

Exhibited

This planning application is open for
public comment until
27 July 2026

Reference no	PLN-25-0212
Site	270 PERTH MILL ROAD PERTH
Proposed Development	Single Dwelling and outbuilding
Zone	10.0 Low Density Residential
Use class	Residential

Written representations may be made during this time to the General Manager;
mailed to PO Box 156, Longford, Tasmania 7301,
delivered to Council offices or
a pdf letter emailed to planning@nmc.tas.gov.au

(no special form required)



PLANNING APPLICATION

FOR BUILDINGS, WORKS AND CHANGE OF USE
(E.g. Residential houses, sheds, carports, retaining walls, visitor accommodation, commercial development, signage etc.)

Exhibited

The Proposal

Description of proposal:

The proposal is to construct a new detached dwelling for residential use by the owner. The house is a modern two-storey design (ground floor plus an upper floor) with a conventional pitched roof form. It will be of timber frame construction with durable lightweight cladding (e.g. weatherboard or similar) and a Colorbond corrugated steel roof in muted tones to blend with the rural setting. The dwelling will include an attached garage for two vehicles on the lower level (providing covered parking and storage) and a raised deck on the north side to take advantage of views and sunlight. Key features of the dwelling include open-plan living areas, 3-4 bedrooms, and compliant amenities. The finished external materials and colours will be non-reflective and in neutral earthy shades, consistent with the local character.

Driveway construction material:	Sealed concrete crossover, part sealed gravel, part concrete
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The Land

Site address:	270 PERTH MILL ROAD PERTH, TAS
Title reference:	C/T: 187993/2
Existing buildings on site:	NO
Existing use of site:	RESIDENTIAL VACANT LAND

Applicant justification of any variation/discretion to the Tasmanian Planning Scheme – Northern Midlands

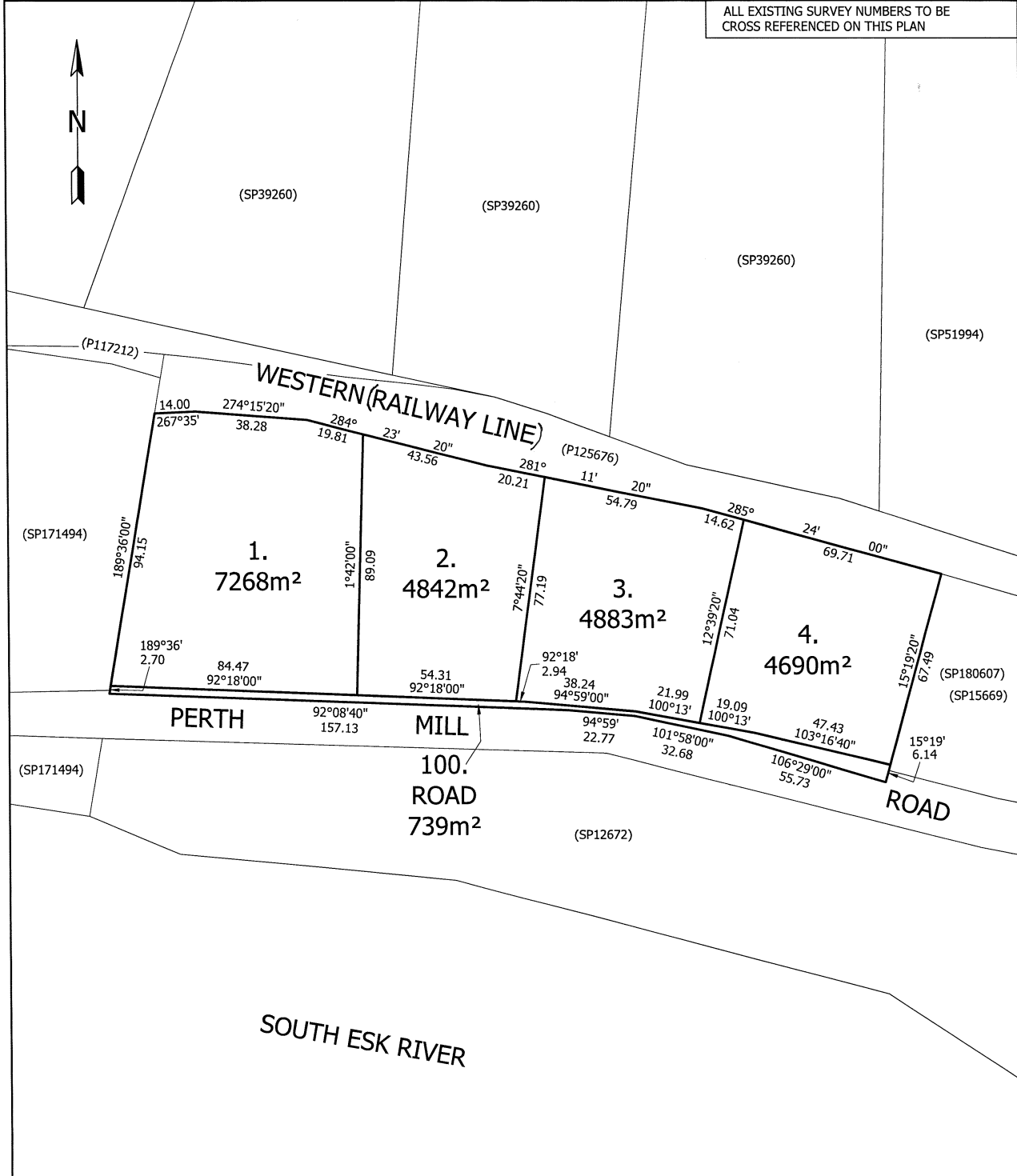
REFER TO COVER LETTER:

241002 - DA - COVER LETTER_260112

Priority Final Plan

OWNER: MELISSA JANE BIFFIN TRENT ROBIN BIFFIN FOLIO REFERENCE: C.T.15829/1 GRANTEE: PART OF 1321A-0R-0P GRANTED TO ROBERT CAMPBELL	PLAN OF SURVEY WOOLCOTT SURVEYS BY SURVEYOR: BRETT RICHARD WOOLCOTT LOCATION: LAND DISTRICT OF CORNWALL PARISH OF PERTH SCALE 1: 1250 LENGTHS IN METRES	REGISTERED NUMBER 5P187993
		APPROVED EFFECTIVE FROM 16 DEC 2024 Recorder of Titles

ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN



2/8/24
 Registered Land Surveyor Date

25/11/24
 Council Delegate Date

Received

19/01/2026

Exhibited



BIFFIN RESIDENCE

270 PERTH MILL RD, PERTH, TAS

ARCHITECTURAL

A000	COVER PAGE
A001	SITE PLAN
A120	GROUND FLOOR PLAN
A121	LEVEL 1 FLOOR PLAN
A210	ELEVATIONS(1 of 2)
A211	ELEVATIONS(2 of 2)

PROJECT TITLE	- BIFFIN RESIDENCE – DEVELOPMENT APPROVAL DRAWINGS
CLIENT / OWNER	- MELISSA BIFFIN & TRENT BIFFIN
LOCAL GOVERNMENT AUTHORITY	- NORTHERN MIDLANDS COUNCIL
SITE ADDRESS	- 270 PERTH MILL ROAD, PERTH, TASMANIA
LOT & PLAN	- LOT 2 – APPROVED SUBDIVISION (NEW TITLE TBC)
TITLE	- CT 187993/2
PARENT TITLE (REFERENCE)	- CT 15829/1 (PARENT LOT)
SITE AREA	- 4,700.33 M ² (APPROX. 0.47 HA)
PLANNING SCHEME ZONE	- LOW DENSITY RESIDENTIAL ZONE
APPLICABLE CODES / OVERLAYS	- BUSHFIRE-PRONE AREAS CODE; ROAD & RAILWAY ASSETS CODE (ATTENUATION AREA); PARKING & SUSTAINABLE TRANSPORT CODE; STORMWATER MANAGEMENT CODE
RAILWAY / ROAD BUFFERS	- 50 M TASRAIL BUFFER; ADDITIONAL NO-BUILD/SETBACK NOTES PER SITE PLAN
EXISTING CONDITIONS	- VACANT APPROVED RESIDENTIAL LOT; FRONTAGE TO PERTH MILL ROAD; RAIL CORRIDOR TO NORTH
PROPOSED DEVELOPMENT STOREYS	- NEW DETACHED DWELLING WITH ATTACHED GARAGE AND DECKS (FAMILY RESIDENCE)
EST. MAX BUILDING HEIGHT	- TWO STOREY (GROUND FLOOR + LEVEL 1)
EXTERNAL MATERIALS	- < 8.5 M (COMPLIES WITH LDR HEIGHT STANDARD)
ACCESS & PARKING	- TIMBER FRAME; TIMBER CLADDING AND/OR COLORBOND ZINCALUME CLADDING; COLORBOND CUSTOM ORB ROOF; ALUMINIUM FRAME DOUBLE-GLAZED WINDOWS; COMPOSITE DECKING
STORMWATER	- NEW SEALED CROSSOVER TO PERTH MILL ROAD; ON-SITE PARKING (GARAGE + DRIVEWAY)
WATER/POWER/COMMS	- ROOF WATER TO TANKS WITH CONTROLLED OVERFLOW; DISCHARGE TO ON-SITE/ROADSIDE DRAINAGE AS SPECIFIED
ONSITE WASTEWATER	- CONNECTIONS AVAILABLE AT ROAD FRONTAGE
BUSHFIRE SUMMARY	- SEPTIC + EVAPOTRANSPIRATION/ABSORPTION FIELD (RESERVE AREA PROVIDED); DESIGNED TO AS/NZS 1547
CONSULTANTS(REPORTS)	- BHMP IN PLACE; TARGET BAL-12.5; 10,000 L STATIC WATER SUPPLY; DEFENDABLE SPACE / HAZARD MANAGEMENT AREAS TO BE MAINTAINED
NOTES	- RAILWAY NOISE & VIBRATION – PITT & SHERRY; TRAFFIC IMPACT – MIDSON TRAFFIC; BUSHFIRE PRACTITIONER – (ACCREDITED)
	- THIS FACT SHEET IS A SUMMARY ONLY. REFER TO THE DRAWING SET AND SUPPORTING REPORTS FOR FULL DETAILS.



