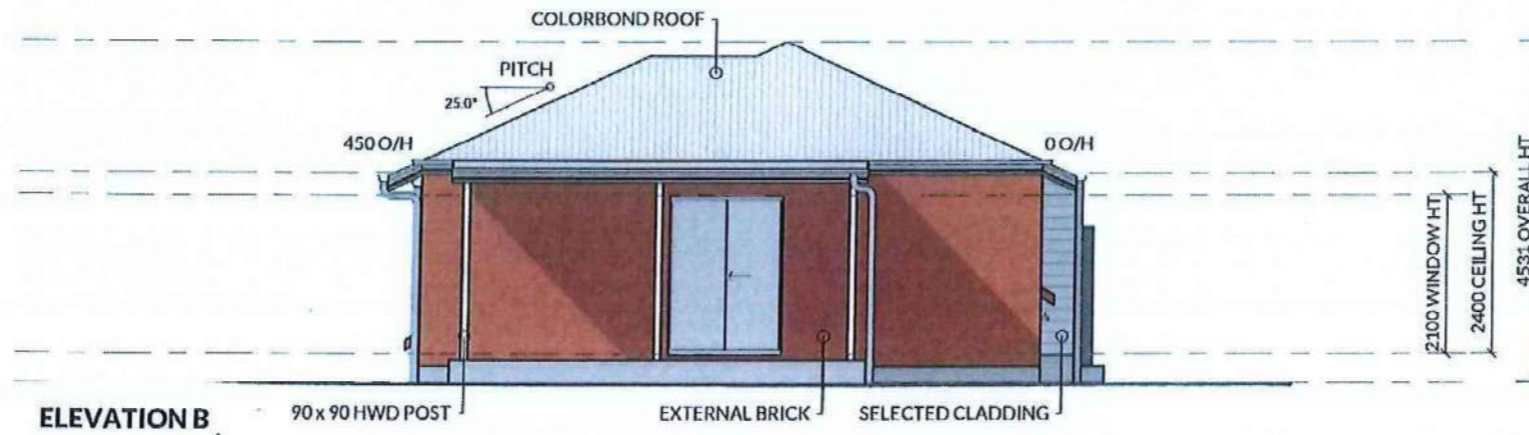
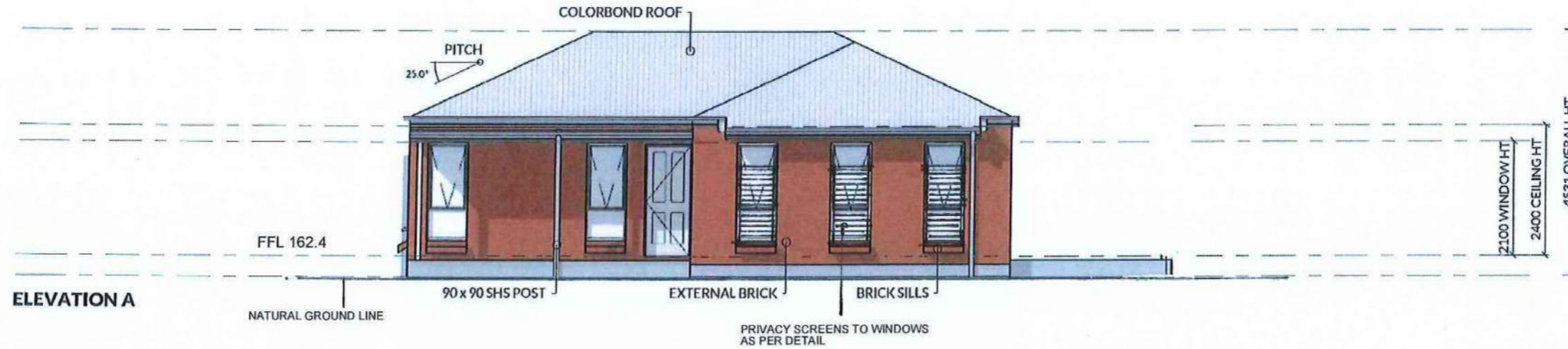
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 DRAWING NAME:  
**FLOOR PLAN**

PROJECT:  
**PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK ST, PERTH**

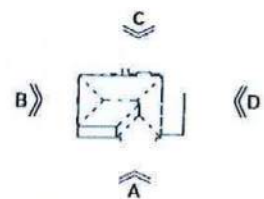
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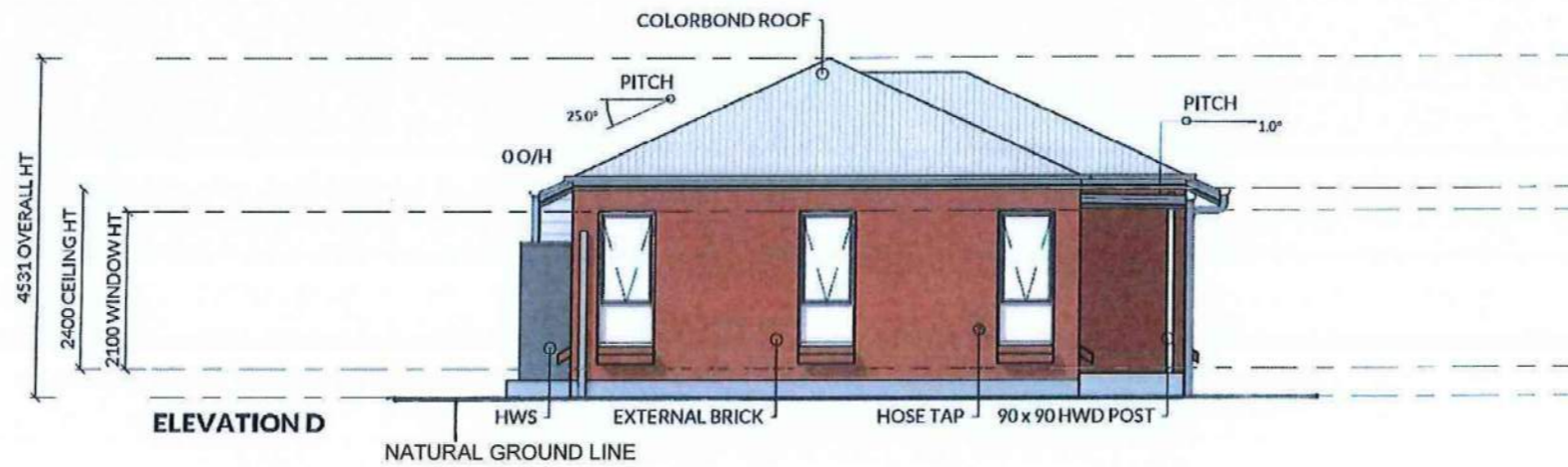
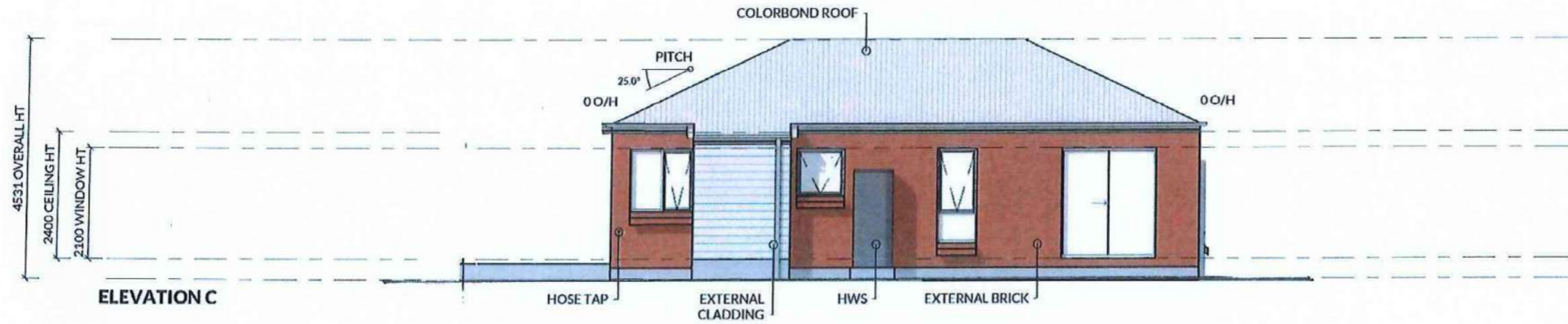
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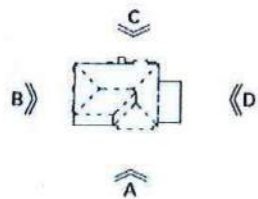
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 DRAWING NAME:  
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MARK	SIZE	DESCRIPTION
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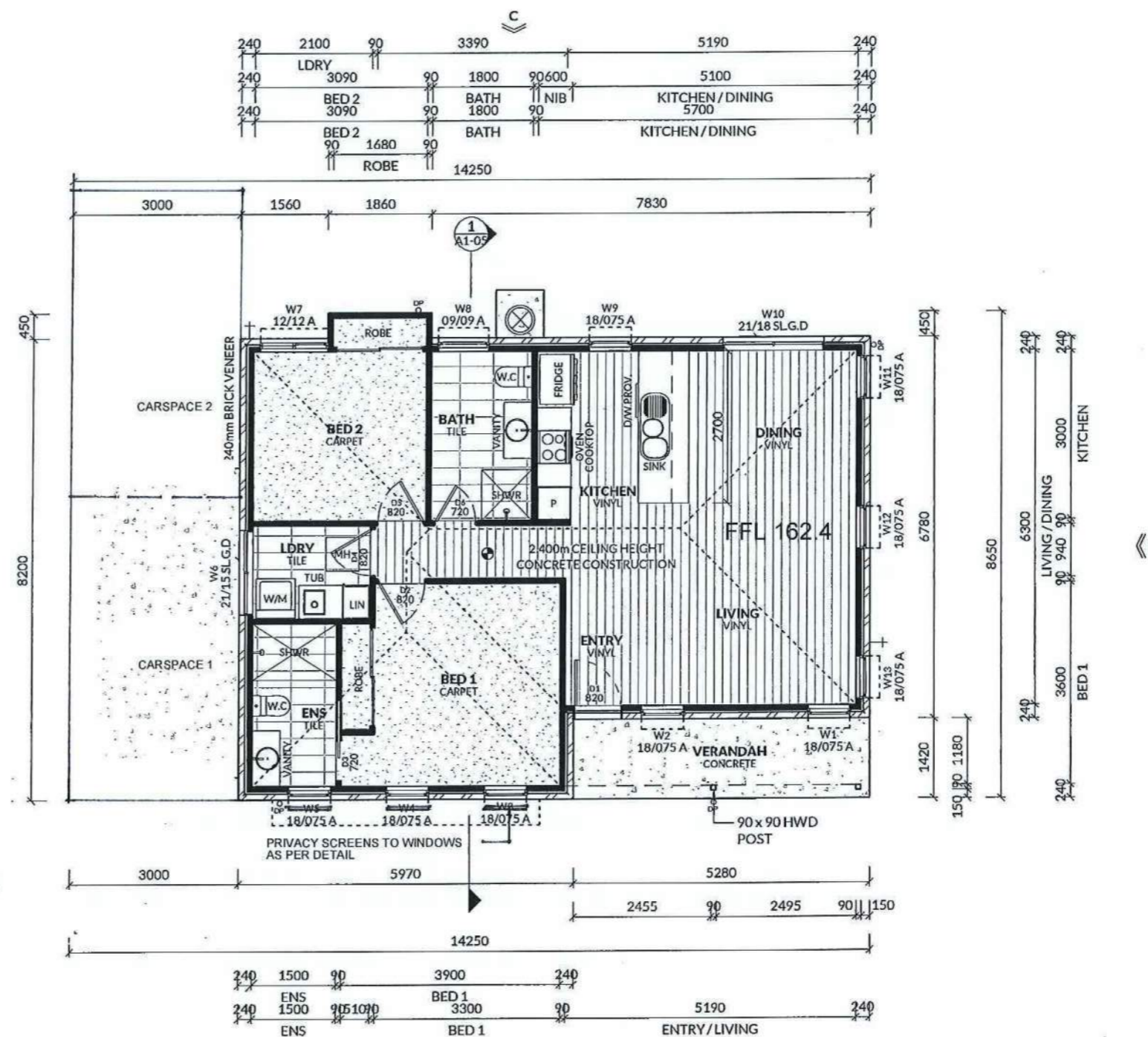
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- C CASEMENT WINDOW
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- S.W SLIDING WINDOW
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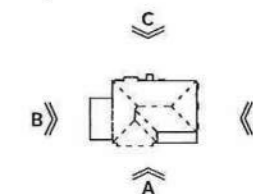
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- ⊗ HOT WATER
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## UNIT 5



