

This planning application is open for
public comment until
13 May 2024

Reference no	PLN-24-0051
Site	31 NORWICH DRIVE LONGFORD (ACCESS OVER F.R. 135661/1)
Proposed Development	Visitor Accommodation - 2 new units
Zone	11.0 Rural Living
Use class	Visitor Accommodation

Written representations may be made during this time to the General Manager;
mailed to PO Box 156, Longford, Tasmania 7301,
delivered to Council offices or
a pdf letter emailed to planning@nmc.tas.gov.au

(no special form required)

PLANNING APPLICATION
Proposal

Description of proposal: Multiple visitors accommodation

.....
.....
.....
.....
.....

(attach additional sheets if necessary)

If applying for a subdivision which creates a new road, please supply three proposed names for the road, in order of preference:

1..... 2..... 3.....

Site address: 31 Norwich drive, Longford Tasmania

.....

CT no:

Estimated cost of project \$ 800,000
(include cost of landscaping, car parks etc for commercial/industrial uses)

Are there any existing buildings on this property? Yes / No
If yes – main building is used as dwelling

If variation to Planning Scheme provisions requested, justification to be provided:

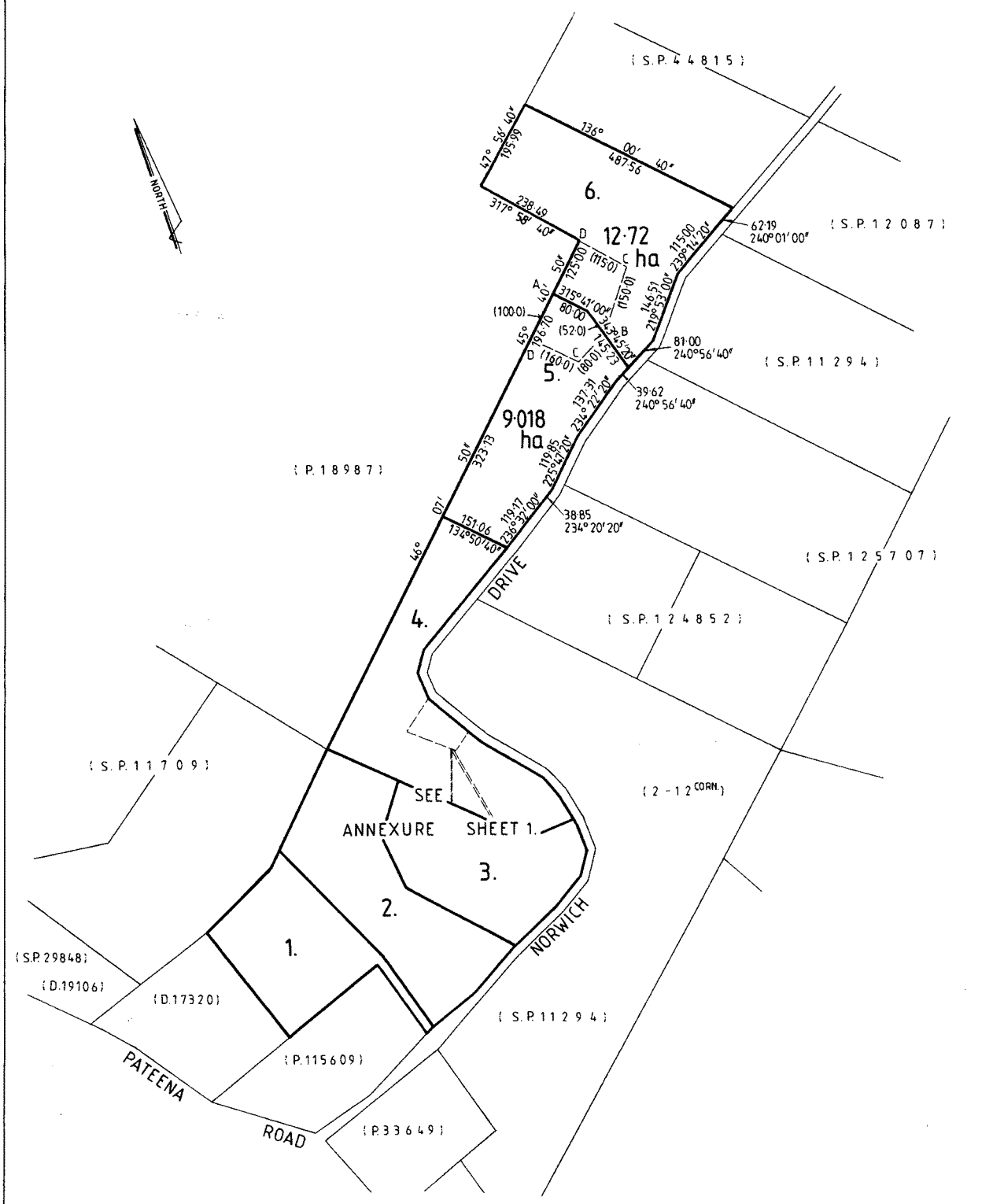
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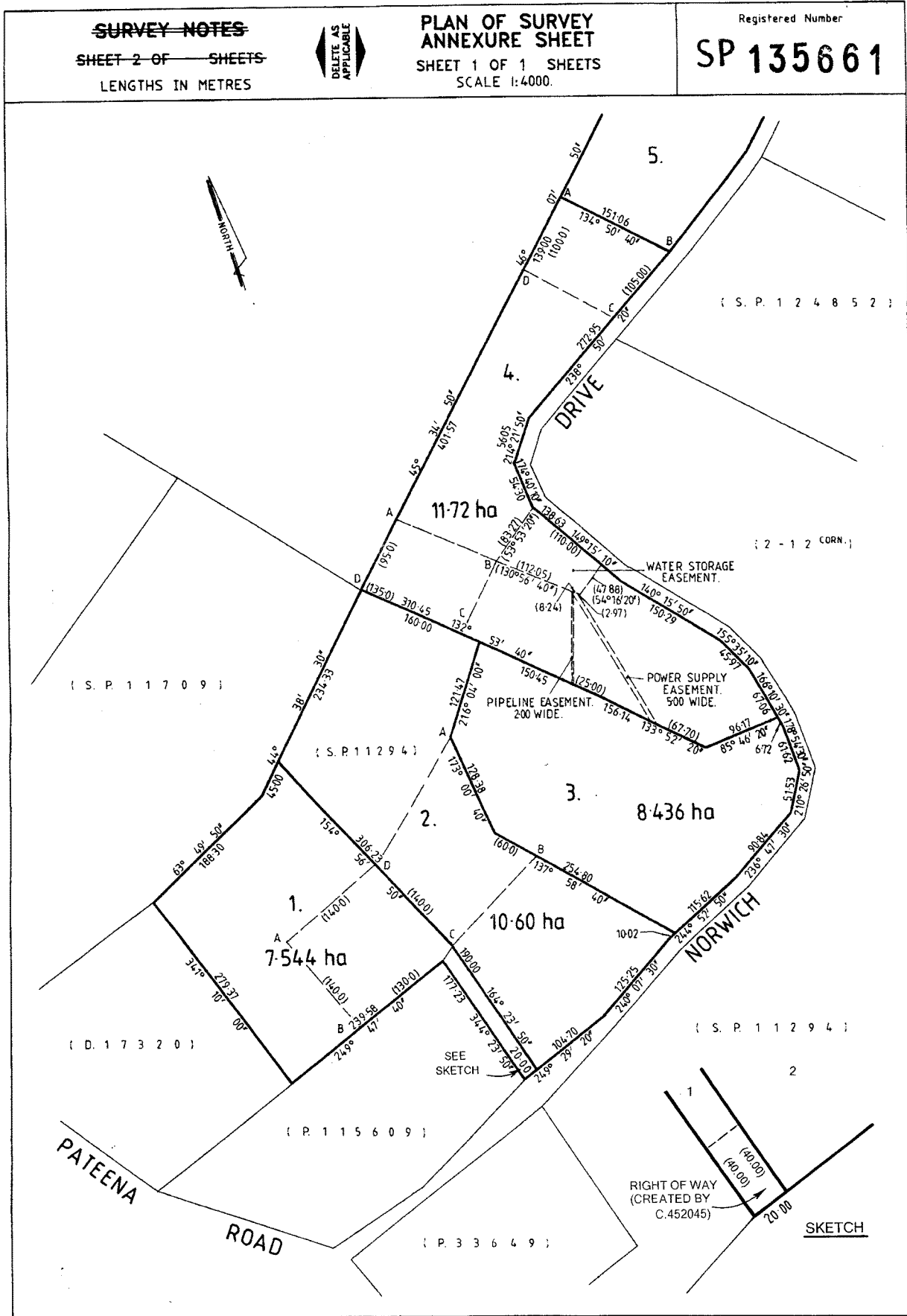
(attach additional sheets if necessary)