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GENERAL NOTES

PROJECT NAME

BIFFIN RESIDENCE

DRAWING TITLE

COVER PAGE

PROJECT ADDRESS

270 PERTH MILL RD, PERTH

CLIENT

MEL & TRENT BIFFIN

REV	DESCRIPTION	DATE
001	ISSUED FOR DEVELOPMENT APPROVAL	12/01/2026

CONTACT
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mia@zuricreativestudio.com
DRAWN BY
MSW
APPROVED BY
MSW
12/01/2026

ISSUE

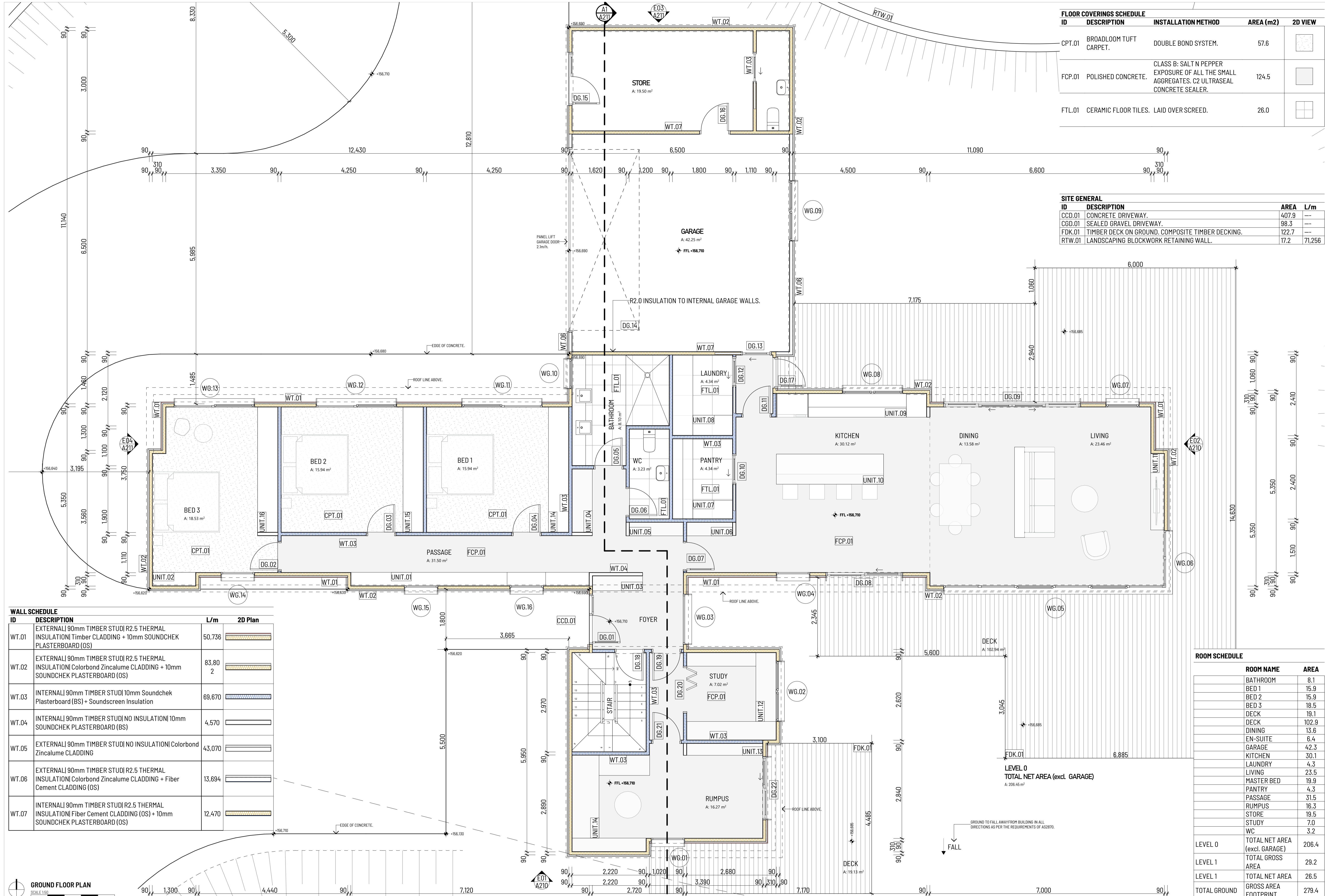
APPROVAL

DRAWING NO

A000 DA01

241002

A1L

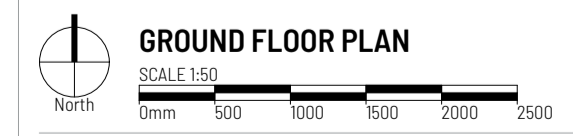


FLOOR COVERINGS SCHEDULE				
ID	DESCRIPTION	INSTALLATION METHOD	AREA (m ²)	2D VIEW
CPT.01	BROADLOOM TUFT CARPET.	DOUBLE BOND SYSTEM.	57.6	
FCP.01	POLISHED CONCRETE.	CLASS B: SALT N PEPPER EXPOSURE OF ALL THE SMALL AGGREGATES. C2 ULTRASEAL CONCRETE SEALER.	124.5	
FTL.01	CERAMIC FLOOR TILES. LAID OVER SCREED.		26.0	

SITE GENERAL			
ID	DESCRIPTION	AREA	L/m
CCD.01	CONCRETE DRIVEWAY.	407.9	---
CGD.01	SEALED GRAVEL DRIVEWAY.	98.3	---
FDK.01	TIMBER DECK ON GROUND. COMPOSITE TIMBER DECKING.	122.7	---
RTW.01	LANDSCAPING BLOCKWORK RETAINING WALL.	17.2	71.256

WALL SCHEDULE			
ID	DESCRIPTION	L/m	2D Plan
WT.01	EXTERNAL 90mm TIMBER STUDI R2.5 THERMAL INSULATION Timber CLADDING + 10mm SOUNDCEK PLASTERBOARD (OS)	50,736	
WT.02	EXTERNAL 90mm TIMBER STUDI R2.5 THERMAL INSULATION Colorbond Zincalume CLADDING + 10mm SOUNDCEK PLASTERBOARD (OS)	83,80	
WT.03	INTERNAL 90mm TIMBER STUDI 10mm SoundceK Plasterboard (BS) + Soundscreen Insulation	69,670	
WT.04	INTERNAL 90mm TIMBER STUDI NO INSULATION 10mm SOUNDCEK PLASTERBOARD (BS)	4,570	
WT.05	EXTERNAL 90mm TIMBER STUDI NO INSULATION Colorbond Zincalume CLADDING	43,070	
WT.06	EXTERNAL 90mm TIMBER STUDI R2.5 THERMAL INSULATION Colorbond Zincalume CLADDING + Fiber Cement CLADDING (OS)	13,694	
WT.07	INTERNAL 90mm TIMBER STUDI R2.5 THERMAL INSULATION Fiber Cement CLADDING (OS) + 10mm SOUNDCEK PLASTERBOARD (OS)	12,470	

ROOM SCHEDULE		
ROOM NAME	AREA	
BATHROOM	8.1	
BED 1	15.9	
BED 2	15.9	
BED 3	18.5	
DECK	19.1	
DECK	102.9	
DINING	13.6	
EN-SUITE	6.4	
GARAGE	42.3	
KITCHEN	30.1	
LAUNDRY	4.3	
LIVING	23.5	
MASTER BED	19.9	
PANTRY	4.3	
PASSAGE	31.5	
RUMPUS	16.3	
STORE	19.5	
STUDY	7.0	
WC	3.2	
LEVEL 0 TOTAL NET AREA (excl. GARAGE)	206.4	
LEVEL 1 TOTAL GROSS AREA	29.2	
LEVEL 1 TOTAL NET AREA	26.5	
TOTAL GROUND GROSS AREA FOOTPRINT	279.4	