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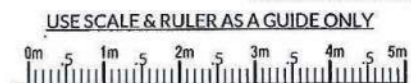
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 DRAWING NAME:  
**FLOOR PLAN**

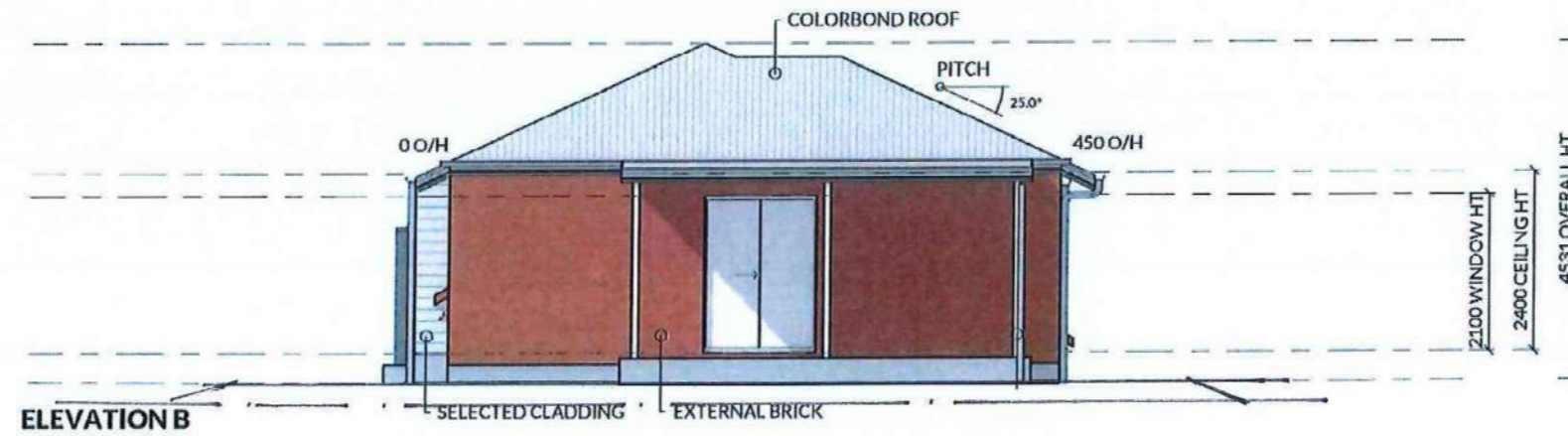
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**PROPOSED RESIDENCE FOR ABODE DESIGNER HOMES AT 23 NORFOLK ST, PERTH**

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 PAGE NO:  
A1-01 OF 41  
 SCALE @ A3  
1:100

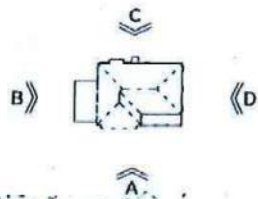
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D	CONSTRUCTION ISSUE	SM	15/08/25

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## UNIT 5



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 W: areidesigns.com.au  
 TAS: CC 7461  
 VIC VBA: CDB-U 73620  
 TAS BSP: 071565667  
 ABN: 31 615 195 818

PLAN NUMBER:  
 #2297  
 BUILDERS NUMBER:  
 N/A  
 AREI PLAN CODE:  
 AREI DESIGNS

CLIENT:  
**ABODE DESIGNER  
 HOMES**  
 DRAWING NAME:  
**ELEVATIONS A & B**

PROJECT:  
**PROPOSED RESIDENCE FOR  
 ABODE DESIGNER HOMES AT  
 23 NORFOLK ST, PERTH**

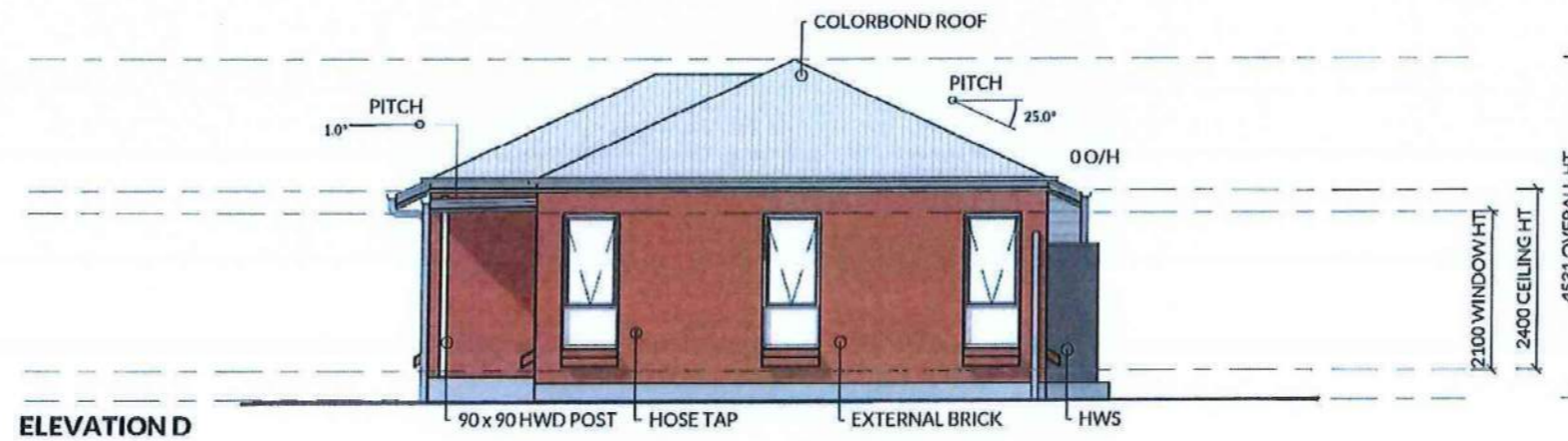
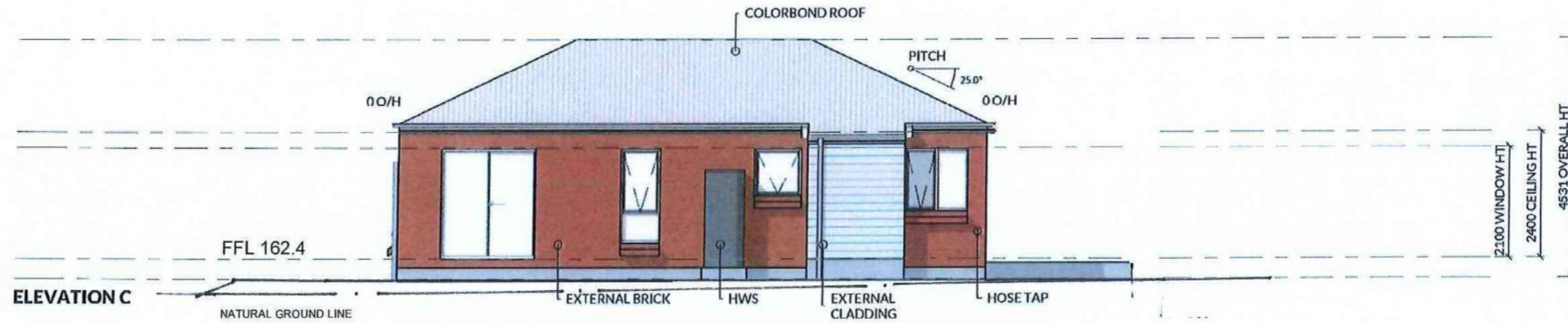
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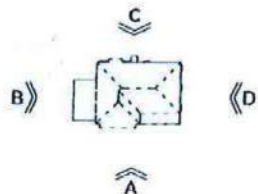
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PLAN NUMBER:  
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 BUILDERS NUMBER:  
 N/A  
 AREI PLAN CODE:  
 AREI DESIGNS