Is any signage required?
(if yes, provide details)

OWNER ROBERT WILLIAM SAUNDERS. VIVIANNE JOY SAUNDERS. FOLIO REFERENCE C.T.11294-1. C.T.12087-19. GRANTEE PART OF 2500 ACRES GTD. TD THDMAS CDDKE DYBALL.	PLAN OF SURVEY BY SURVEYOR R.V.TAIT. — G.J.WALKEM & CO. LAUNCESTON. LOCATION LAND DISTRICT OF CORNWALL. PARISH OF BREADALBANE. SCALE 1:7500. LENGTHS IN METRES		REGISTERED NUMBER SP135661
			APPROVED EFFECTIVE FROM 3 AUG 2001 <i>Alice Kawa</i> Recorder of Titles

MAPSHEET MUNICIPAL CODE No. 123. (5040)	LAST UPI No. 5600019 5600020	LAST PLAN No. SP.11294. SP.12087.	ALL EXISTING SURVEY NUMBERS TO BE CROSS REFERENCED ON THIS PLAN
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DRAWING SCHEDULE

NUMBER	NAME	DATE
A00	COVER SHEET	02/04/24
A01	SITE PLAN 1	02/04/24
A02	SITE PLAN 2	02/04/24
A03	FLOOR PLAN UNIT 1 AND UNIT 2	02/04/24
A04	SITE PERSPECTIVE ELEVATION	02/04/24
A05	ELEVATION UNIT 1	02/04/24
A06	ELEVATION UNIT 2	02/04/24
A07	SHADOW DIAGRAMS	02/04/24
A08	3D VISUALISATION	02/04/24
A09	3D VISUALISATION	02/04/24

Exhibited



GENERAL INFORMATION

Accredited Architect: Jonathan Buist
 Accreditation Number: 947052254
 Certificate of Title:
 PID:
 Soil Classification: N/A
 Wind Classification: N/A
 Alpine Area: N/A
 Bushfire-prone Area BAL rating: N/A
 Corrosive Environment: N/A
 Site Area: 10.60 ha
 Existing Upper level Building Area: m²
 Proposed Building Area: m²
 Planning Zone: 8.0 GENERAL RESIDENTIAL
 Heritage Listing: No

**PLANNING
APPLICATION**



REV	DATE	DESCRIPTION
0	08 - 02 - 2024	PRELIM WORK IN PROGRESS
1	04 - 03 - 2024	DEVELOPMENT APPLICATION (FOR REVIEW)
2	02 - 04 - 2024	DEVELOPMENT APPLICATION

DWG	COVER SHEET	PROJECT #	Project Number
PROJECT		Visitors accommodation in Longfoed	DWG #
CLIENT		DB INVESTMENTS	A00
ADDRESS		31 Norwich drive Longford Tasmania	ACCREDITED DESIGNER 947052254
SCALE @ A3	1 : 5000	DRAWN	RK
CHKD	JB		

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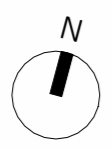