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Received
 19/01/2026
Exhibited

BIFFIN RESIDENCE

GROUND FLOOR PLAN

PROJECT ADDRESS
 270 PERTH MILL RD,
 PERTH
 CLIENT
 MEL & TRENT BIFFIN

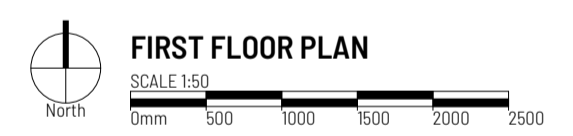
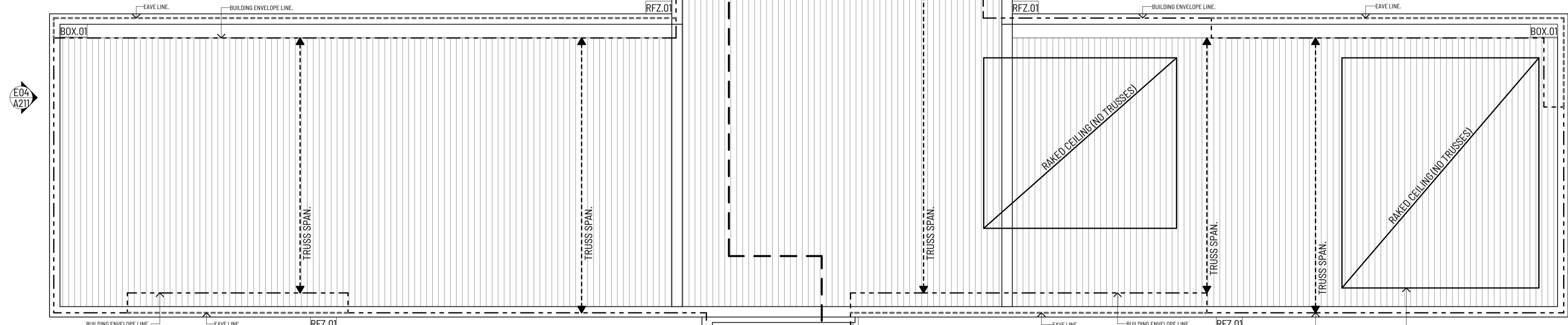
REV	DESCRIPTION	DATE
001	ISSUED FOR DEVELOPMENT APPROVAL	01/01/2025

CONTACT
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 DRAWN BY
 MHW
 APPROVED BY
 MHW
 12/01/2026

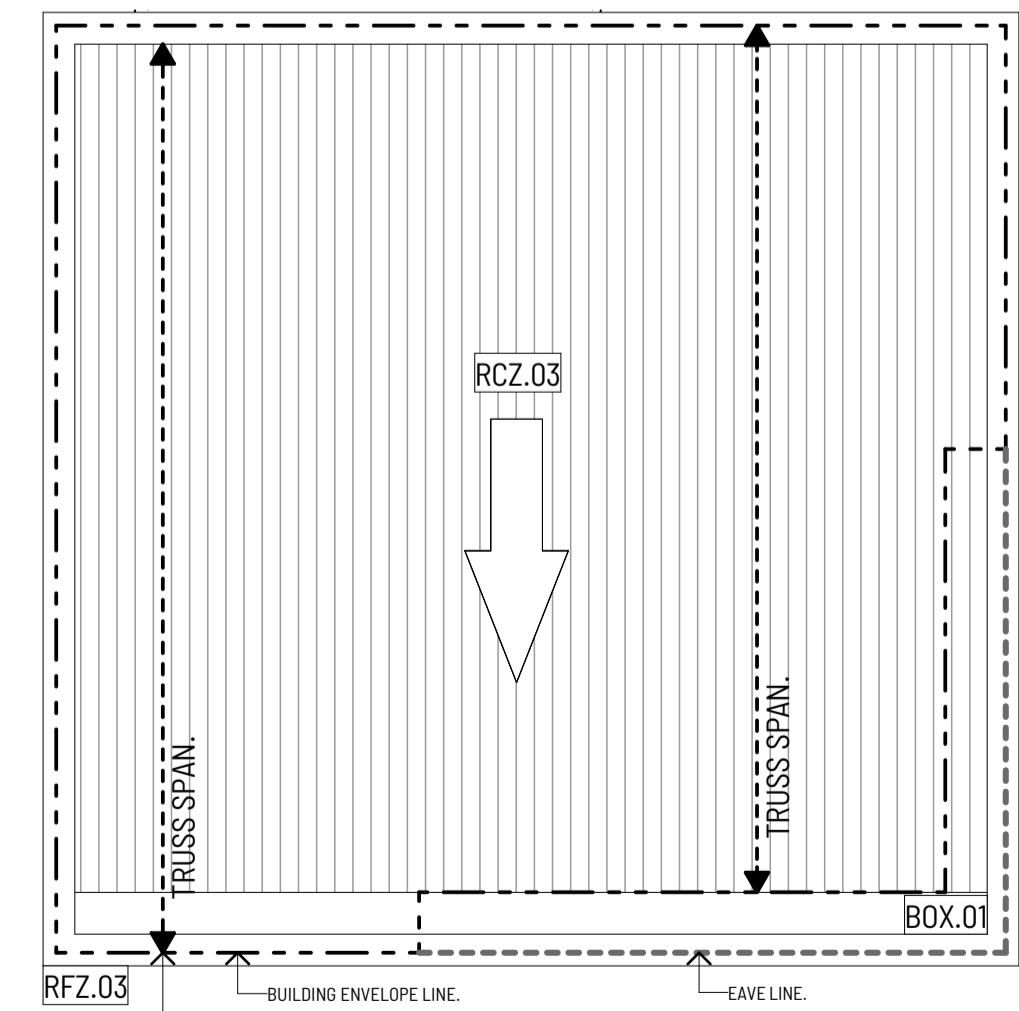
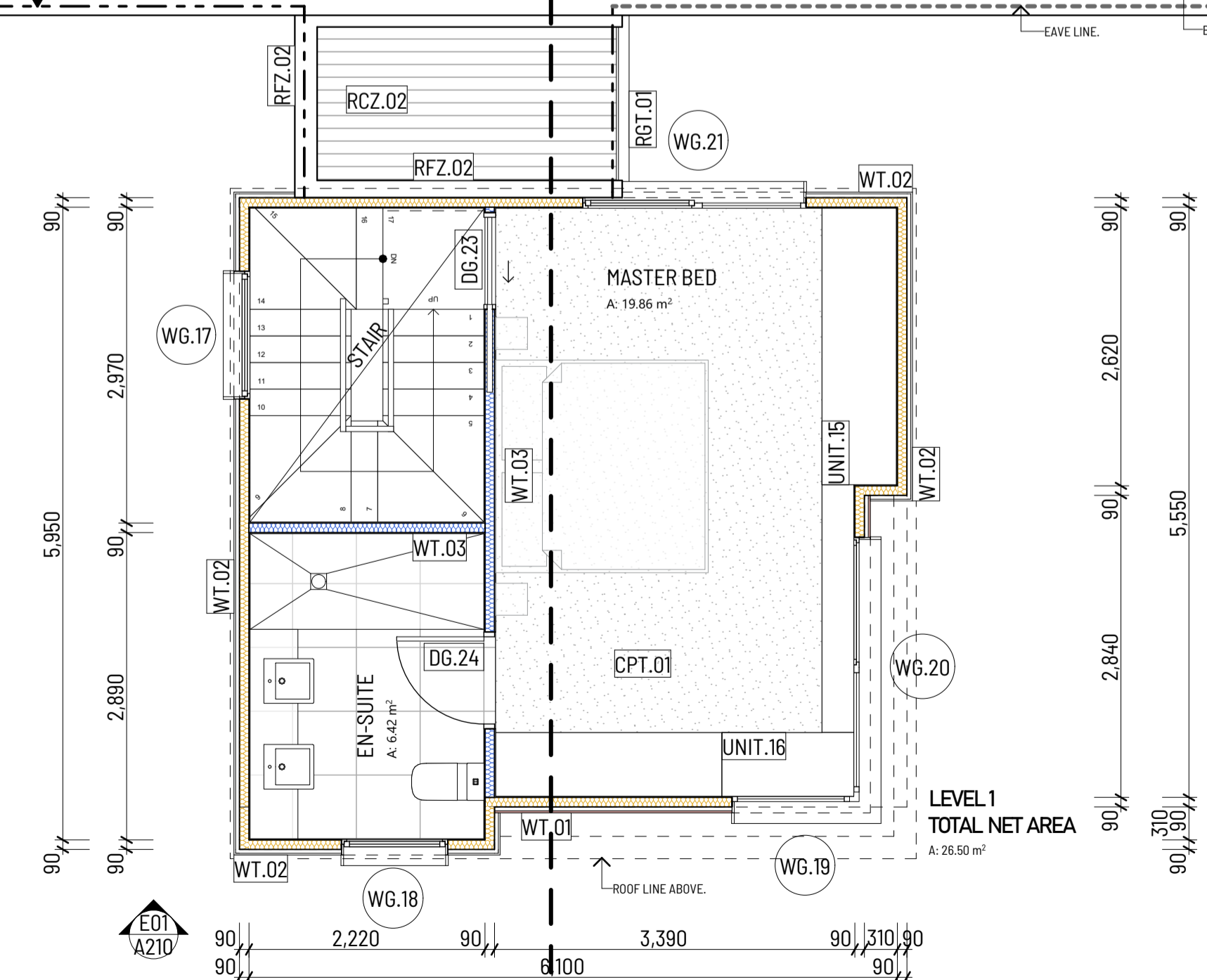
ISSUE
APPROVAL
 A120
 DA01
 241002
 N
 ATL