CLIENT:  
**ABODE DESIGNER HOMES**  
 DRAWING NAME:  
**ELEVATIONS C & D**

PROJECT:  
**PROPOSED RESIDENCE FOR  
 ABODE DESIGNER HOMES AT  
 23 NORFOLK ST, PERTH**

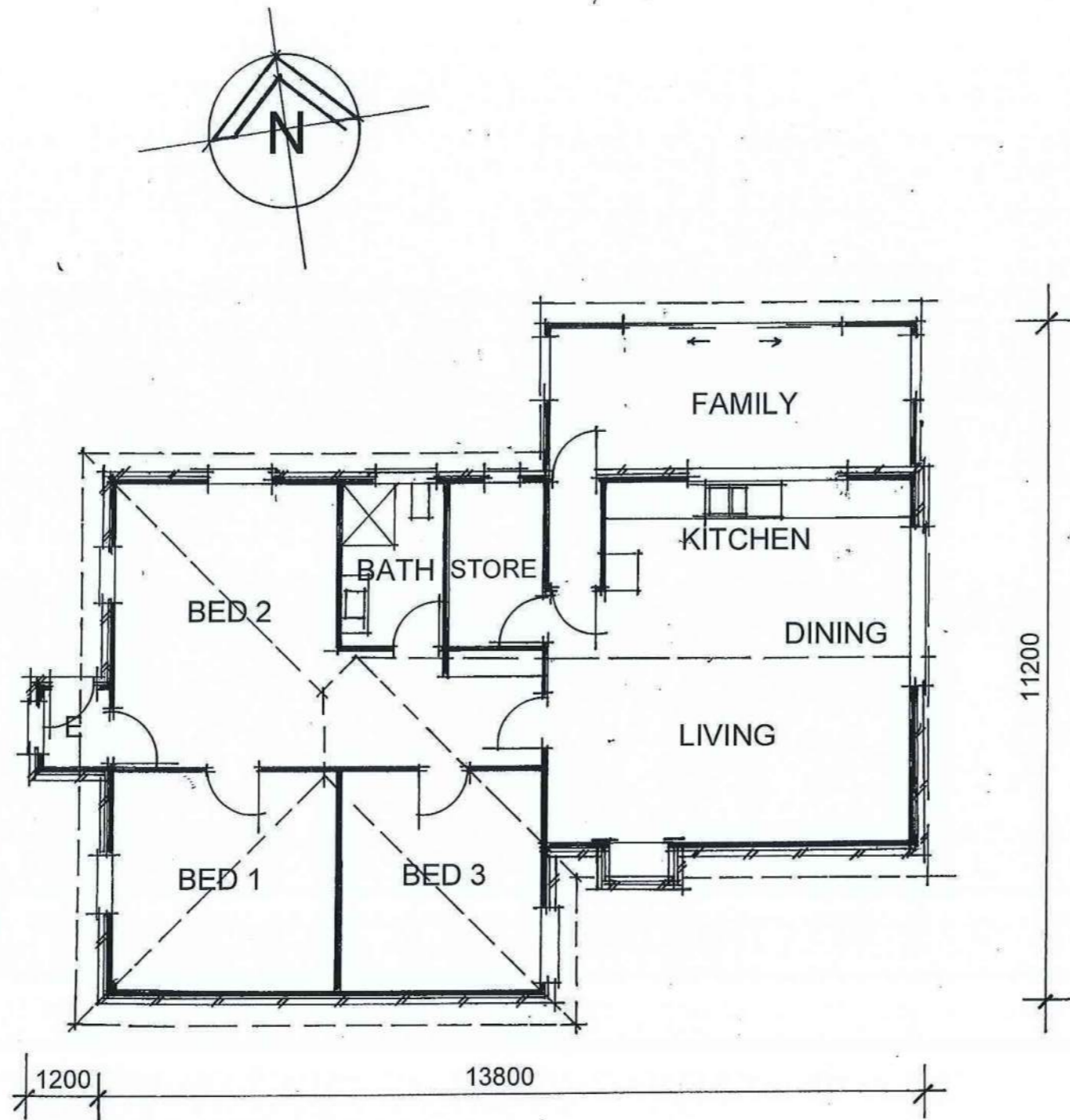
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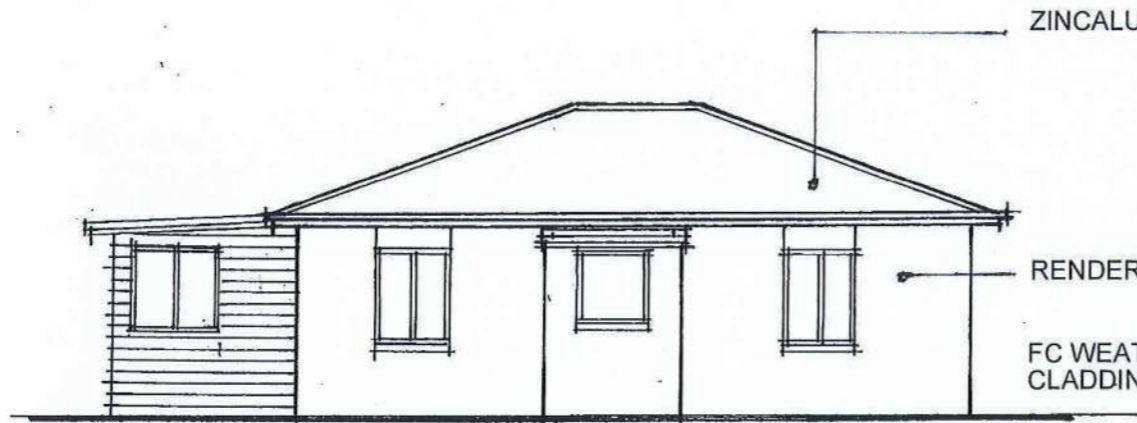
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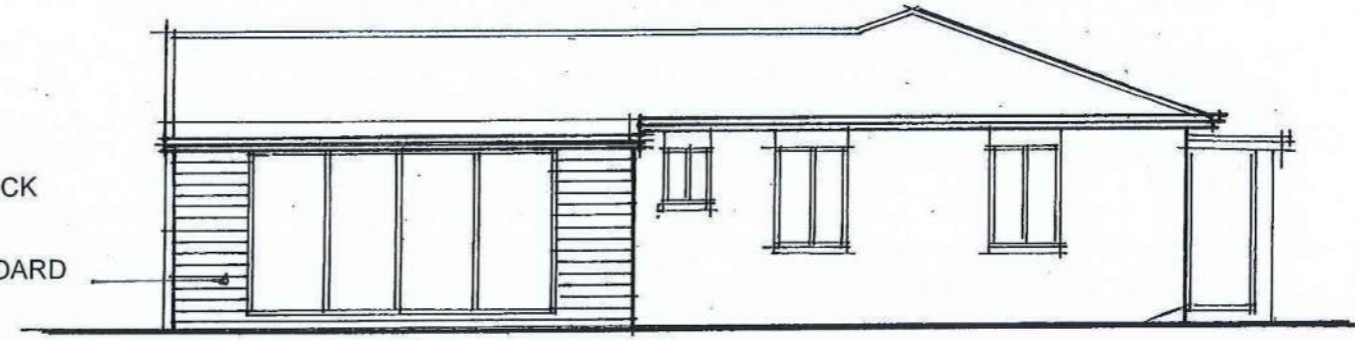


FLOOR PLAN 1:100  
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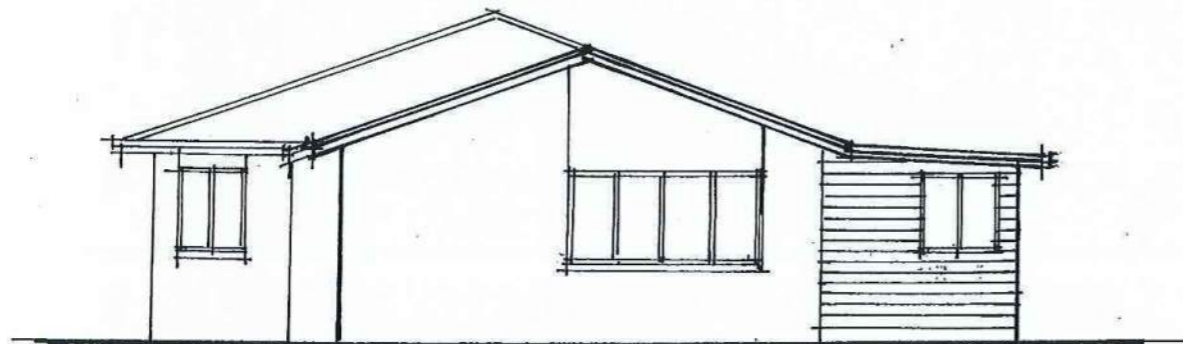
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B	PRELIMINARY ISSUE	SM	19/02/25
C	CONSTRUCTION ISSUE	SM	11/08/25
D	CONSTRUCTION ISSUE	SM	15/08/25



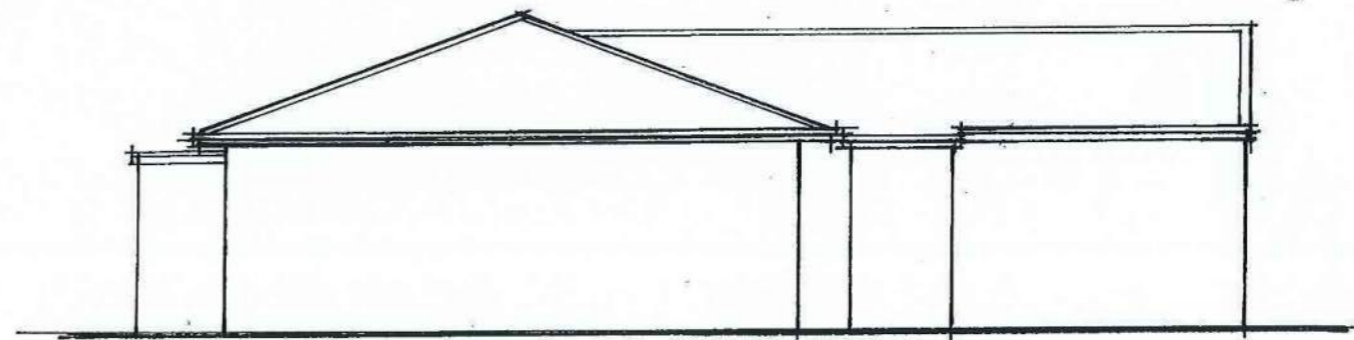
WEST ELEVATION



NORTH ELEVATION



EAST ELEVATION



SOUTH ELEVATION

EXISTING RESIDENCE UNIT 6



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 W: areidesigns.com.au  
 TAS: CC 7461  
 VIC VBA: CDB-U 73620  
 TAS BSP: 071565667  
 ABN: 31 615 195 818

PLAN NUMBER:  
 #2297  
 BUILDERS NUMBER:  
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 AREI PLAN CODE:  
 AREI DESIGNS

CLIENT:  
**ABODE DESIGNER  
 HOMES**

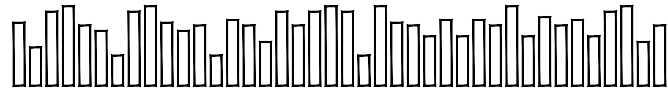
PROJECT:  
**PROPOSED RESIDENCE FOR  
 ABODE DESIGNER HOMES AT  
 23 NORFOLK ST, PERTH**

CHECKED:  
 ODW

SCALE @ A3  
 1:100

REV	DESCRIPTION	DRAWN	DATE
A	PRELIMINARY ISSUE	SM	24/08/24
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**ABODE DESIGNER HOMES  
23 NORFOLK STREET,  
PERTH**

CSE TASMANIA REF: 7606-50  
MARCH / 2026

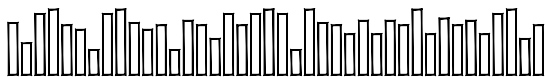
**DRAWING SCHEDULE**

DRAWING No.	DRAWING NAME	REVISIONS
7606-50_G01	COVER SHEET AND LOCALITY PLAN	Rev P0
7606-50_G02	GENERAL DETAILS PLAN	Rev P0
7606-50_G03	GENERAL NOTES PLAN	Rev P0
7606-50_G04	GENERAL ARRANGEMENT LAYOUT PLAN	Rev P0
7606-50_C01	DRIVEWAY 01 LAYOUT AND LONG SECTION PLAN	Rev P0
7606-50_C02	DRIVEWAY 01 CROSS SECTIONS PLAN SHEET 01	Rev P0
7606-50_C03	DRIVEWAY 01 CROSS SECTIONS PLAN SHEET 02	Rev P0
7606-50_C04	CONSTRUCTION DETAILS PLAN	Rev P0
7606-50_C05	STORMWATER LAYOUT PLAN	Rev P0
7606-50_C06	STORMWATER LONG SECTION PLAN	Rev P0
7606-50_C07	SEWER RETICULATION LAYOUT PLAN	Rev P0
7606-50_C08	SEWER RETICULATION LONG SECTION PLAN	Rev P0
7606-50_C09	WATER RETICULATION LAYOUT PLAN	Rev P0



**LOCALITY PLAN**

SCALE: 1:1500



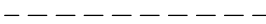

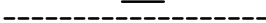

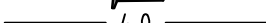
























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	Approved CHRIS MARTIN		
	Date MARCH 2026		