Exhibited

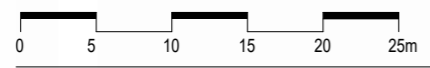


1 SITE PLAN 1
1 : 2500

PLANNING APPLICATION



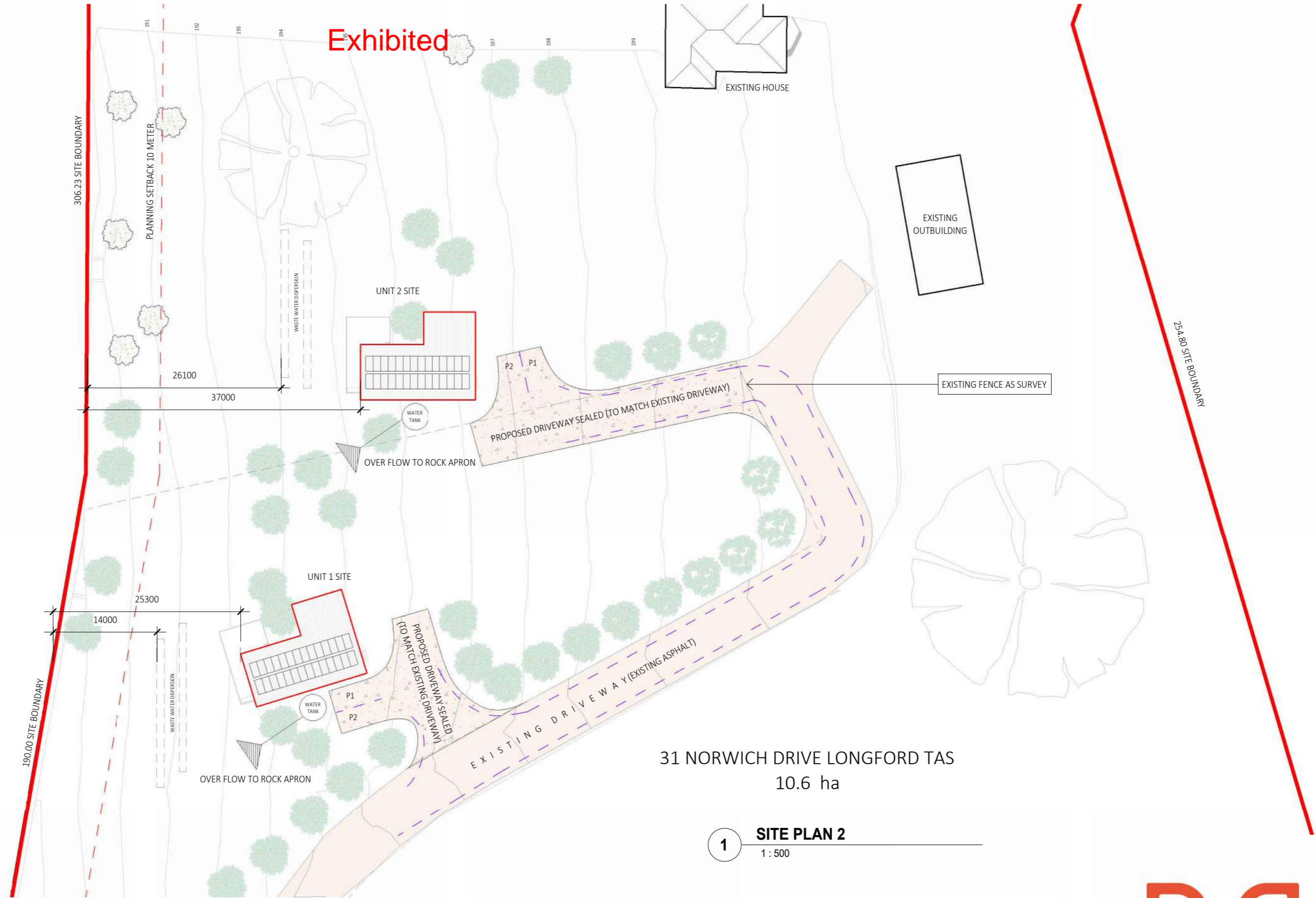
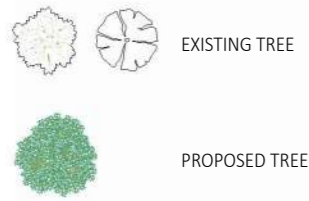
REV	DATE	DESCRIPTION
0	08 - 02 - 2024	PRELIM WORK IN PROGRESS
1	04 - 03 - 2024	DEVELOPMENT APPLICATION (FOR REVIEW)
2	02 - 04 - 2024	DEVELOPMENT APPLICATION



DWG SITE PLAN 1		PROJECT # Project Number
PROJECT Visitors accommodation in Longfoed		DWG # A01
CLIENT DB INVESTMENTS	SCALE @ A3 1 : 2500	ACCREDITED DESIGNER 947052254
ADDRESS 31 Norwich drive Longford Tasmania	DRAWN CHKD RK JB	

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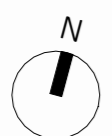




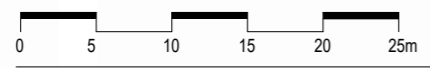
31 NORWICH DRIVE LONGFORD TAS
10.6 ha

1 SITE PLAN 2
1 : 500

PLANNING APPLICATION



REV	DATE	DESCRIPTION
0	08 - 02 - 2024	PRELIM WORK IN PROGRESS
1	04 - 03 - 2024	DEVELOPMENT APPLICATION (FOR REVIEW)
2	02 - 04 - 2024	DEVELOPMENT APPLICATION

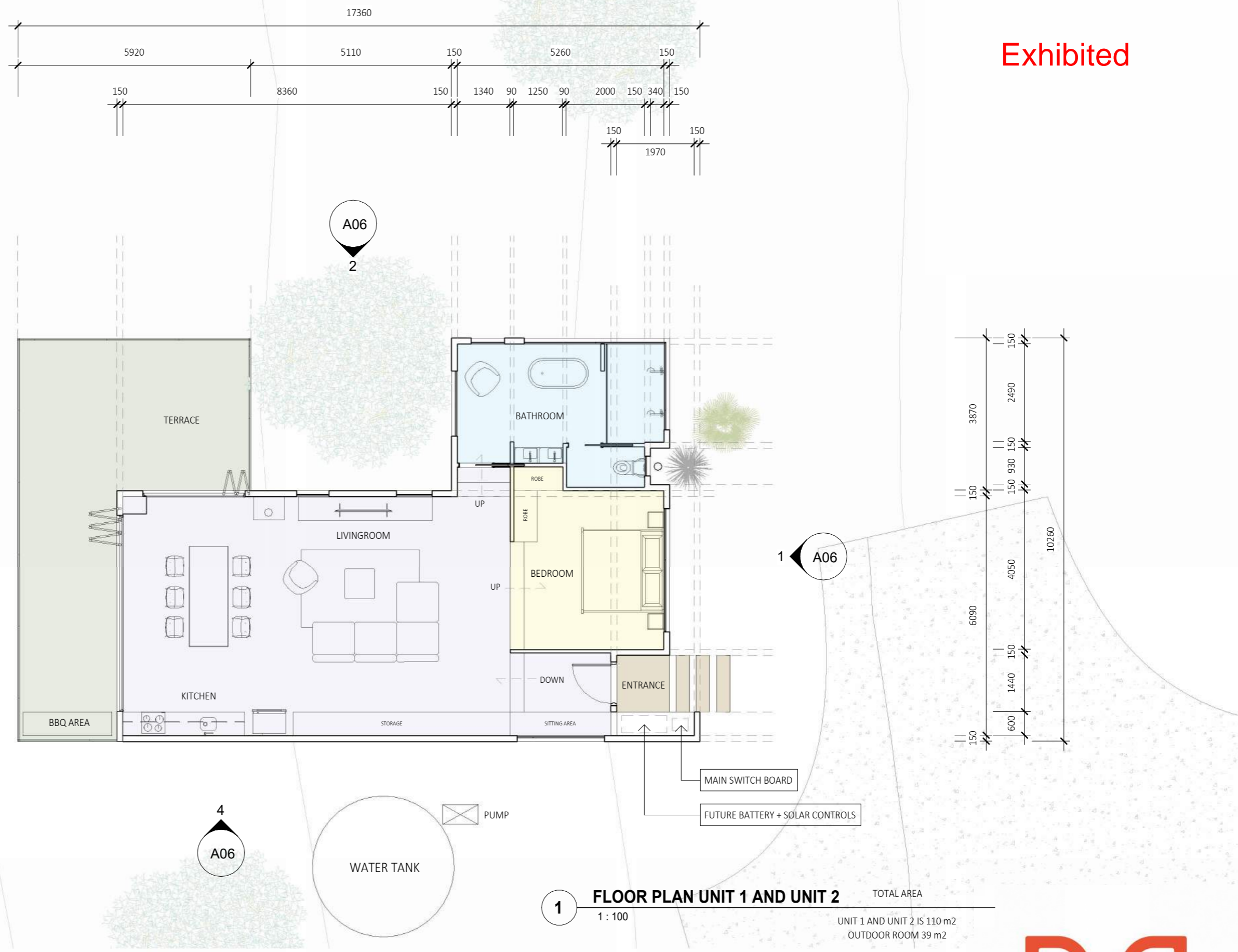


DWG	SITE PLAN 2	PROJECT #	Project Number
PROJECT	Visitors accommodation in Longford	DWG #	A02
CLIENT	DB INVESTMENTS	SCALE @ A3	1 : 500
ADDRESS	31 Norwich drive Longford Tasmania	DRAWN	RK CHKD JB
		ACCREDITED DESIGNER	947052254