WALL SCHEDULE			
ID	DESCRIPTION	L/m	2D Plan
WT.01	EXTERNAL 90mm TIMBER STUD R2.5 THERMAL INSULATION Timber CLADDING + 10mm SOUNDCEK PLASTERBOARD (OS)	50,736	
WT.02	EXTERNAL 90mm TIMBER STUD R2.5 THERMAL INSULATION Colorbond Zincalume CLADDING + 10mm SOUNDCEK PLASTERBOARD (OS)	83,802	
WT.03	INTERNAL 90mm TIMBER STUD 10mm Soundchek Plasterboard (BS) + Soundscreen Insulation	69,670	
WT.04	INTERNAL 90mm TIMBER STUD NO INSULATION 10mm SOUNDCEK PLASTERBOARD (BS)	4,570	
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ROOF LEGEND		
ID	ITEM DESCRIPTION	PITCH
BOX.01	SS 150/h x 300mm/w BOXED GUTTER.	---
RCZ.01	COLORBOND ZINCALUME CUSTOM ORB ROOF SHEETING.	4.00°
RCZ.02	COLORBOND ZINCALUME CUSTOM ORB ROOF SHEETING.	3.00°
RCZ.03	COLORBOND ZINCALUME CUSTOM ORB ROOF SHEETING.	-10.00°
RFZ.01	COLORBOND ZINCALUME CUSTOM ORB ROOF FLASHING.	4.00°
RFZ.02	COLORBOND ZINCALUME CUSTOM ORB ROOF FLASHING.	3.00°
RFZ.03	COLORBOND ZINCALUME CUSTOM ORB ROOF FLASHING.	-10.00°
RGT.01	SS EAVES GUTTER.	---



FIRST FLOOR PLAN
SCALE 1:50



LEVEL 1 ROOF PLAN
SCALE 1:50

FLOOR COVERINGS SCHEDULE				
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CPT.01	BROADLOOM TUFT CARPET.	DOUBLE BOND SYSTEM.	57.6	
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FTL.01	CERAMIC FLOOR TILES. LAID OVER SCREED.		26.0	

ROOM SCHEDULE		
	ROOM NAME	AREA
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	BED 1	15.9
	BED 2	15.9
	BED 3	18.5
	DECK	19.1
	DECK	102.9
	DINING	13.6
	EN-SUITE	6.4
	GARAGE	42.3
	KITCHEN	30.1
	LAUNDRY	4.3
	LIVING	23.5
	MASTER BED	19.9
	PANTRY	4.3
	PASSAGE	31.5
	RUMPUS	16.3
	STORE	19.5
	STUDY	7.0
	WC	3.2
LEVEL 0	TOTAL NET AREA (excl. GARAGE)	206.4
LEVEL 1	TOTAL GROSS AREA	29.2
LEVEL 1	TOTAL NET AREA	26.5
TOTAL GROUND	GROSS AREA FOOTPRINT	279.4



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GENERAL NOTES

Received
19/01/2026

PROJECT NAME
BIFFIN RESIDENCE
Exhibited

DRAWING TITLE
LEVEL 1 FLOOR PLAN

PROJECT ADDRESS
270 PERTH MILL RD,
PERTH
CLIENT
MEL & TRENT BIFFIN

REV	DESCRIPTION	DATE
001	ISSUED FOR DEVELOPMENT APPROVAL	02/10/2025

CONTACT
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MHW
APPROVED BY
MHW
12/01/2026

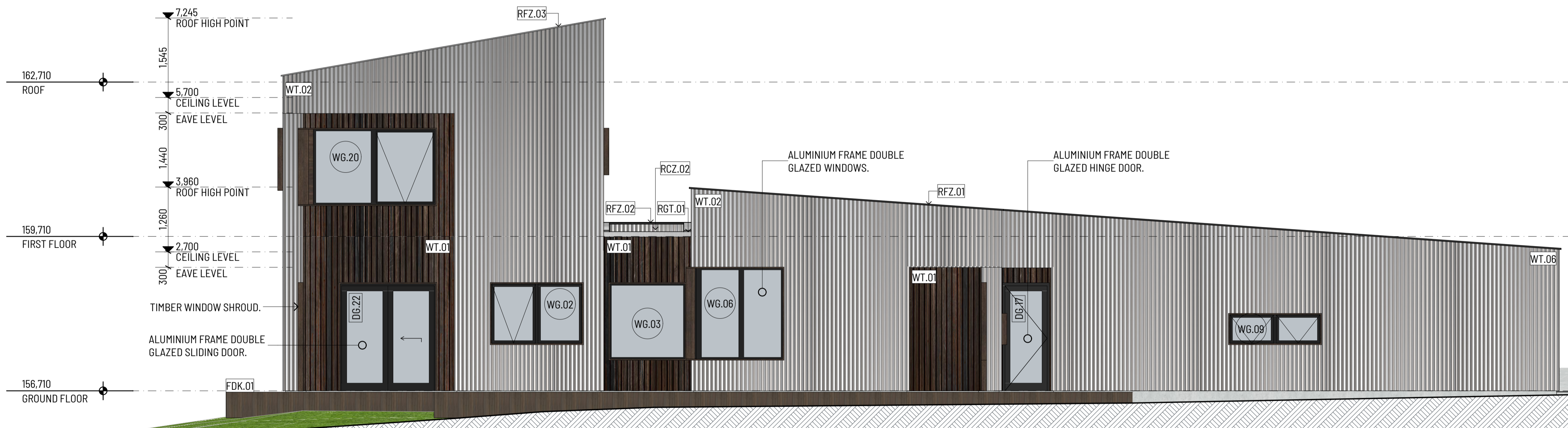
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APPROVAL

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A121 DA01

241002
N
A1L



SOUTH ELEVATION
SCALE 1:50
0mm 500 1000 1500 2000 2500



EAST ELEVATION
SCALE 1:50
0mm 500 1000 1500 2000 2500

ROOF LEGEND		
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RGT.01	SS EAVES GUTTER.	---

SITE GENERAL			
ID	DESCRIPTION	AREA	L/m
CCD.01	CONCRETE DRIVEWAY.	407.9	---
CGD.01	SEALED GRAVEL DRIVEWAY.	98.3	---
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RTW.01	LANDSCAPING BLOCKWORK RETAINING WALL.	17.2	71.256

WALL SCHEDULE			
ID	DESCRIPTION	L/m	2D Plan
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Received
19/01/2026

PROJECT NAME
BIFFIN RESIDENCE

DRAWING TITLE
ELEVATIONS(1 of 2)

PROJECT ADDRESS
270 PERTH MILL RD, PERTH
CLIENT
MEL & TRENT BIFFIN

REV	DESCRIPTION	DATE
001	ISSUED FOR DEVELOPMENT APPROVAL	01/10/2025

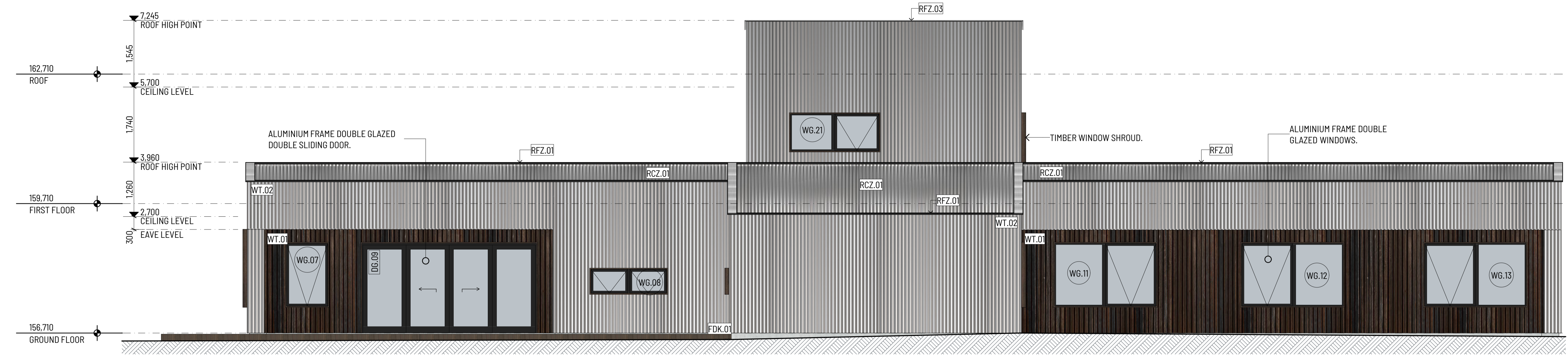
CONTACT
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EMAIL
mia@zuricreativestudio.co
DRAWN BY
MMW
APPROVED BY
MMW
12/01/2026