No	Revision	Drawn	Date

Client	<b>ABODE DESIGNER HOMES</b>
Project	<b>23 NORFOLK STREET, PERTH</b>
Title	<b>COVER SHEET AND LOCALITY PLAN</b>
Drawing No:	<b>7606-50_G01</b>
Revision:	<b>P0</b>

# TYPICAL LEGEND:

-  PROPOSED LOT BOUNDARIES
-  EXISTING LOT BOUNDARIES
-  PROPOSED EASEMENTS
-  PROPOSED BUILDING ENVELOPES
-  PROPOSED PAVEMENT SAW CUT EDGE
-  PROPOSED CONCRETE DRIVEWAY/FOOTPATHS
-  PROPOSED CONTOURS
-  EXISTING CONTOURS
-  EXISTING FENCE LINE
-  EXISTING GATE
-  EXISTING TOP OF BANK
-  EXISTING TOE OF BANK
-  PROPOSED STORMWATER DRAINAGE LINE
-  PROPOSED STORMWATER PIT
-  PROPOSED STORMWATER MANHOLE
-  EXISTING STORMWATER DRAINAGE LINE
-  EXISTING STORMWATER MANHOLE
-  PROPOSED WATER MAIN
-  PROPOSED WATER METER
-  PROPOSED WATER STOP VALVE
-  EXISTING WATER MAIN
-  PROPOSED SEWER MAIN
-  PROPOSED SEWER MANHOLE
-  EXISTING SEWER MAIN
-  EXISTING SEWER MANHOLE
-  EXISTING ELECTRICAL LINE
-  EXISTING ELECTRICAL POLE
-  EXISTING TREES
-  EXISTING TREES TO BE REMOVED

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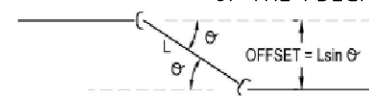
## STANDARD ROAD, DRAINAGE SEWER & WATER DOCUMENTS

- DOCUMENTS AS LISTED IN TASWATER SUPPLEMENTS TO
- WATER SUPPLY CODE OF AUSTRALIA (WSA 03-2011-3.1 MRWA V2.0 ISSUE NUMBER PUBLIC 05)
  - SEWERAGE SUPPLY CODE OF AUSTRALIA (WSA 02-2014-3.1 MRWA GRAVITY CODE OF AUSTRALIA (MRWA EDITION) VERSION 2.0)

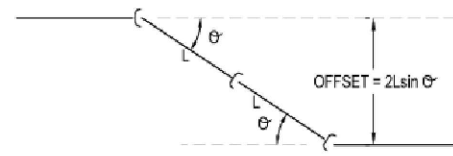
DOCUMENTS AS LISTED FOR ROAD AND STORMWATER DRAWINGS (TSD)

- TASMANIAN STANDARD DRAWINGS (TSD)-v3
- TASMANIAN SUBDIVISION GUIDELINES (2020)
- TASMANIAN MUNICIPAL STANDARD SPECIFICATIONS (MARCH 2020)

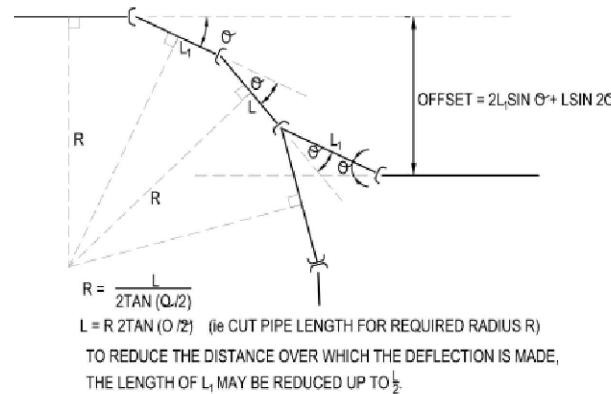
NOTE - FOLLOWING DEFLECTION VALUES MUST BE AMENDED FOR TASWATER TO 0.75% OF THE 1 DEGREE MANUFACTURERS DEFLECTION



**DETAIL A: 1 PIPE DEFLECTION OFFSET**



**DETAIL B: 2 PIPE DEFLECTION OFFSET**



**DETAIL C: 3 PIPE DEFLECTION OFFSET or CURVED MAIN**

**TABLE 3:  
DEFLECTIONS**

	PIPE / JOINT TYPE			COMMENTS
	TYPICAL RETIC PVC PIPE	TYPICAL RETIC DI PIPE	TYPICAL PVC PIPE + DOUBLE SOC CONNECTOR	
TYPICAL FULL LENGTH FOR L (m)	6	5.5	6 (PVC)	
TYPICAL MINIMUM LENGTH FOR L (m)	3	2.25	3 (PVC)	
TYPICAL $\theta$ MAX (degrees)	1	3.5	7	VARIES DEPENDING ON MANUFACTURER
MAX 1 PIPE MAX OFFSET (mm) <sup>1</sup>	100	340	730	HORIZONTAL OR VERTICAL DEFLECTION
MAX 2 PIPE MAX OFFSET (mm) <sup>1</sup>	210	670	146	2 or 3 PIPE HORIZONTAL DEFLECTION USUALLY NOT PREFERRED ALONG STRAIGHT ROADS DUE TO DISRUPTION OF OTHER ASSETS
MAX 3 PIPE MAX OFFSET (mm) <sup>1</sup>	420	1340	2910	
TYPICAL MIN R (m)	344	90	49	ASSUMING USE OF FULL PIPE LENGTHS
VERTICAL BLOCKING REQUIREMENTS	NO THRUST BLOCK REQUIRED	THRUST CALCULATION REQUIRED <sup>2</sup>	THRUST CONTROL REQUIRED <sup>3</sup>	VERTICAL BLOCKS REQUIRE WATER AGENCY APPROVAL <sup>5</sup>
HORIZONTAL BLOCKING REQUIREMENTS	NO THRUST BLOCK REQUIRED	THRUST CALCULATION REQUIRED <sup>2</sup>	THRUST BLOCK REQUIRED <sup>4</sup>	

**NOTES ON TABLE 3:**

- ALL FIGURES HAVE BEEN CALCULATED ASSUMING FULL PIPE LENGTHS
- 1 MAX OFFSETS CALCULATED USING FULL LENGTH PIPES.
  - 2 THRUST CONTROL REQUIREMENTS NEED TO BE CALCULATED AS PER THE METHOD DESCRIBED IN MRWA-W-204.
  - 3 BLOCK AS PER TABLE 1 OF MRWA-W-205A USING 1/2 OF THE MASS VOLUME OF THE 1.1.25° BEND.
  - 4 BLOCK AS PER 6 DEG BENDS OF MRWA-W-204.
  - 5 FLANGED OR WELDED BENDS PREFERRED TO VERTICAL BLOCKS.

TO REDUCE EXCAVATION DEPTHS AND / OR LIMIT DISRUPTION TO HORIZONTAL ALIGNMENTS, THE FOLLOWING ARRANGEMENTS ARE GENERALLY PREFERRED:

- FIGURES IN CIRCLES INDICATED THAT THIS OFFSET IS NORMALLY BETTER ACHIEVED USING 22 1/2° BENDS (FOR 300 TO 600 OFFSETS).
- ◇ FIGURES IN DIAMONDS INDICATE THAT THIS OFFSET IS NORMALLY BETTER ACHIEVED USING 45° BENDS (FOR > 600 OFFSETS).

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	Approved CHRIS MARTIN		
	Date MARCH 2026		

No	Revision	Drawn	Date

Client	<b>ABODE DESIGNER HOMES</b>
Project	<b>23 NORFOLK STREET, PERTH</b>
Title	<b>GENERAL DETAILS PLAN</b>
Drawing No:	<b>7606-50_G02</b>
Revision:	<b>P0</b>

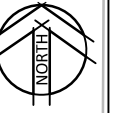


THERE MAY BE EXISTING SERVICES WITHIN THE WORKS AREA THAT ARE NOT SHOWN ON THESE PLANS. THE CONTRACTOR IS TO LOCATE ALL SERVICES PRIOR TO THE COMMENCEMENT OF WORKS.



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WESTERN LINE RAILWAY

NORFOLK STREET

PROPOSED 4.5m WIDE EASEMENT FOR STORMWATER, SEWER AND WATER

INSTALL NEW DRIVEWAY EDGE AND TRANSITION TO EXISTING ROAD EDGE OF BITUMEN AND MAKE GOOD

TYPICAL 4.4m WIDE CONCRETE DRIVEWAY TO BE INSTALLED IN ACCORDANCE WITH DRAWING TSD-R09-v3 FOR URBAN ROADS

1 2115.4m<sup>2</sup> DRIVEWAY 01

UNIT 1

UNIT 2

UNIT 3

UNIT 5

UNIT 4



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Drawn

CJG

Accred. No.

CC4109V

Approved

CHRIS MARTIN

Date

MARCH 2026

No Revision

Drawn

Date

Client **ABODE DESIGNER HOMES**

Project **23 NORFOLK STREET, PERTH**

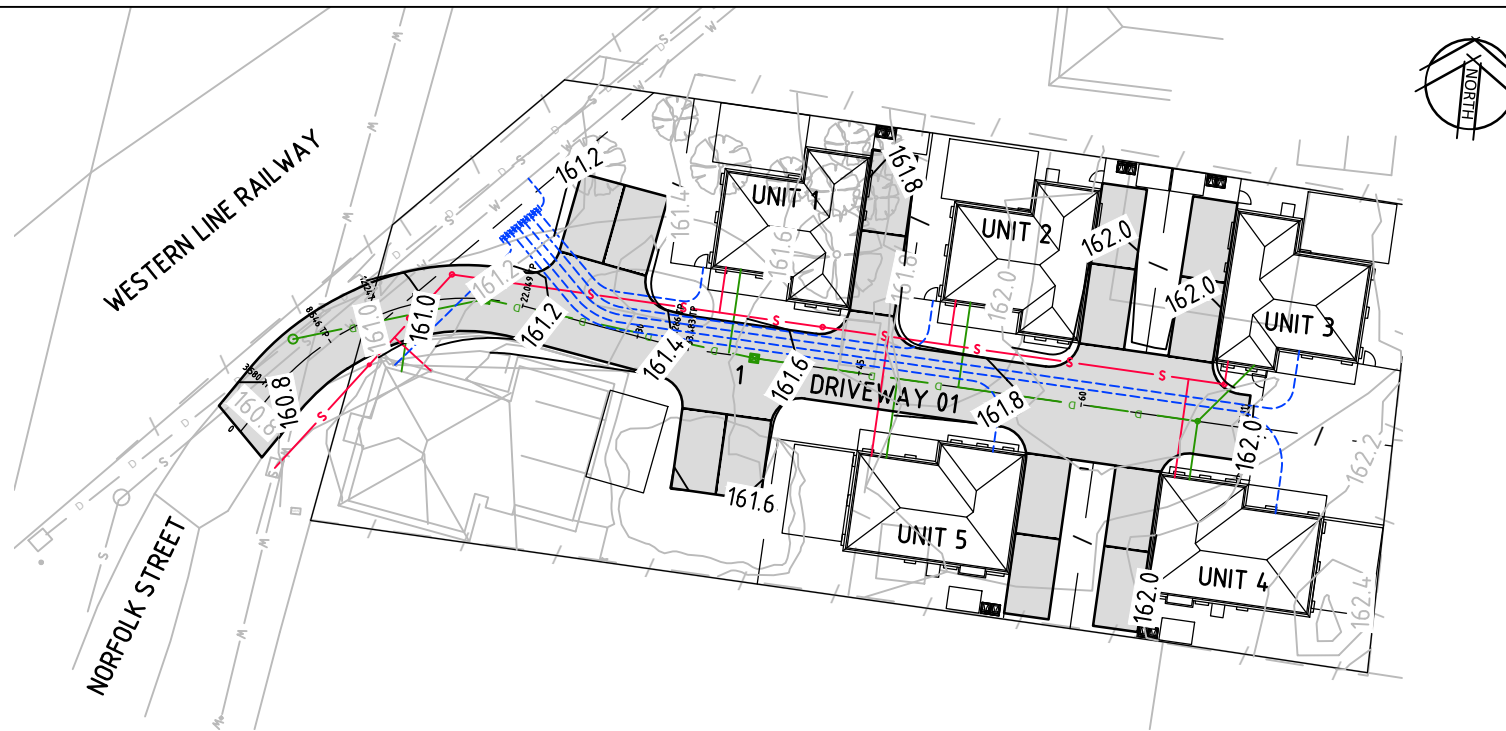
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Drawing No: **7606-50\_G04**