DRAWING KEY
GENERAL NOTES
 ARCHITECTURAL PLANS & DRAWINGS & SPECIFICATIONS ARE TO BE READ IN CONJUNCTION WITH DRAWINGS BY OTHERS FOR ALL ELECTRICAL, MECHANICAL, LIGHTING, FIRE DETECTION, CIVIL & STRUCTURAL WORKS SPECIFICATIONS.

Exhibited



1 FLOOR PLAN UNIT 1 AND UNIT 2 TOTAL AREA
 1 : 100 UNIT 1 AND UNIT 2 IS 110 m2
 OUTDOOR ROOM 39 m2

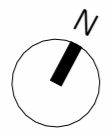
REV	DATE	DESCRIPTION
0	08 - 02 - 2024	PRELIM WORK IN PROGRESS
1	04 - 03 - 2024	DEVELOPMENT APPLICATION (FOR REVIEW)
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DWG FLOOR PLAN UNIT 1 AND UNIT 2		PROJECT # Project Number
PROJECT Visitors accommodation in Longfoed		DWG #
CLIENT DB INVESTMENTS	SCALE @ A3 1:100	A03
ADDRESS 31 Norwich drive Longford Tasmania	DRAWN CHKD RK JB	ACCREDITED DESIGNER 947052254

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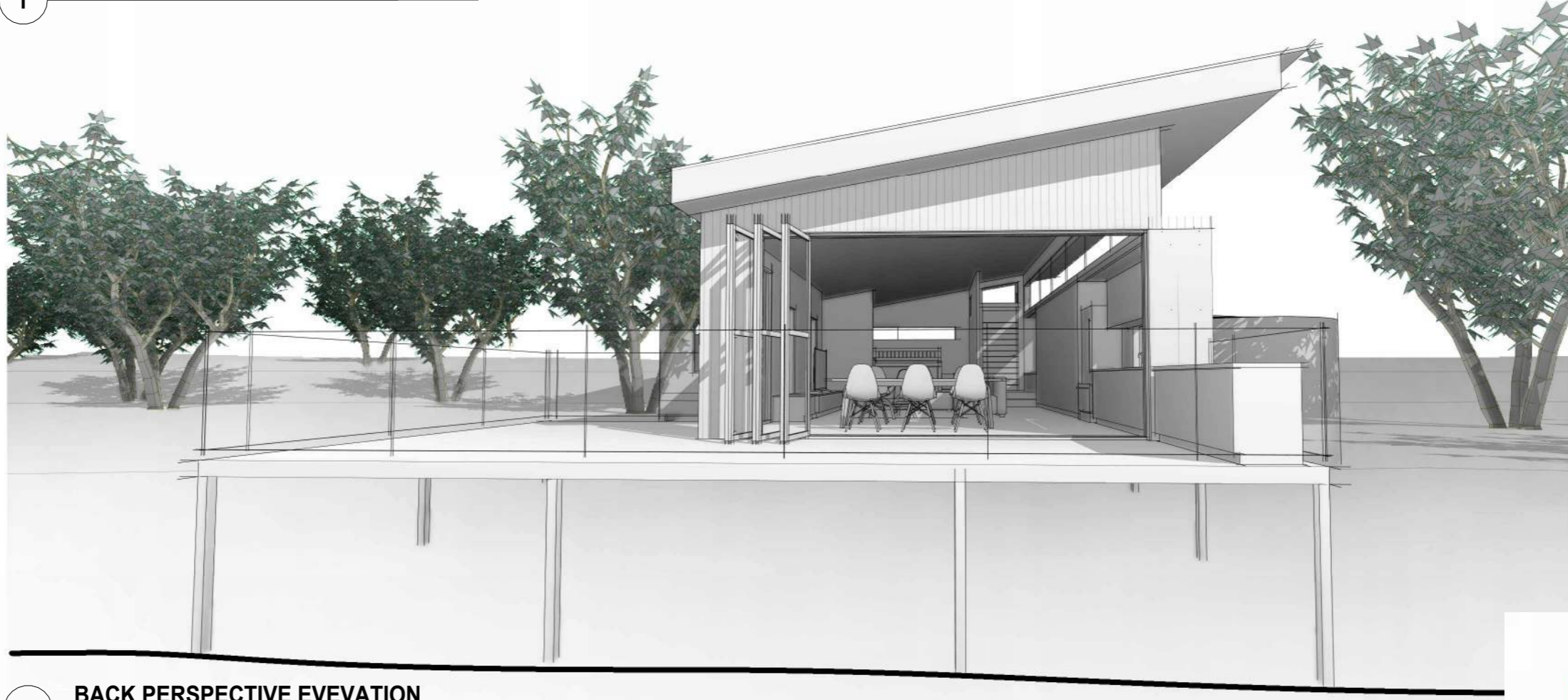
PLANNING APPLICATION



Exhibited



1 FRONT PERSPECTIVE EVEVATION



2 BACK PERSPECTIVE EVEVATION

REV	DATE	DESCRIPTION
0	08 - 02 - 2024	PRELIM WORK IN PROGRESS
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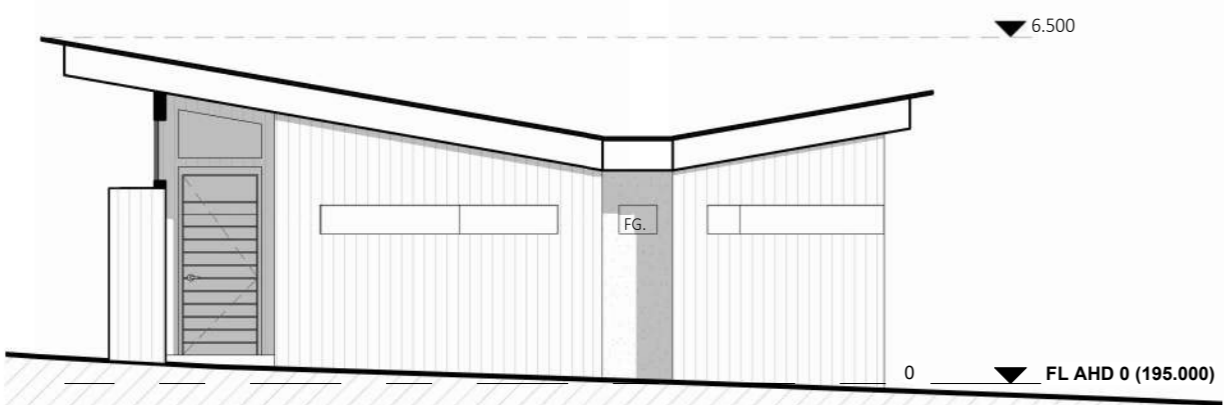
DWG SITE PERSPECTIVE EVEVATION		PROJECT # Project Number
PROJECT Visitors accommodation in Longfoed		DWG #
CLIENT DB INVESTMENTS	SCALE @ A3	A04
ADDRESS 31 Norwich drive Longford Tasmania	DRAWN CHKD RK JB	ACCREDITED DESIGNER 947052254