ISSUE
APPROVAL

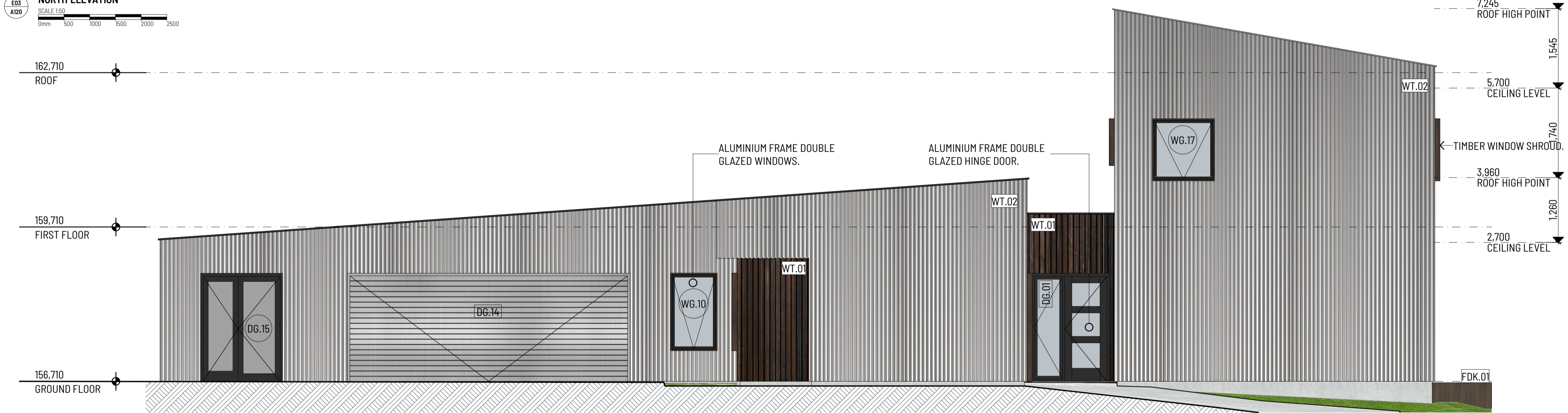
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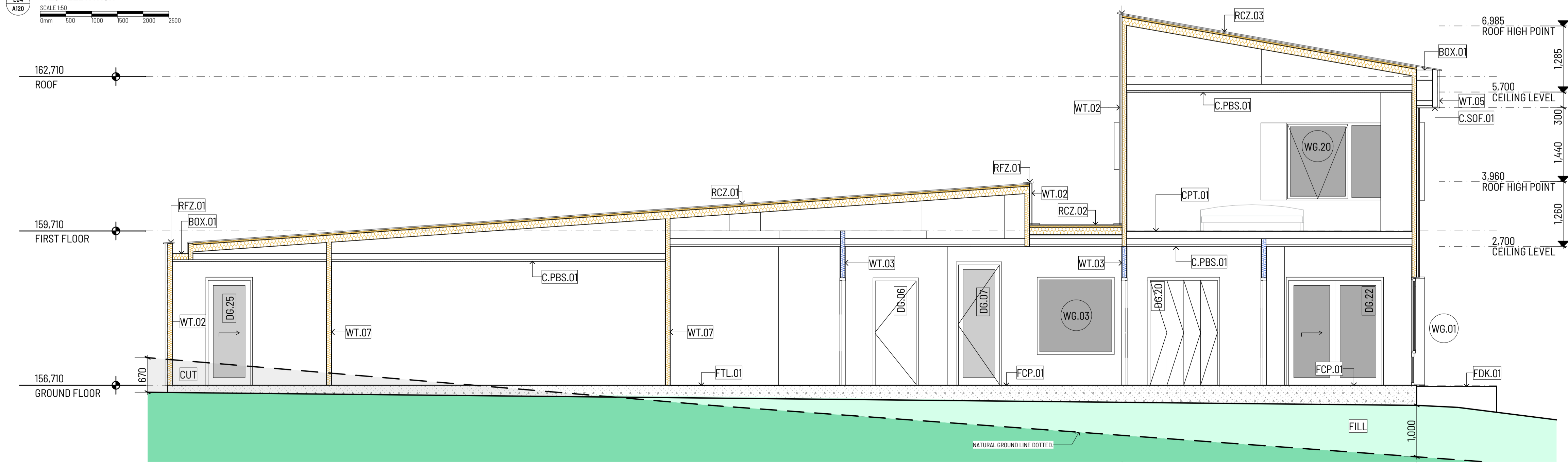




NORTH ELEVATION
SCALE 1:50
0mm 500 1000 1500 2000 2500



WEST ELEVATION
SCALE 1:50
0mm 500 1000 1500 2000 2500



A1 SECTION 1
SCALE 1:50
0mm 500 1000 1500 2000 2500

ROOF LEGEND		
ID	ITEM DESCRIPTION	PITCH
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RG.01	SS EAVES GUTTER.	---

SITE GENERAL			
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WALL SCHEDULE			
ID	DESCRIPTION	L/m	2D Plan
WT.01	EXTERNAL 90mm TIMBER STUDI R2.5 THERMAL INSULATION Timber CLADDING + 10mm SOUNDCEK PLASTERBOARD (OS)	50,736	[Pattern]
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WT.03	INTERNAL 90mm TIMBER STUDI 10mm SoundceK Plasterboard (BS) + Soundscreen Insulation	69,670	[Pattern]
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WT.06	EXTERNAL 90mm TIMBER STUDI R2.5 THERMAL INSULATION Colorbond Zincalume CLADDING + Fiber Cement CLADDING (OS)	13,694	[Pattern]
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GENERAL NOTES

Received
19/01/2026

PROJECT NAME
BIFFIN RESIDENCE

DRAWING TITLE
ELEVATIONS (2 of 2)

PROJECT ADDRESS
270 PERTH MILL RD, PERTH

REV	DESCRIPTION	DATE
001	ISSUED FOR DEVELOPMENT APPROVAL	01/10/2025

CONTACT
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mia@zuricreativestudio.com
DRAWN BY
MHW

ISSUE
APPROVAL

DRAWING NO.
A211 DA01

241002
ATL

28 May 2026

Trent Biffin
270 Perth Mill Road
Lot 2 270 Perth Mill Road
Perth TAS 7300

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Located nationally —
Melbourne
Sydney
Brisbane
Hobart
Launceston
Newcastle
Devonport

Dear Trent,

Re: Railway Noise and Vibration Impact at 270 Perth Mill Road, Perth



1. Introduction

This noise and ground vibration assessment has been prepared to support the development application for a proposed residential dwelling at 270 Perth Mill Road, Perth. The assessment is required as the site falls within the 50 metre attenuation zone of the TasRail Western Line Railway corridor. The site is zoned *Low Density Residential* under the Tasmanian Planning Scheme - *Northern Midlands*. The location of the site and proposed dwelling are shown in Figure 1 and Figure 2, below.

2. Tasmanian Planning Scheme Requirements

The *Tasmanian Planning Scheme – Northern Midlands*, Clause C3, *Road and Railway Assets Code* defines an attenuation zone of 50 metres from the boundary of a rail corridor or a major road with a speed limit above 60km/h. The parcel including the proposed dwelling falls within the attenuation zone of the TasRail Western Line. (Note Perth Mill Road is not a Major Road.)