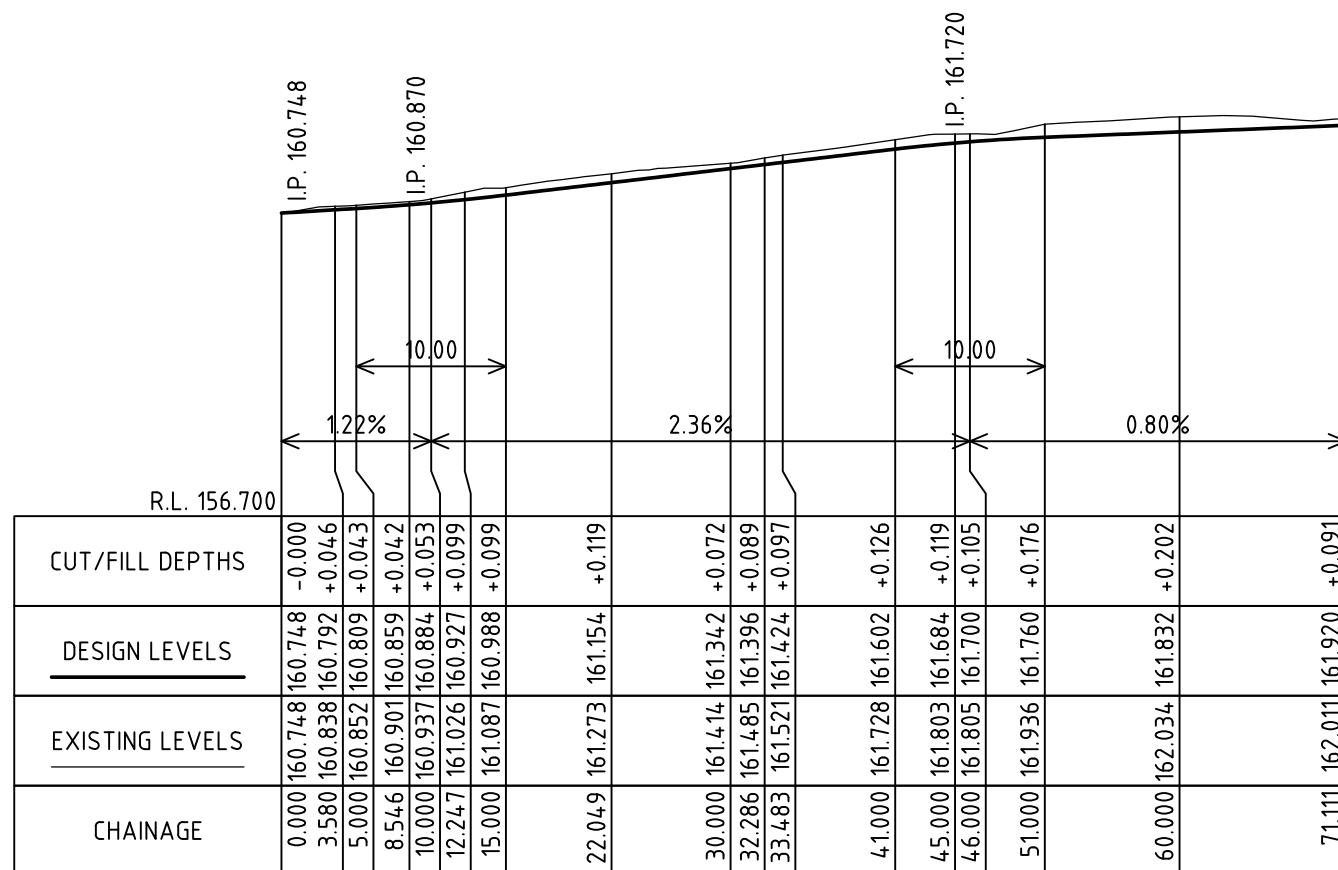
Revision: **P0**

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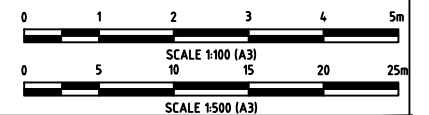
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LAYOUT PLAN  
SCALE 1:500



LONGITUDINAL SECTION DRIVEWAY 01 CH 0.000 TO 72.754  
SCALES: HORIZ 1:500 VERT 1:100



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Approved	CHRIS MARTIN		
Date	MARCH 2026		

No	Revision	Drawn	Date

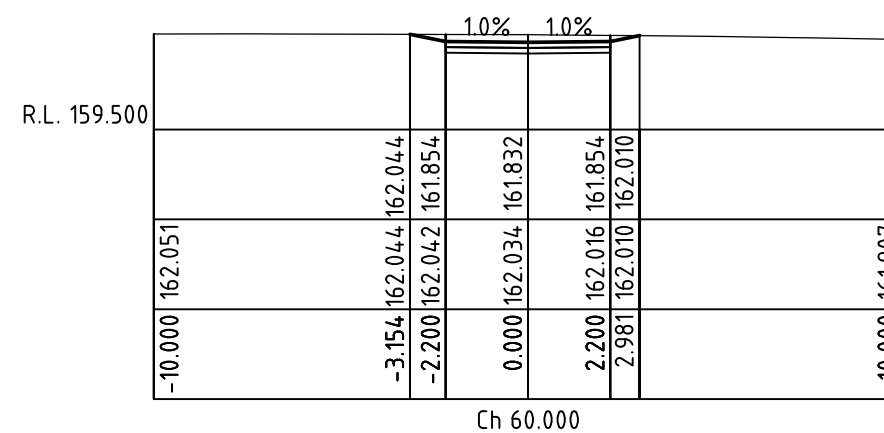
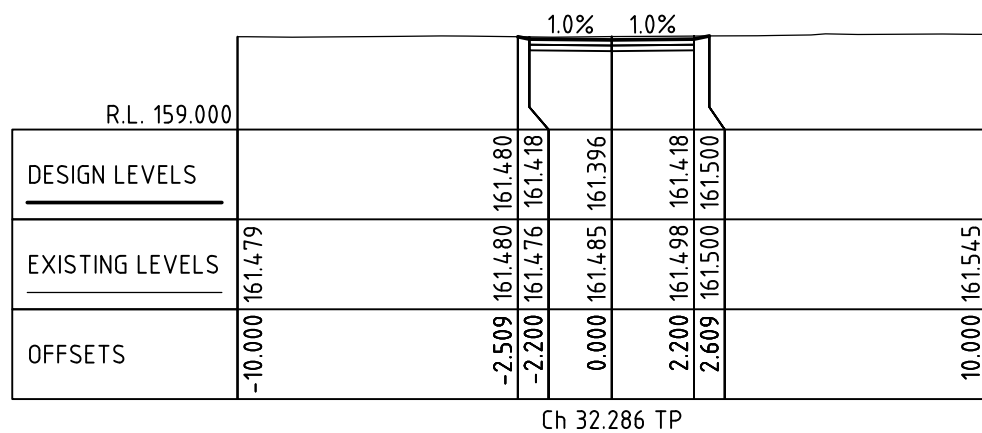
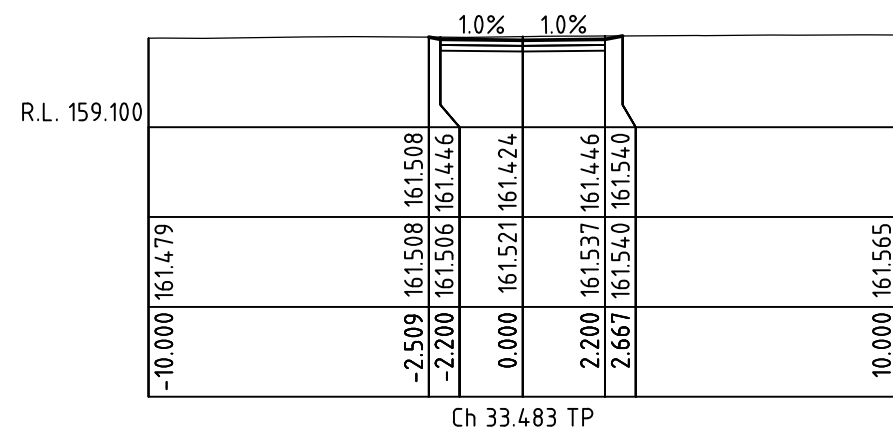
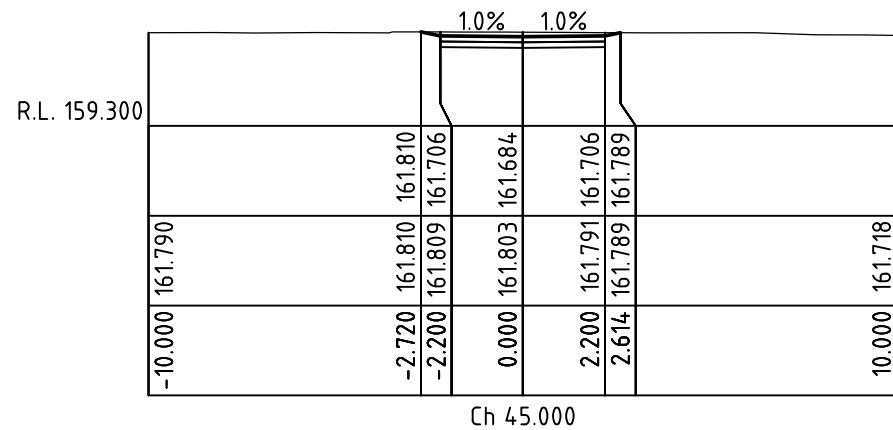
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Project	<b>23 NORFOLK STREET, PERTH</b>
Title	<b>DRIVEWAY 01 LAYOUT AND LONG SECTION PLAN</b>
Drawing No:	<b>7606-50_C01</b>
Revision:	<b>P0</b>



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**DRIVEWAY 01 CROSS SECTIONS**  
SCALE 1:200H 1:200V