PLANNING APPLICATION

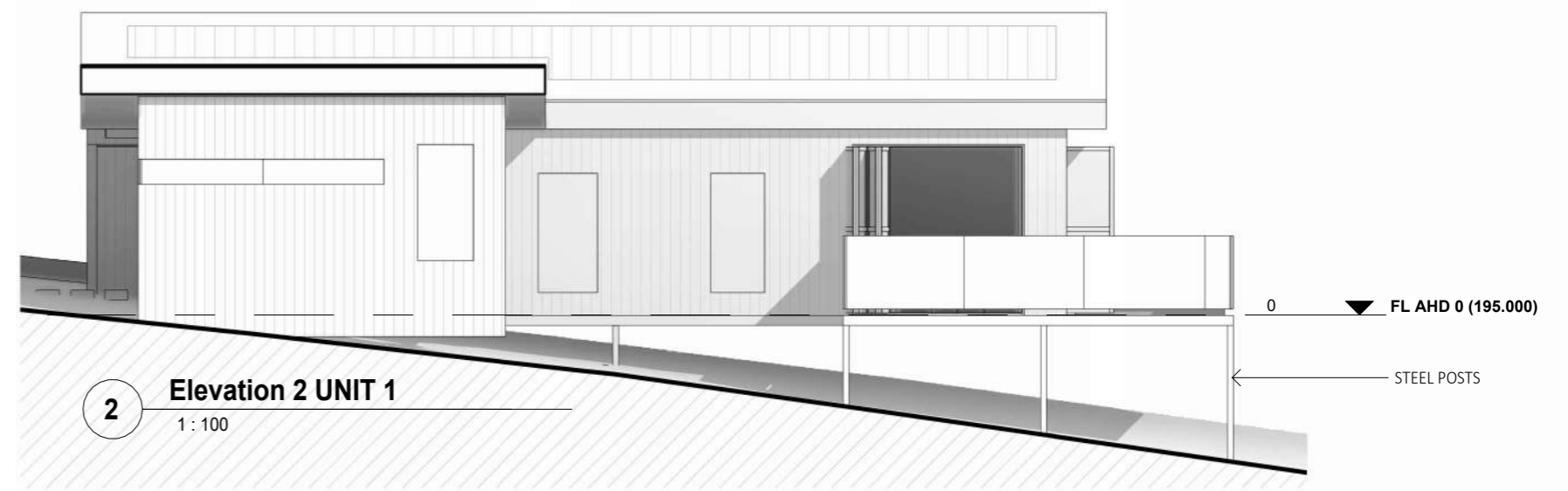


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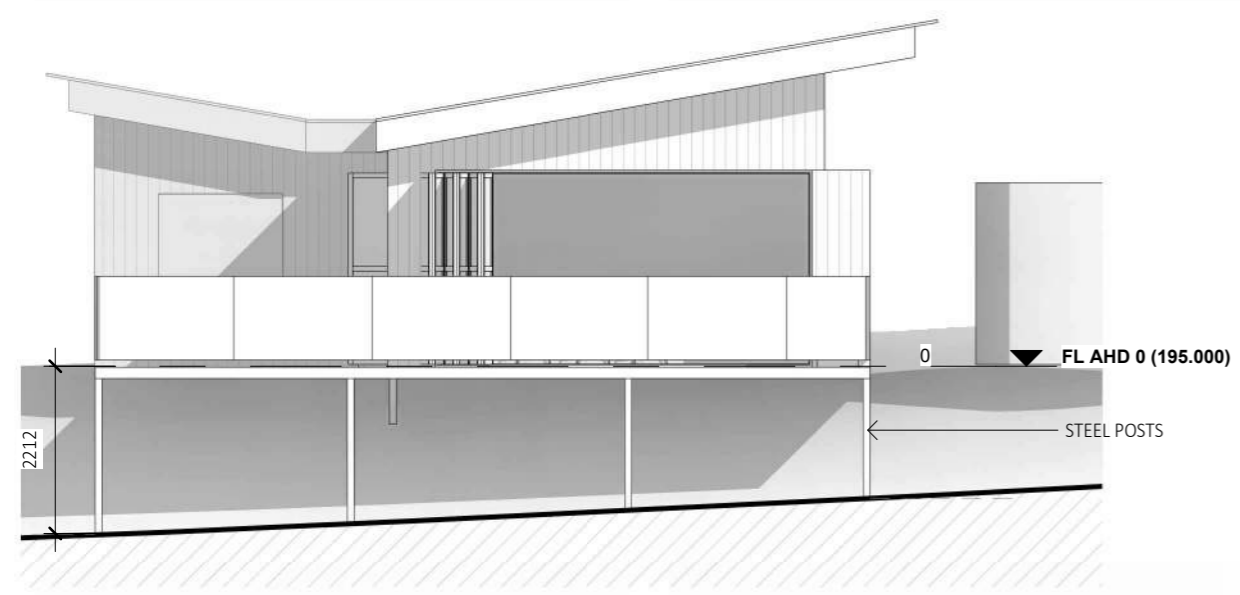
GENERAL NOTE
 WALL TYPE : CHARRED TIMBER CLADDING, COLOR : MONUMENT
 ROOF TYPE : 10 DEGREE COLOBOND CUSTOM ORB ROOF, COLOR : MONUMENT
 CEMENT FINISH FEATURE WALL
 WINDOW TYPE : ALUMINUM FRAME & DOUBLE GLAZED WINDOW SUITE
 BALUSTRADE : GLASS (FRAMELESS)
 FG. = FROSTED GLASS