Clause C3.6.1 provides development standards for habitable buildings for sensitive uses within a road or rail attenuation area and is reproduced below:

Objective:	
To minimise the effects of noise, vibration, light and air emissions on lots for sensitive uses within a road or railway attenuation area, from existing and future major roads and the rail network.	
Acceptable Solution	Performance Criteria
A1	P1
Unless within a building area on a sealed plan approved under this planning scheme, habitable buildings for a sensitive use within a road or railway attenuation area, must be:	Habitable buildings for sensitive uses within a road or railway attenuation area, must be sited, designed or screened to minimise adverse effects of noise, vibration, light and air emissions from the existing or future major road or rail network, having regard to:
<ul style="list-style-type: none"> a. within a row of existing habitable buildings for sensitive uses and no closer to the existing or future major road or rail network than the adjoining habitable building; b. an extension which extends no closer to the existing or future major road or rail network than: <ul style="list-style-type: none"> i. the existing habitable building; or ii. an adjoining habitable building for a sensitive use; or c. located or designed so that external noise levels are not more than the level in Table C3.2 measured in accordance with Part D of the Noise Measurement Procedures Manual, 2nd edition, July 2008. 	<ul style="list-style-type: none"> a. the topography of the site; b. the proposed setback; c. any buffers created by natural or other features; d. the location of existing or proposed buildings on the site; e. the frequency of use of the rail network; f. the speed limit and traffic volume of the road; g. any noise, vibration, light and air emissions from the rail network or road; h. the nature of the road; i. the nature of the development; j. the need for the development; k. any traffic impact assessment; l. any mitigating measures proposed; m. any recommendations from a suitably qualified person for mitigation of noise; and n. any advice received from the rail or road authority.

Table C3.2 identifies the following acceptable limits within a road or railway attenuation area:

- **Railways:** An average noise level over 24 hours ($L_{Aeq, 24 \text{ hour}}$) of 65 dB(A) and a maximum (L_{Amax}) noise level of 87 dB(A) assessed as a single event maximum sound pressure level.

3. Noise

Train noise measurements have been previously undertaken by **pitt&sherry** over a 3 day period, on the Western Line. At a measurement distance of 8.5 metres from the centreline of the railway line the following noise levels from passing trains were identified:

- A typical $L_{Aeq, 10min}$ of 76.4 dB(A) for the 10 minute period in which trains were passing,
- A peak L_{Apeak} of 86.6 dB(A) (i.e., the peak noise level as the train passed), and
- A maximum L_{Amax} of 95.6 dB(A) (i.e., from train horns)

Based on the proposed building plans the minimum distance between the closest façade to the railway line and the centre of the railway line is 37.3 metres. The nearest level crossings to the site are 800 to 1400 metres to east and west of the property. Assuming the horn is sounded 200 metres prior to the train reaching the crossing and a maximum of 4 trains along the rail over a 24 hour period this provides the following noise levels:

- A $L_{Aeq, 24hour}$ of 48.0 dB(A)
- A L_{Apeak} Of 73.8 dB(A)
- A L_{Amax} Of 74.0 dB(A)

These noise levels are all below the relevant limit by a significant margin, therefore the proposed dwelling meets the Acceptable Solution A1 (c).

4. Conclusion

Calculated noise levels from trains passing the proposed dwelling are significantly below the noise levels given in the Table C3.2. At the proposed building setback of 37m, the level of ground vibration from the rollingstock currently operating in Tasmania is not sufficient to pose a threat to building structures or annoyance to residents.

As such the proposed dwelling meets the Acceptable Solution A1 (c) of the *Tasmanian Planning Scheme – Northern Midlands, Clause C3, Road and Railway Assets Code*.

Please do not hesitate to contact us if you have any queries.

Yours sincerely,



Alexander Seen
Mechanical Engineer / Noise Specialist



Douglas Ford
Principal Mechanical Engineer / Noise Specialist

Author Qualifications

Douglas Ford (Bachelor of Engineering (Mechanical), University of Queensland, MIEAust, CPEng, RPEQ)

Doug has over 30 years' mechanical engineering experience working in design, technical support, research, maintenance and project management roles. He has 13 years' experience in the area of writing noise assessments in support of planning permit applications in Tasmania, industrial and traffic noise modelling and the design of noise attenuation measures for industrial and commercial building applications. He has appeared as an expert witness before the Tasmanian Civil and Administrative Tribunal on a number of occasions and submitted numerous assessment reports to the Tasmanian EPA. He also has significant experience modelling and assessing other emissions including dust, odour, airborne chemical pollutants, and nuisance lighting.

Alexander Seen (Bachelor of Engineering (Marine and Offshore Engineering), Australian Maritime College)

Alex has four years' experience undertaking noise measurements, noise modelling and writing noise assessments. Under the supervision of Douglas Ford, he has completed a wide range of projects including assessments for a number of different industrial and commercial developments, residential subdivisions and road upgrades.

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Figure 1: Aerial view of 270 Mill Road and surrounding area.

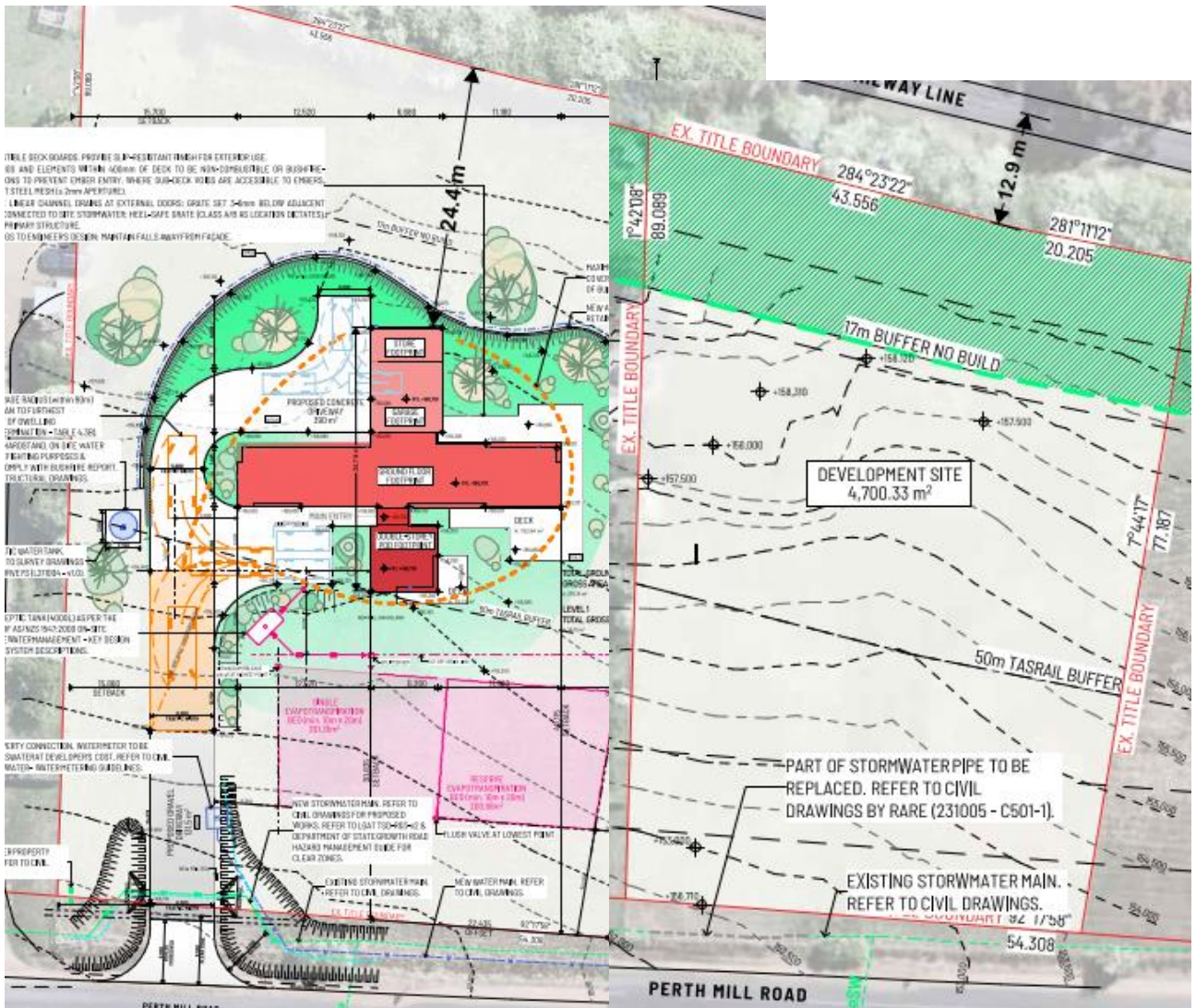


Figure 2: Proposed building location(right) and site plan (left), including setback from railway line.

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Memo

To Trent Biffin
From Theo Risley, Pitt&Sherry
Date 8 July 2026
RE **Overland Flow Assessment at 270 Perth Mill Road, Perth – Rev01**

1. Introduction

This overland flow assessment has been prepared to support the Development Application for a proposed residential dwelling at 270 Perth Mill Road, Perth (as shown in Figure 2). The assessment is required as council has identified an overland flow path across the property which originates from flows discharging from an existing TasRail culvert. The location of the site is shown in Figure 1.