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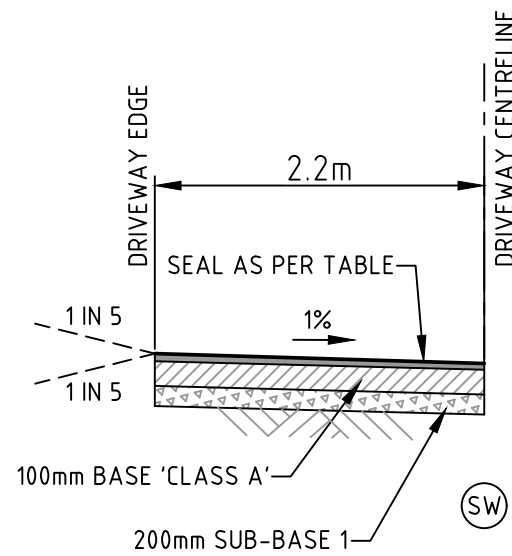
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Scale	1:200H 1:200V	Designed	CHRIS MARTIN
Drawn	CJG	Accred. No.	CC4109V
Approved	CHRIS MARTIN		
Date	MARCH 2026		

No	Revision	Drawn	Date

Client	ABODE DESIGNER HOMES
Project	23 NORFOLK STREET, PERTH
Title	DRIVEWAY 01 CROSS SECTIONS PLAN SHEET 02
Drawing No:	7606-50_C03
Revision:	P0

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**TYPICAL HALF WIDTH  
DRIVEWAY 01 CROSS SECTION**

SCALE: 1:50

**NOTES:**

**PAVEMENT**

1. SUBGRADE - PASS PROOF ROLL AND WHERE MOVEMENT EXISTS UNDERTAKE CBR TESTING TO VERIFY ADDITIONAL PAVEMENT DEPTH.
2. DESIGN PAVEMENT DEPTHS BASED ON CBR 4 - FINAL PAVEMENT DEPTHS TO BE DEEPENED IN ACCORDANCE WITH THE ON SITE CBR TESTING DETAILS AND TABLE AS DIRECTED BY THE SUPERINTENDENT.
3. ALL BATTERS TO BE CONSTRUCTED IN ACCORDANCE WITH THE B.C.A. - TABLE 3.1.1.1

**CBR TESTING NOTES:**

CBR TESTING TO BE UNDERTAKEN WHEN BOX OUT TO 340mm BELOW FSL IS COMPLETED.

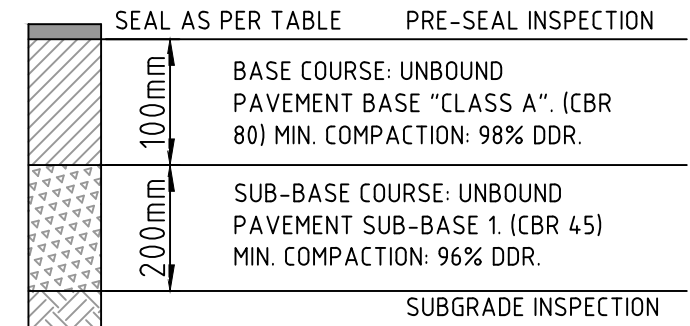
ALL IN-SITU CBR READINGS ARE TO BE ADJUSTED IN ACCORDANCE WITH THE TABLE R23.2 OF DEPARTMENT OF STATE GROWTH SPECIFICATION R23 (REPRODUCED RIGHT) WHERE SUBGRADE IS IN A DRY CONDITION TO ACCOUNT FOR STRENGTH DECREASE WHEN SATURATED.

CBR TESTS TO BE PERFORMED EVERY 200m<sup>2</sup> OF PAVEMENT AREA BY THE DESIGN ENGINEER. SOFT AREAS MAY BE PLOTTED ON SITE FOR ADDITIONAL EXCAVATION.

PROVIDE MINIMUM 24 HOURS NOTICE TO THE ENGINEER PRIOR TO STRIPPING THE SITE TO SUB-GRADE LEVEL TO FACILITATE CBR TESTING.

**PENETROMETER CBR  
ADJUSTMENT**

SOURCE: STATE GROWTH SPECIFICATION R23 - TABLE R23.2	
IN-SITU CBR FROM PENETROMETER (CORRELATION FROM CONE RESISTANCE)	ADJUSTED CBR VALUE
2	1
3	2
4	3
5	4
6	4
7	5
8	6



NATURAL SUBGRADE MIN. COMPACTION: 95% DDR FOR FILL PLACED TO WITHIN 300mm OF LEVEL AND 97% DDR FOR THE TOP 300mm BELOW THE Finished SUBGRADE LEVEL.

**TYPICAL PAVEMENT DETAIL**

SCALE: N.T.S.

**SEALED PAVEMENT CONSTRUCTION DETAILS**

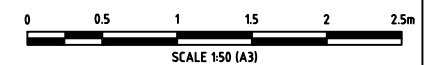
BASED ON CBR4 & DESA OF 4x10<sup>4</sup>  
TOTAL DEPTH - 300mm MIN  
WEARING COURSE - 40mm DG10 AC

**STABILISATION GRASS MIX FOR  
SURFACE DRAINS & BATTERS:**

STERILE RYECORN:	3.0 g/m <sup>2</sup>
RYEGRASS VAR VICTORIAN:	1.5 g/m <sup>2</sup>
RYECORN VAR CONCORDE:	1.5 g/m <sup>2</sup>
CHEWING FESCUE:	0.5 g/m <sup>2</sup>
HARD FESCUE:	0.5 g/m <sup>2</sup>
SHEEP FESCUE:	0.5 g/m <sup>2</sup>
WHITE CLOVER VAR HUIA:	1.0 g/m <sup>2</sup>
SUB CLOVER VAR TRIKKALA:	0.5 g/m <sup>2</sup>
BROWNTOP BENT:	0.5 g/m <sup>2</sup>
RED CREEPING FESCUE:	0.5 g/m <sup>2</sup>
TOTAL:	10.0 g/m <sup>2</sup>

**FERTILISER**

N : P : K = 8 : 4 : 10 @ 50.0g/m<sup>2</sup>



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Scale	AS NOTED	Designed	CHRIS MARTIN
Drawn	CJG	Accred. No.	CC4109V
Approved	CHRIS MARTIN		
Date	MARCH 2026		

**PRELIMINARY**

Client **ABODE DESIGNER HOMES**  
Project **23 NORFOLK STREET, PERTH**  
Title **CONSTRUCTION DETAILS PLAN**

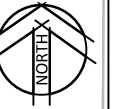
Drawing No: **7606-50\_C04**

Revision: **P0**

# Exhibited

# Received

1.4.2026



### NOTES FOR DRAINAGE WORKS

ALL WORKS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE RELEVANT SECTIONS OF THE PLUMBING AND DRAINAGE CODE, WITH PARTICULAR REFERENCE TO THE SPECIFIC CLAUSES BELOW.

THE CIVIL CONTRACTOR SHALL ENGAGE A PLUMBER FAMILIAR WITH THE PLUMBING CODES TO OVERSEE AND SIGN OFF THE WORKS.

### FOR INTERNAL DRAINAGE (SECTION 7.4 OF AS 3500.3:2018)

- INSPECTION OPENINGS SHALL BE EXTENDED TO AND CAPPED AT THE FINISHED SURFACE LEVEL INSTALLED AT:
  - EACH POINT OF CONNECTION;
  - EVEN SPACINGS NOT MORE THAN 30m APART;
  - EACH END OF ANY INCLINED JUMP-UP THAT EXCEEDS 6m IN LENGTH;
  - EACH CONNECTION TO AN EXISTING SITE STORMWATER DRAIN AND;
  - AT ANY CHANGE OF DIRECTION GREATER THAN 45 DEGREES.

NOTE - THE COUNCIL PLUMBING INSPECTOR MAY PERMIT 10'S TO REMAIN BURIED.

INSPECTION OPENINGS MAY BE REPLACED BY AN INLET OR PIT

SIZE SHALL BE DN150 FOR PIPES 150 DIA AND LESS AND NOT LESS THAN DN 150 FOR LARGER PIPES

### SECTION 7.6 - JUNCTIONS SHALL BE MADE BY MEANS OF

- AN OBLIQUE JUNCTION OR SWEEP JUNCTION AT AN UPSTREAM ANGLE OF NOT GREATER THAN 60 DEGREES AND PREFERABLY LESS THAN 45 DEGREES, OR;
- AN OPENING COMPLIANT WITH FIG 7.7.113 FOR PIPES GREATER THAN 375mm DIA, OR;
- A PIT

SECTION 7.9 - WHERE THE GRADIENT OF A SITE DRAIN EXCEEDS 1 IN 5 ANCHOR BLOCKS SHALL BE INSTALLED AT THE BEND OR JUNCTION AT THE TOP OF THE INCLINED SITE DRAIN AND AT INTERVALS NOT EXCEEDING 3m. REFER TO CLAUSE FOR DETAILS OF ANCHOR DIMENSIONS.

PROPOSED 4.5m WIDE EASEMENT FOR STORMWATER, SEWER AND WATER

TYPICAL PROPOSED HOUSE  
Ø150 STUB TO BE INSTALLED

TYPICAL PROPOSED HOUSE  
Ø150 STUB TO BE INSTALLED

TYPICAL PROPOSED HOUSE  
Ø150 STUB TO BE INSTALLED

TYPICAL PROPOSED HOUSE  
Ø150 STUB TO BE INSTALLED

TYPICAL PROPOSED HOUSE  
Ø150 STUB TO BE INSTALLED

TYPICAL PROPOSED HOUSE  
Ø150 STUB TO BE INSTALLED

WESTERN LINE RAILWAY

NORFOLK STREET

DRIVEWAY 01

UNIT 1

UNIT 2

UNIT 3

UNIT 5

UNIT 4



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**A3**

Scale

1:300

Designed

CHRIS MARTIN

Drawn

CJG

Accred. No.