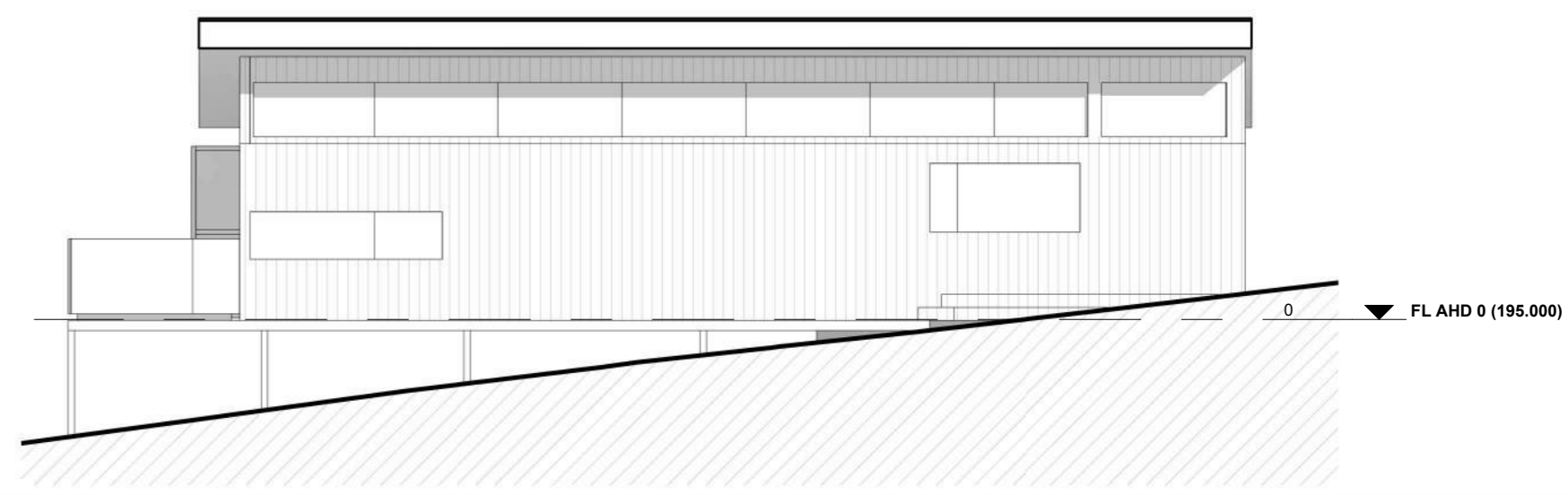
1 Elevation 1 UNIT 1
1 : 100



2 Elevation 2 UNIT 1
1 : 100



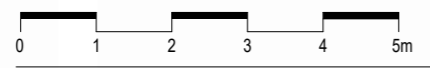
3 Elevation 3 UNIT 1
1 : 100



4 Elevation 4 UNIT 1
1 : 100

PLANNING APPLICATION

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0	08 - 02 - 2024	PRELIM WORK IN PROGRESS
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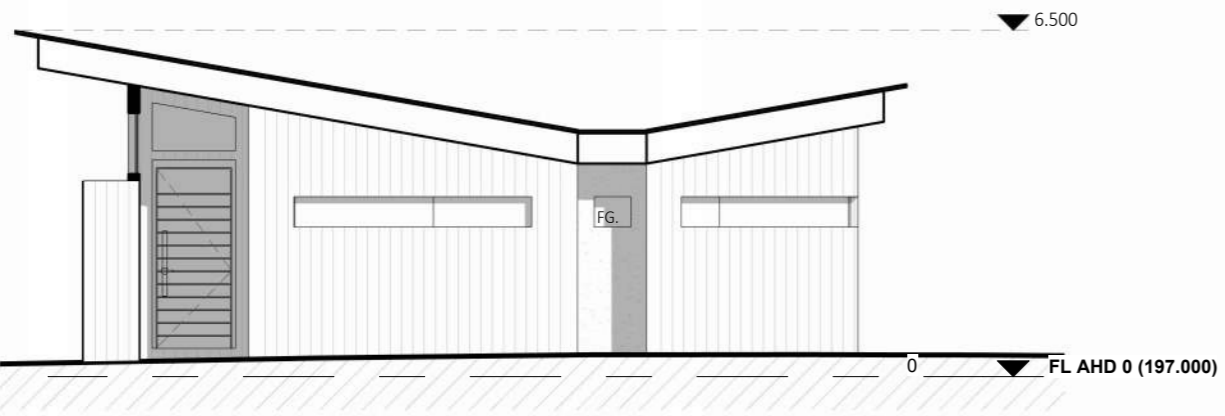
DWG	ELEVATION UNIT 1		PROJECT #	Project Number
PROJECT	Visitors accommodation in Longfoed		DWG #	A05
CLIENT	DB INVESTMENTS	SCALE @ A3	1 : 100	ACCREDITED DESIGNER 947052254
ADDRESS	31 Norwich drive Longford Tasmania	DRAWN	CHKD	

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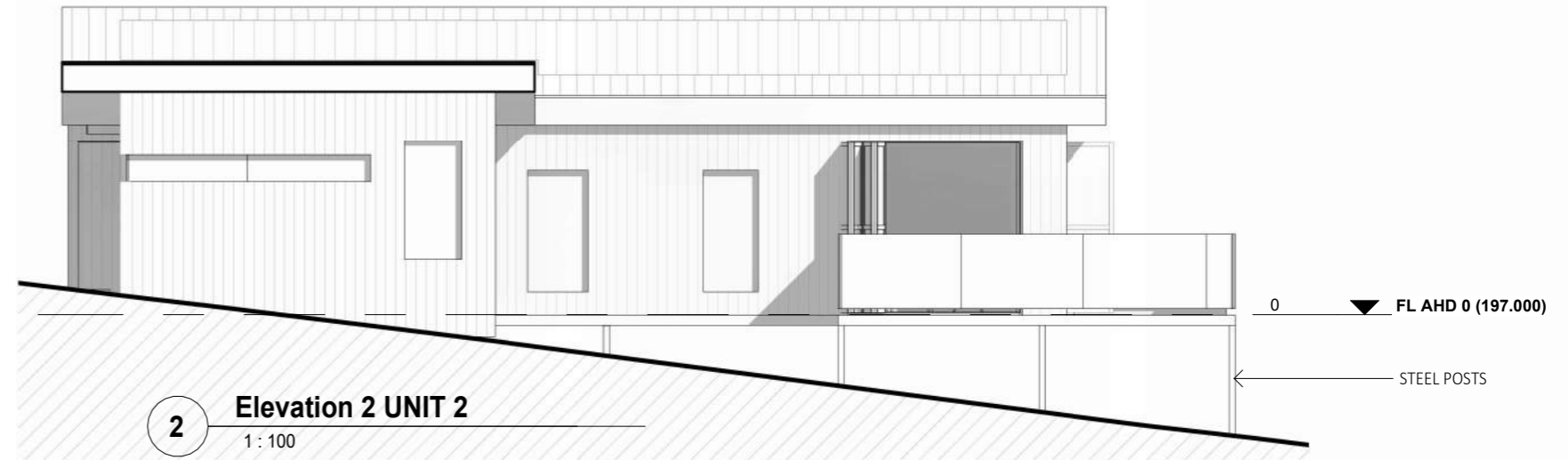