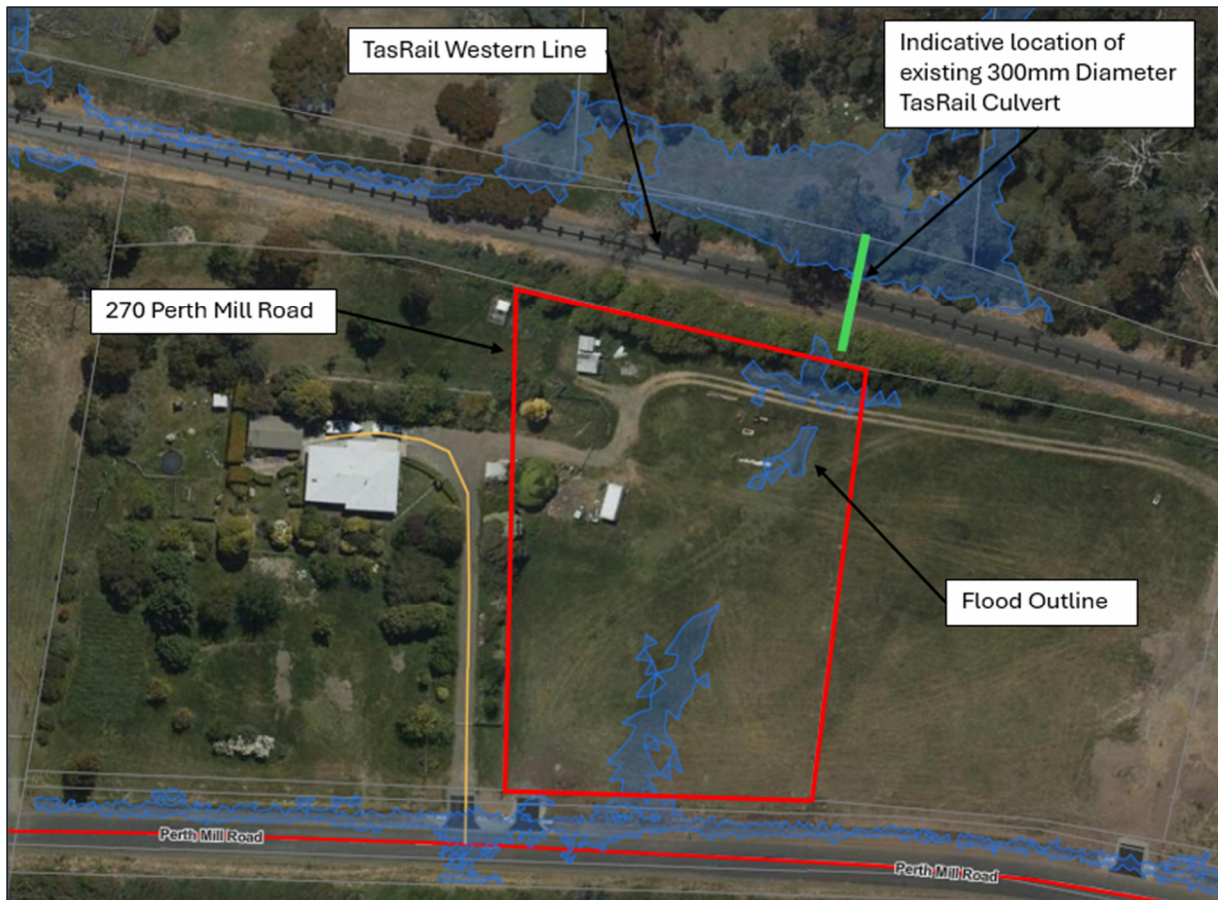


Figure 1: Aerial view of 270 Mill Road with flood outline.

Acceptable Solution	Performance Criteria
<p>A1</p> <p>No Acceptable Solution.</p>	<p>P1.1</p> <p>Buildings and works within a flood-prone hazard area must achieve and maintain a tolerable risk from a flood, having regard to:</p> <ul style="list-style-type: none"> a) the type, form, scale and intended duration of the development; b) whether any increase in the level of risk from flood requires any specific hazard reduction or protection measures; c) any advice from a State authority, regulated entity or a council; and d) the advice contained in a flood hazard report.
	<p>P1.2</p> <p>A flood hazard report also demonstrates that the building and works:</p> <ul style="list-style-type: none"> a) do not cause or contribute to flood on the site, on adjacent land or public infrastructure; and b) can achieve and maintain a tolerable risk from a 1% annual exceedance probability flood event for the intended life of the use without requiring any flood protection measures.

3. Site Context and Catchment

The catchment area associated with the existing TasRail culvert is in the order of 8.45 ha (see Figure 3) and has the following general features:

- Steep grade at an average of 8%
- Generally, bushland with some properties within it.
- The railway culvert (KPW 021.777) is 300mm Diameter and 16 metres long as confirmed by TasRail via email on the 11th June 2026. The culvert is shown below in Figure 5.



Figure 3. TasRail Culvert (KPW 021.777) Catchment Area

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4. Proposed Overland Flow Management

To manage overland flow directed towards the site, a diversion ditch is proposed around the site perimeter to intercept and convey runoff. The ditch will capture flows and direct them southwards towards the drainage infrastructure along Perth Mill Road, as incorporated within the civil design. The proposed drainage channel (ditch) will incorporate erosion protection measures, including riprap lining. Riprap will also be installed at the outlet connection to the existing roadside drain to prevent localised scour. In addition, the outlet connection at the existing ditch will connect at approx. 45 degrees to improve flow alignment.

The proposed alignment is shown in Figure 4.

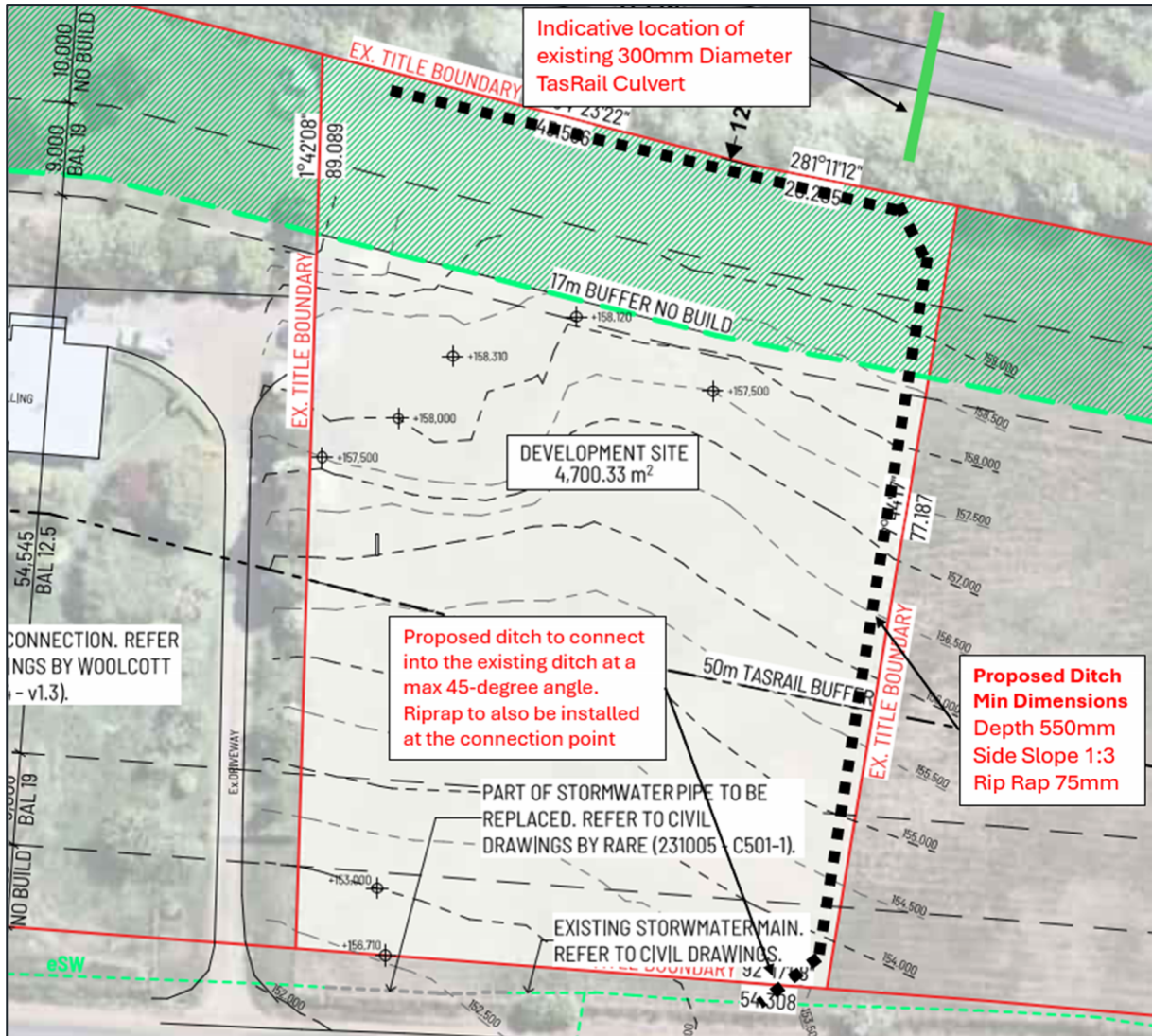


Figure 4. Proposed overland flood management strategy (background Drawing A004 Rev BA, Zuri Creative Studio, Dated 22/04/2026)