CC4109V

Approved

CHRIS MARTIN

Date

MARCH 2026

**PRELIMINARY**

No Revision

Drawn

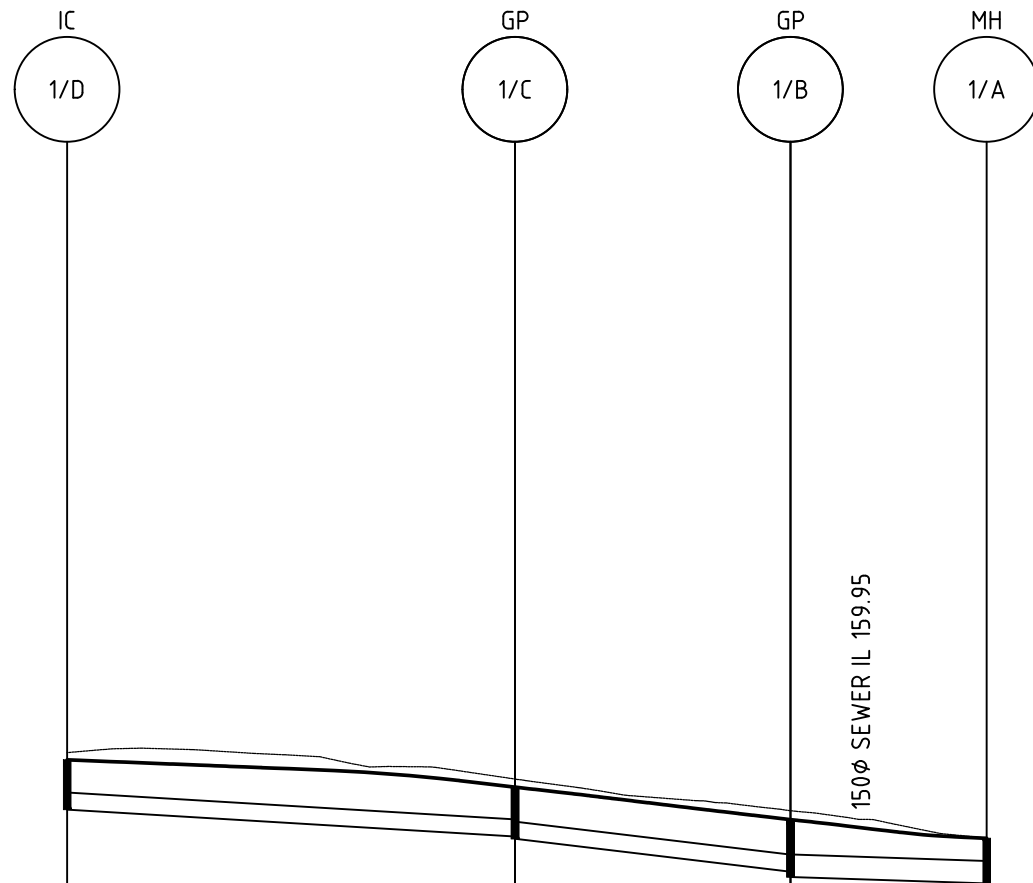
Date

Client **ABODE DESIGNER HOMES**  
Project **23 NORFOLK STREET, PERTH**  
Title **STORMWATER LAYOUT PLAN**

Drawing No: **7606-50\_C05**

Revision: **P0**

**Received**  
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**Exhibited**



PIPE DETAILS  
SLOPE/GRADE  
DATUM RL 156.2

EXISTING SURFACE	161.99	161.64	161.21	160.85
FINISHED SURFACE	161.89	161.53	161.09	160.85
INVERT LEVEL	161.23	160.87 160.84	160.40 160.33	160.24 160.21
LENGTH	29.92	18.41	13.11	

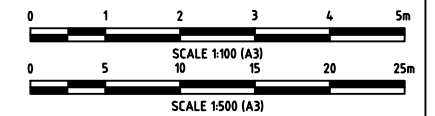
DRAINAGE LONGITUDINAL SECTION FOR LINE 1  
SCALES: HORIZONTAL 1:500 VERTICAL 1:100

**LEGEND:**

- EX SEP EXISTING SIDE ENTRY PIT
- SEP SIDE ENTRY PIT WITH 1.2m LINTEL
- ALL LINTELS ARE TO BE:
  - TYPE 1 FOR ALL PITS ON-GRADE
  - TYPE 3 FOR ALL SAG PITS UNO.
- LGE LINTEL TYPE 2 FOR ALL PITS ON GRADE
- LGE SEP TYPE 4 FOR ALL SAG PITS
- GP REFER TSD-SW30 (1.55x0.90 PIT)
- MH GRATED PIT TO TSD-SW15
- IC φ1050 SHAFT MANHOLE U.N.O.
- INSPECTION CHAMBER

**NOTES:**

1. ALL SEP's GREATER THAN 1500mm DEEP TO BE CONSTRUCTED TO THE LARGE SIDE ENTRY PIT DETAIL TSD-SW30.
2. ALL MANHOLES THAT DO NOT HAVE TRAFFIC LOADING TO HAVE LIGHT DUTY COVER.
3. ALL TRAFFICABLE MANHOLES TO HAVE CLASS D IC LIDS.
4. FCR BACKFILL UNDER ALL ROADWAYS.
5. ALL LOT CONNECTIONS TO BE 150φ UPVC.



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<b>PRELIMINARY</b>		Drawn CJG	Accred. No. CC4109V
		Approved CHRIS MARTIN	Date MARCH 2026

Client	<b>ABODE DESIGNER HOMES</b>
Project	<b>23 NORFOLK STREET, PERTH</b>
Title	<b>STORMWATER LONG SECTION PLAN</b>
Drawing No:	<b>7606-50_C06</b>
Revision:	<b>P0</b>

**NOTES FOR SEWER WORKS**  
 ALL WORKS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE RELEVANT SECTIONS OF THE PLUMBING AND DRAINAGE CODE, WITH PARTICULAR REFERENCE TO THE SPECIFIC CLAUSES BELOW.

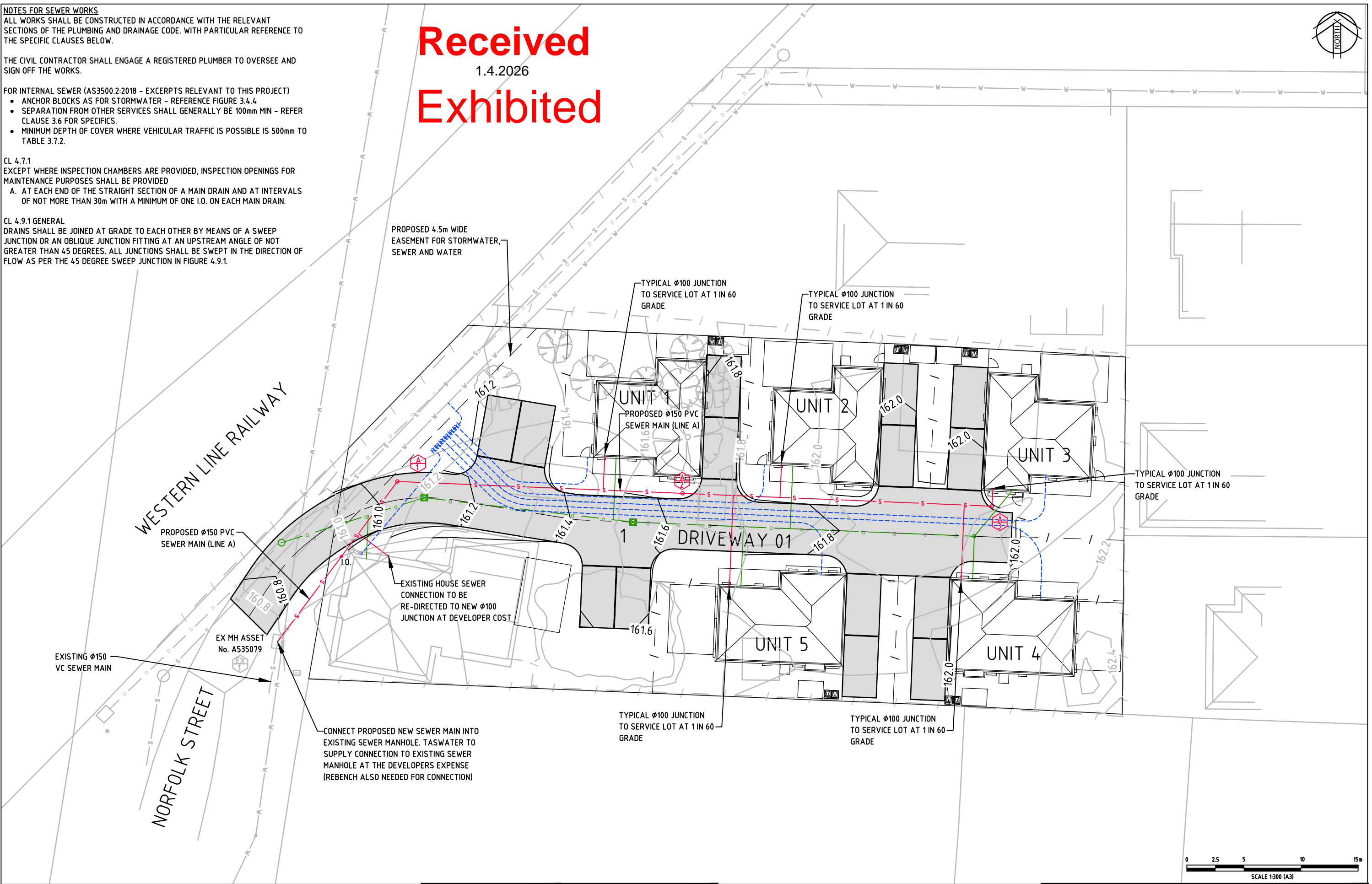
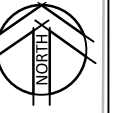
THE CIVIL CONTRACTOR SHALL ENGAGE A REGISTERED PLUMBER TO OVERSEE AND SIGN OFF THE WORKS.

FOR INTERNAL SEWER (AS3500.2:2018 - EXCERPTS RELEVANT TO THIS PROJECT)  
 • ANCHOR BLOCKS AS FOR STORMWATER - REFERENCE FIGURE 3.4.4  
 • SEPARATION FROM OTHER SERVICES SHALL GENERALLY BE 100mm MIN - REFER CLAUSE 3.6 FOR SPECIFICS.  
 • MINIMUM DEPTH OF COVER WHERE VEHICULAR TRAFFIC IS POSSIBLE IS 500mm TO TABLE 3.7.2.

CL 4.7.1  
 EXCEPT WHERE INSPECTION CHAMBERS ARE PROVIDED, INSPECTION OPENINGS FOR MAINTENANCE PURPOSES SHALL BE PROVIDED  
 A. AT EACH END OF THE STRAIGHT SECTION OF A MAIN DRAIN AND AT INTERVALS OF NOT MORE THAN 30m WITH A MINIMUM OF ONE I.O. ON EACH MAIN DRAIN.

CL 4.9.1 GENERAL  
 DRAINS SHALL BE JOINED AT GRADE TO EACH OTHER BY MEANS OF A SWEEP JUNCTION OR AN OBLIQUE JUNCTION FITTING AT AN UPSTREAM ANGLE OF NOT GREATER THAN 45 DEGREES. ALL JUNCTIONS SHALL BE SWEEPED IN THE DIRECTION OF FLOW AS PER THE 45 DEGREE SWEEP JUNCTION IN FIGURE 4.9.1.