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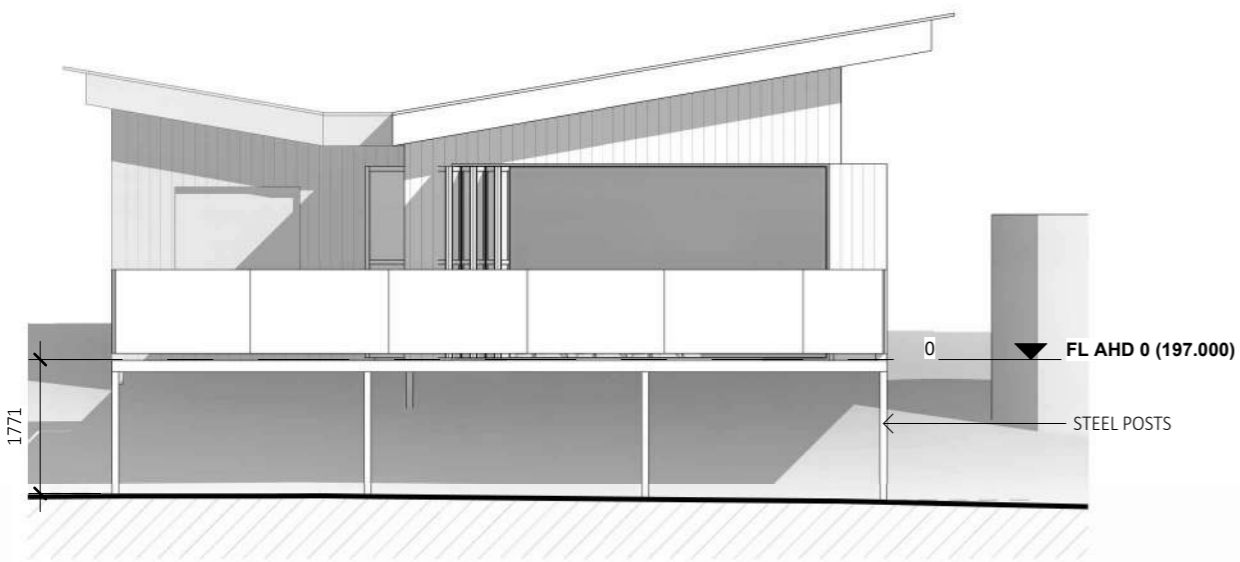
GENERAL NOTE
 WALL TYPE : CHARRED TIMBER CLADDING, COLOR : MONUMENT
 ROOF TYPE : 10 DEGREE COLOBOND CUSTOM ORB ROOF, COLOR : MONUMENT
 CEMENT FINISH FEATURE WALL
 WINDOW TYPE : ALUMINUM FRAME & DOUBLE GLAZED WINDOW SUITE
 BALUSTRADE : GLASS (FRAMELESS)
 FG. = FROSTED GLASS