5. Hydrology & Hydraulics

The hydrologic and hydraulic analysis has been undertaken utilising the software DRAINS to estimate the discharge from the existing culvert towards the site. This includes the following assumptions and inputs:

- Initial Loss (IL) – Continuing Loss (CL) model with IL 19mm and CL 5.2mm/hr
- Rainfall IFD information from BOM at location 41.56 degrees south, 147.19 degrees east
- Default IFD rainfall values adjusted for each duration as per updated ARR guidance
- Temporal rainfall patterns obtained from the ARR datahub
- Median Pre-burst rainfall depths from ARR datahub
- Pipe Manning 'n' = 0.013
- Inlet Headloss Coefficient = 0.5
- Time of Concentration 12-minutes estimated using the Friends Equation
- Catchment Delineation using 1m LiDAR from ELVIS (SouthWestLaunceston2024)
- Details of culvert as confirmed by TasRail. However, invert levels have been estimated based on LiDAR, with the assumption that the culvert invert level is set to surrounding terrain RL. The culvert is shown in Figure 5.
- There is a significant volume of water that could be attenuated behind the rail, This has not been included in the hydraulic model and is therefore this approach is considered conservative.
- When water levels reach approximately 164.2 m AHD, floodwaters overtop the high point to the west of the culvert and flow overland, running adjacent to the railway line. This flow path limits the applicable headwater at the culvert. The railway line itself overtops at approximately 165 m AHD, indicating that in extreme flood events, flows are not directed towards the site.
- A climate change allowance of 1.66 for sub-hourly events has been included which is in line with the SSP3-7.0 in 2100.



Figure 5. Photos of the TasRail culvert from site.

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6. Results of the Culvert Assessment

As shown in Figure 6, the peak discharge from the culvert in the 1% AEP is 0.21 m³/s. Overland flow to the south of the railway which may be intercepted by the proposed ditch have also been considered as part of the assessment. Adopting a minimum channel depth of 550mm with side slopes of 1 in 3 allows the channel to manage flows up to the 1% AEP with 300mm freeboard. The peak velocity in the channel is 2.0 m/s. To manage potential erosion, in line with Figure 2.22 in Austroads Guide to Road Design Part 5B, a minimum riprap stone diameter of 75mm is required.

It is recommended that the minimum ditch dimensions and riprap specifications are adopted along the section adjacent to the proposed development to ensure adequate protection of the property. Riprap will also need to be installed at the outlet connection to the existing roadside drain to prevent localised scour.

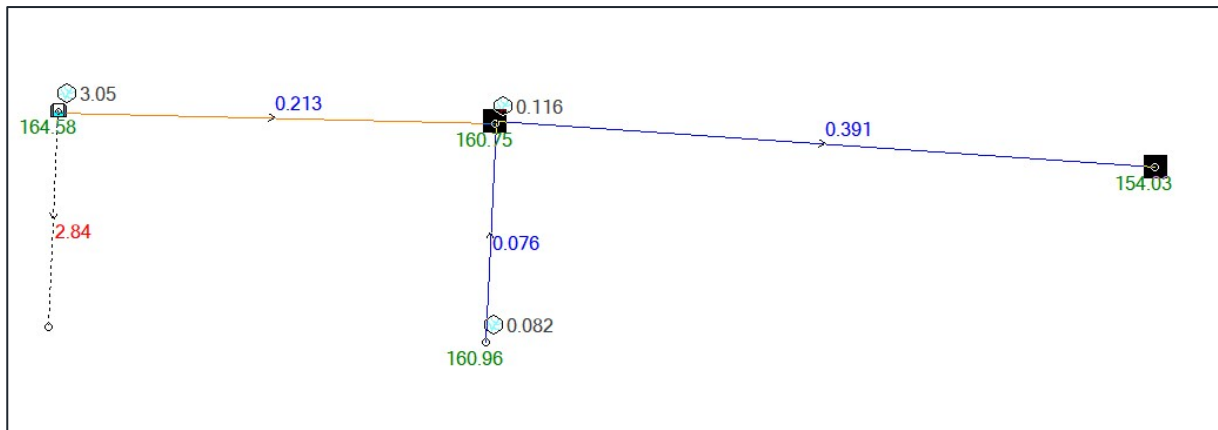


Figure 6. DRAINS results for the 1% AEP in 2100 (SSP3-7.0)

A Manning’s equation assessment has been undertaken to estimate the capacity of the existing roadside drainage channel, which is calculated to be approximately 1.1 m³/s. The peak flow conveyed by the proposed drainage during the 1% AEP + climate change event is estimated to be 0.40 m³/s. Based on this comparison, the existing channel has capacity to receive the expected peak flow. Noting that the existing ditch (designed for the wider subdivision) is diverting existing flows, so there is no increase in runoff because of the proposal.

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7. Response to Tasmania Planning Scheme Requirements

Responses to the Tasmania Planning Scheme Requirements have been provided in Table 2. The assessment considers the management of the overland flow path directed towards the site. Refer to the civil drawings associated with the development regarding the details of how stormwater from the proposed building will be managed.

Table 2: Responses to C12.6.1 Buildings and works within a flood-prone hazard area

Objective	
<p>That:</p> <p>(a) building and works within a flood-prone hazard area can achieve and maintain a tolerable risk from flood; and</p> <p>(b) buildings and works do not increase the risk from flood to adjacent land and public infrastructure.</p>	
Acceptable Solution	Performance Criteria
<p>A1</p> <p>No Acceptable Solution.</p>	<p>P1.1</p> <p>Buildings and works within a flood-prone hazard area must achieve and maintain a tolerable risk from a flood, having regard to:</p> <ul style="list-style-type: none"> a) the type, form, scale and intended duration of the development; b) whether any increase in the level of risk from flood requires any specific hazard reduction or protection measures; c) any advice from a State authority, regulated entity or a council; and d) the advice contained in a flood hazard report.
<p>A1 Response</p> <p>N/A</p>	<p>P1.1 Response</p> <p>The proposed development has been designed to achieve and maintain a tolerable level of flood risk, taking into account its type, scale, and intended use, with site-specific mitigation measures incorporated to manage overland flood behaviour. In particular, a diversion ditch has been implemented to intercept and redirect flows from the existing TasRail culvert away from the development, thereby reducing flood hazard and limiting residual risk to acceptable levels. This document serves as the flood hazard report and demonstrates that the development appropriately addresses flood risk and satisfies the required criteria for development within a flood-prone hazard area.</p> <p>Refer to the civil drawings associated with the development regarding the details of how stormwater from the proposed building will be managed.</p>
	<p>P1.2</p> <p>A flood hazard report also demonstrates that the building and works:</p> <ul style="list-style-type: none"> a) do not cause or contribute to flood on the site, on adjacent land or public infrastructure; and b) can achieve and maintain a tolerable risk from a 1% annual exceedance probability flood event for the intended life of the use without requiring any flood protection measures.
	<p>P1.2 Response</p> <p>The flood hazard assessment, associated with managing the overland flow path, demonstrates that the proposed building and works do not cause or contribute to flooding on the site, adjacent land, or public infrastructure, as the design only diverts existing flows without increasing discharge offsite. A high-level manning's assessment shows that the existing ditch has capacity to receive the existing</p>

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flows. Though the responsibility of the design for this ditch relies on the wider subdivision design.

The intercepting ditch has been designed to accommodate the 1% AEP flood event, incorporating a minimum 300 mm freeboard to ensure a tolerable level of flood risk is achieved and maintained over the life of the development.

Refer to the civil drawings associated with the development regarding the details of how stormwater from the proposed building will be managed.


8. Conclusion and Recommendations

A hydrologic and hydraulic assessment has been undertaken to assess overland flows from the existing TasRail culvert and inform appropriate management measures for the proposed development. The results demonstrate that the 1% AEP flows from the TasRail culvert can be effectively managed through the implementation of a diversion ditch designed to safely convey flows around the site, maintaining a tolerable level of flood risk over the life of the development. This is achieved through adoption of the following measures:

- A diversion ditch with a minimum depth of 550 mm and side slopes of 1 in 3, providing sufficient capacity to convey the 1% AEP flow with 300 mm freeboard.
- Riprap protection along the proposed ditch and at the outlet connection to the existing roadside drain with a minimum stone diameter of 75 mm to mitigate potential erosion.

These measures are considered sufficient to protect the development and manage overland flow in accordance with relevant planning requirements.

Yours sincerely



Theo Risley
Senior Engineer

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