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<b>PRELIMINARY</b>	Drawn CJG	Accred. No. CC4109V	
	Approved CHRIS MARTIN		
	Date MARCH 2026		

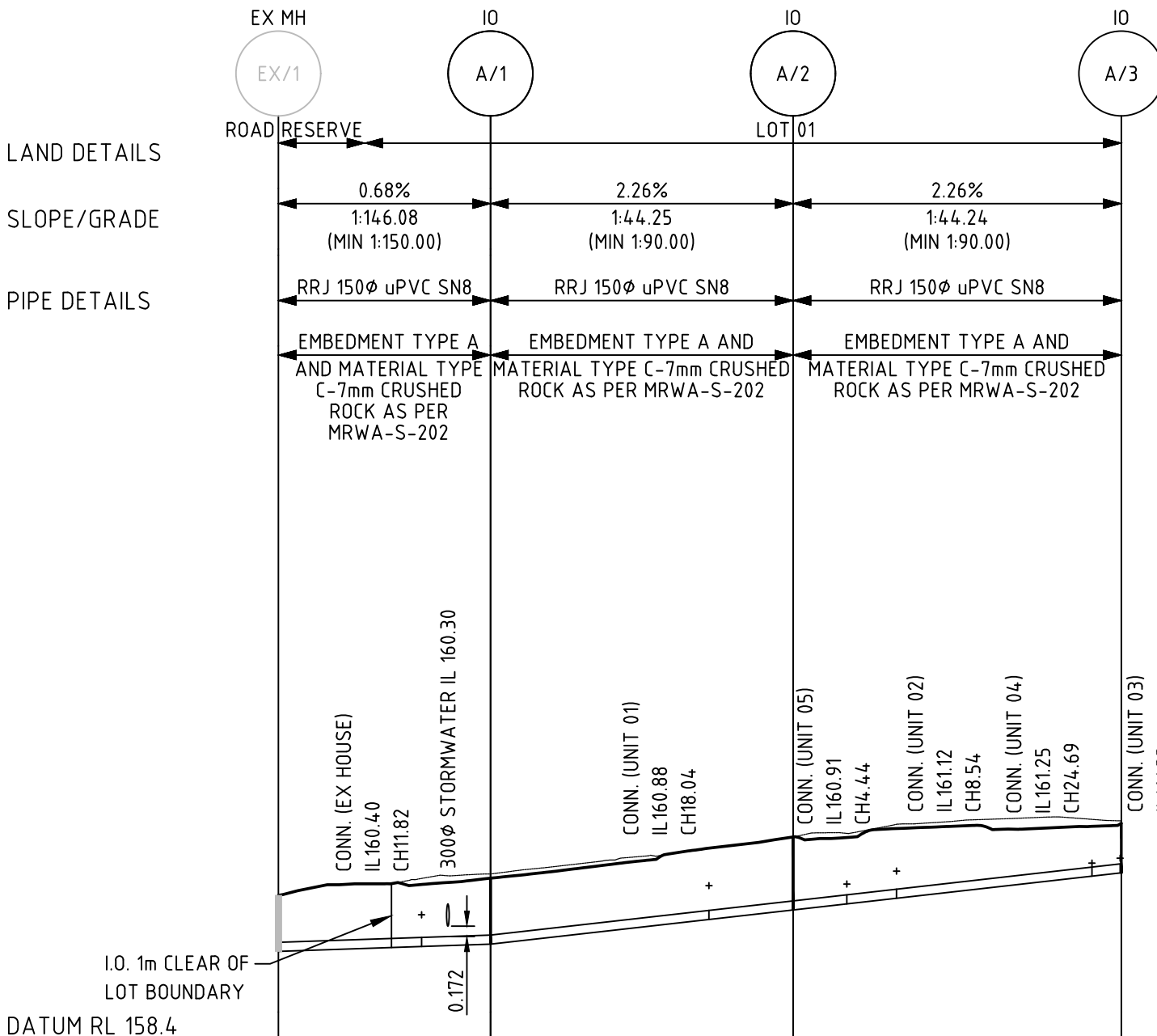
No	Revision	Drawn	Date

Client	<b>ABODE DESIGNER HOMES</b>
Project	<b>23 NORFOLK STREET, PERTH</b>
Title	<b>SEWER RETICULATION LAYOUT PLAN</b>
Drawing No:	<b>7606-50_C07</b>
Revision:	<b>P0</b>

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NATURAL SURFACE	160.78	161.13	161.74	162.01
DESIGN SURFACE	160.78	161.06	161.74	161.97
INVERT LEVEL	159.82 159.85	159.97	160.54	161.15
DEPTH TO INVERT	0.96 0.93	1.09	1.21	0.82
LENGTH/CHAINAGE	CH0.00 17.53	CH17.53 25.00	CH42.53 27.12	CH69.65

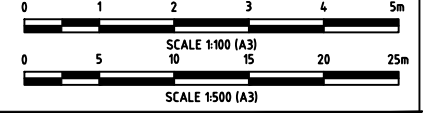
SEWER LONGITUDINAL SECTION FOR LINE A  
 SCALES: HORIZONTAL 1:500 VERTICAL 1:100

**LEGEND:**

- MH 1050φ MANHOLE. REFER MRWA-S-300
- MS 300φ MAINTENANCE SHAFT. REFER MRWA-S-300

**NOTES:**

- ALL SEWER MANHOLES 1050 φ MASTIC JOINTED SHAFT TYPE P2 WITH CONVERSION SLAB TO MRWA-S-309 AND MRWA-S-313. FINISH 75mm ABOVE GROUND FSL UNO
- ALL INSPECTION SHAFTS THAT DO NOT HAVE TRAFFIC LOADING TO HAVE LIGHT DUTY COVER AS PER MRWA-S-305-C.
- ALL TRAFFICABLE INSPECTION SHAFTS TO HAVE CLASS D CILIDS AS PER MRWA-S-305-C.
- ALL PROPERTY SEWER CONNECTIONS TO BE TYPE 4 AS PER MRWA-S-304 WITH I.O. NOMINALLY 1.2m WITHIN EACH LOT UNO.
- FCR BACKFILL UNDER ALL TRAFFICABLE PAVEMENT INCLUDING DRIVEWAY CROSS OVERS AND ROADWAYS.
- ALL SEWERAGE PIPES SCJ UNO.
- STEP IRONS ARE NOT REQUIRED ON MANHOLES
- ALL INSPECTION SHAFTS TO BE TYPE 1 IN ACCORDANCE WITH MRWA-S-302 UNO.
- ALL HOUSE CONNECTIONS TO HAVE A MINIMUM COVER OF 450mm AND HAVE A MINIMUM GRADE OF 1 IN 60.
- CONCRETE BULKHEADS SHALL BE KEYED INTO THE SIDE AND BOTTOM OF THE TRENCH AGAINST BEARING SURFACE OF UNDISTURBED SOIL.
- SLOPING MAINS AND TRENCH DRAINAGE AS PER STANDARD DRAWING MRWA-S-205.
- MANHOLES ARE TO BE CAST IN SITU OR OTHER APPROVED EQUIVALENT WHERE WATER TABLES ABOVE THE PIPE INVERT ARE ENCOUNTERED.
- ALL LONG SECTION GRADES ARE CALCULATED FROM CENTRE OF MANHOLE TO CENTRE OF MANHOLE. CONTRACTOR SHOULD CALCULATE GRADES FROM OUTSIDE OF MANHOLE FOR ACCURATE LAYING GRADES.



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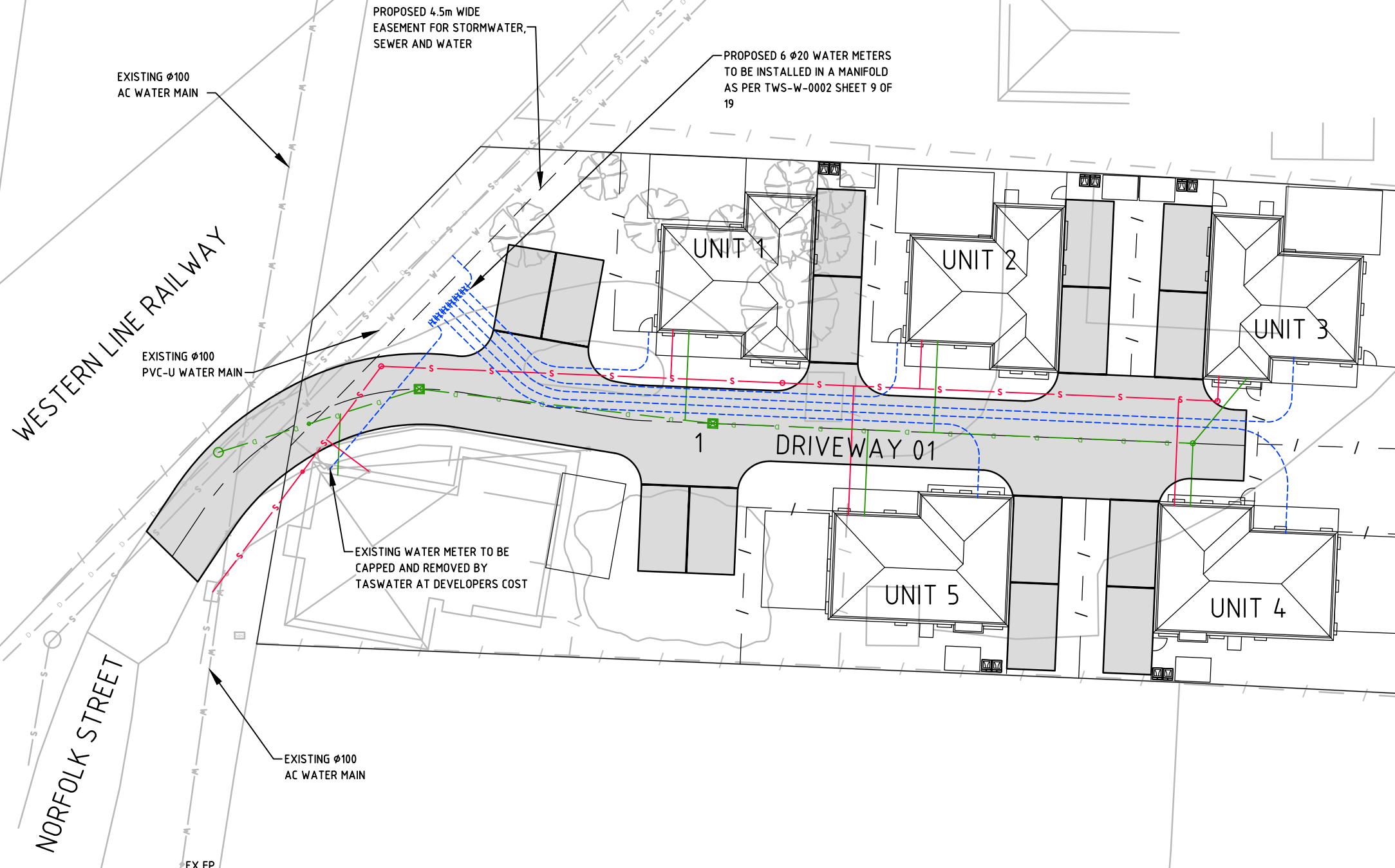
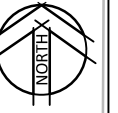
**DO NOT SCALE** Original Size **A3**  
 Scale 1:500H 1:100V  
 Designed CHRIS MARTIN  
 Drawn C.J.G.  
 Accred. No. CC4109V  
 Approved CHRIS MARTIN  
 Date MARCH 2026

**PRELIMINARY**

No	Revision	Drawn	Date
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Client **ABODE DESIGNER HOMES**  
 Project **23 NORFOLK STREET, PERTH**  
 Title **SEWER RETICULATION LONG SECTION PLAN**  
 Drawing No: **7606-50\_C08** Revision: **P0**

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<b>PRELIMINARY</b>		Drawn CJG	Accred. No. CC4109V
		Approved CHRIS MARTIN	
		Date MARCH 2026	

No	Revision	Drawn	Date

Client	<b>ABODE DESIGNER HOMES</b>
Project	<b>23 NORFOLK STREET, PERTH</b>
Title	<b>WATER RETICULATION LAYOUT PLAN</b>
Drawing No:	<b>7606-50_C09</b>
Revision:	<b>P0</b>