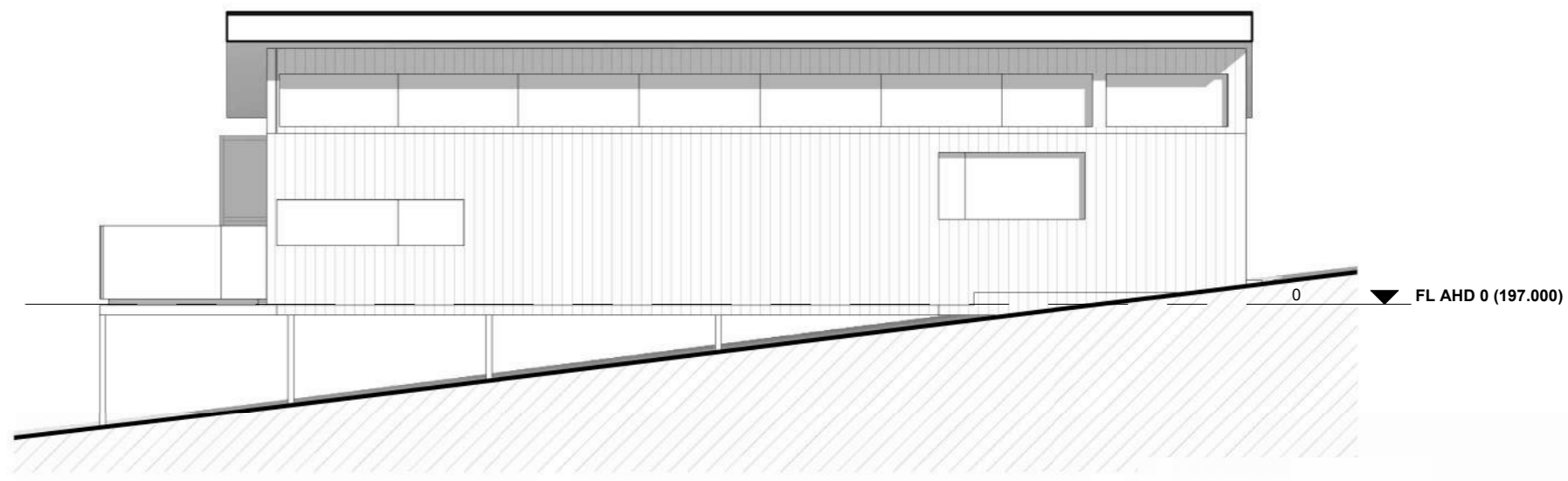
1 Elevation 1 UNIT 2
 1 : 100



2 Elevation 2 UNIT 2
 1 : 100



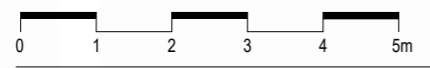
3 Elevation 3 UNIT 2
 1 : 100



4 Elevation 4 UNIT 2
 1 : 100

PLANNING APPLICATION

REV	DATE	DESCRIPTION
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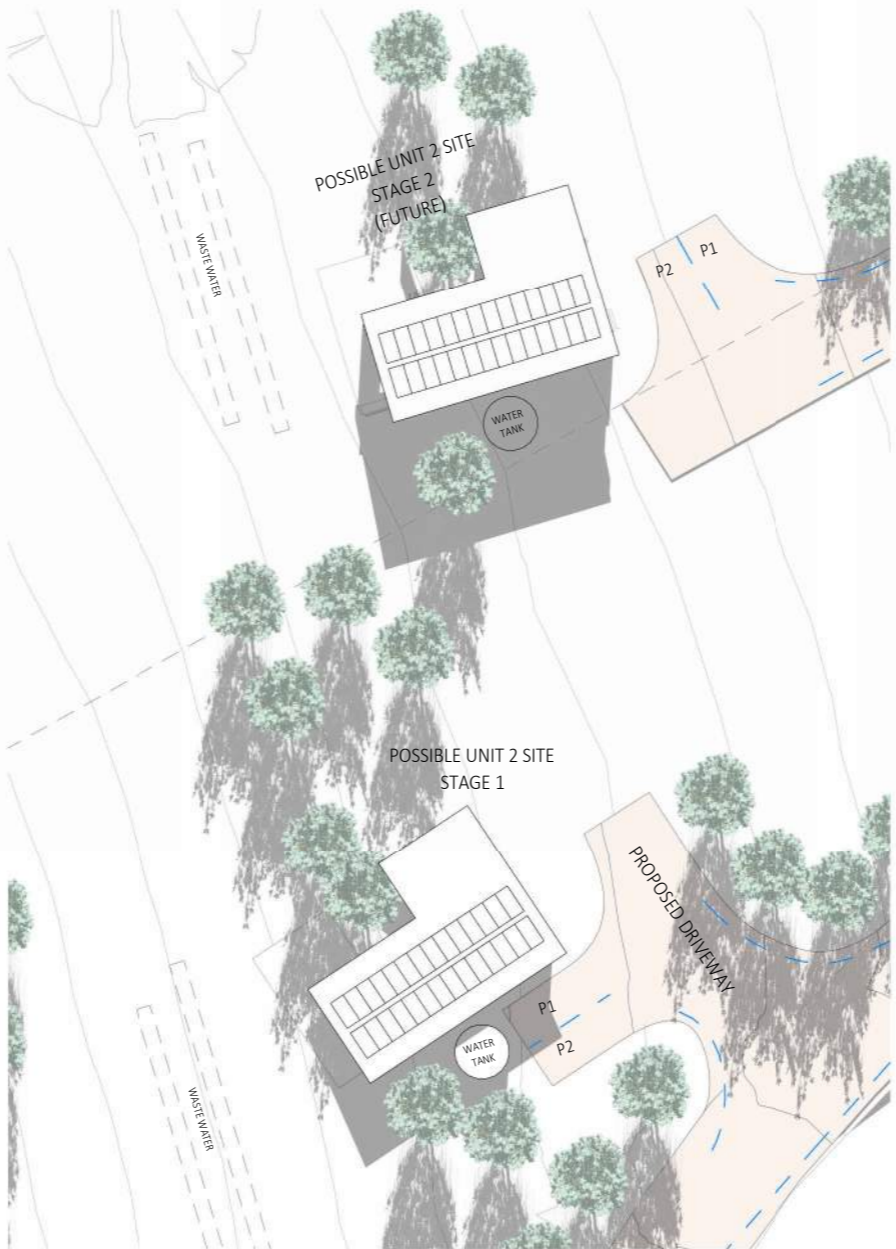


DWG ELEVATION UNIT 2		PROJECT # Project Number
PROJECT Visitors accommodation in Longfoed		DWG # A06
CLIENT DB INVESTMENTS	SCALE @ A3 1 : 100	ACCREDITED DESIGNER 947052254
ADDRESS 31 Norwich drive Longford Tasmania	DRAWN CHKD RK JB	

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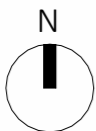


1 SHADOW DIAGRAM 21st JUNE AT 10 AM
1 : 500

2 SHADOW DIAGRAM 21st JUNE AT 12 PM
1 : 500

3 SHADOW DIAGRAM 21st JUNE AT 3 PM
1 : 500

PLANNING APPLICATION



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0	08 - 02 - 2024	PRELIM WORK IN PROGRESS
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2	02 - 04 - 2024	DEVELOPMENT APPLICATION



DWG SHADOW DIAGRAMS		PROJECT # Project Number
PROJECT Visitors accommodation in Longfoed		DWG #
CLIENT DB INVESTMENTS		A07
ADDRESS 31 Norwich drive Longford Tasmania		ACCREDITED DESIGNER 947052254
SCALE @ A3 1 : 500	DRAWN CHKD	RK JB

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2	02 - 04 - 2024	DEVELOPMENT APPLICATION

DWG	3D VISUALISATION		PROJECT #	Project Number
PROJECT	Visitors accommodation in Longfoed		DWG #	A08
CLIENT	DB INVESTMENTS		SCALE @	
ADDRESS	31 Norwich drive Longford Tasmania		DRAWN CHKD	RK JB ACCREDITED DESIGNER 947052254

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**PLANNING
APPLICATION**

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2	02 - 04 - 2024	DEVELOPMENT APPLICATION

DWG	3D VISUALISATION	PROJECT #	Project Number
PROJECT	Visitors accommodation in Longfoed	DWG #	A09
CLIENT	DB INVESTMENTS	SCALE @	A3
ADDRESS	31 Norwich drive Longford Tasmania	DRAWN CHKD	Author Checker ACCREDITED DESIGNER 947052254

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SURVEYING
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PLANNING SUPPORTING REPORT

Application for Use and Development – Visitor Accommodation

NORWICH DRIVE, LONGFORD

March 2024

Exhibited

Job Number: L231113

Prepared by: Michelle Schleiger (michelle@woolcottsurveys.com.au)

Town Planner

Reviewed by: James Stewart (james@woolcottsurveys.com.au)

Senior Planner

Rev. no	Description	Date
1	Draft	28 March 2024
2	Review	3 April 2024
3	Review	3 April 2024
4	Final	3 April 2024

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Exhibited

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Annexure 2 – Proposal plan.....	11

Exhibited

1. Introduction

This report has been prepared in support of a planning permit application under Section 57 of the Land Use Planning and Approval Act 1993.

This application is to be read in conjunction with the following supporting documentation:

Document	Consultant
Proposal Plan	Design Construction Management

1.1 Application and site summary

The following is a summary of the application and site information:

Address	NORWICH DRIVE LONGFORD TAS 7301
Property ID	2048542
Title	135661/2
Land area	10.60ha
Planning Authority	Northern Midlands Council
Easements	Right of Way
Application status	Discretionary application
Existing Access	Norwich Drive via right of way
Proposed Use	Visitor Accommodation
Proposed development	2 x accommodation units
Zone	Rural Living Zone D
General Overlay	None
Code Overlay/s	Airport obstacle limitation area Bushfire-prone areas Low landslip hazard band (partial) Priority vegetation area (partial)
Existing development	Single dwelling and various outbuildings (refer to site plan)

1.2 Proposal

This application is for use and development of the land for visitor accommodation. The proposal consists of the development of two accommodation units. The units will be identical, each having one bedroom, one bathroom, an open plan kitchen and living area, and a balcony/terrace.

The units will be positioned off the existing driveway; new sections of driveway will be made for access. The units will be elevated on the west elevation of the land, compensating for the slope of the land.

Additional vegetation is included in the plan to soften the boundary and potential visual impact of the buildings.

1.3 Site and context

The subject site is 10.6ha, irregular in shape, with frontage to Norwich Drive of 229.95m. The site is sloped with an elevated ridgeline at 200m AHD, sloping down to the west. Existing buildings, including a dwelling are located on the ridgeline, central to the lot. The site includes several outbuildings (sheds) from previous uses (farm sheds and stables).

Norwich Drive intersects with Pateena Road, 320m west of the subject site frontage, and terminates approximately 3km north east of the site, providing access to the Rural Living lots in residential cluster this south east of Longford.



Figure 1 Aerial view of the subject site (Source: LIST)

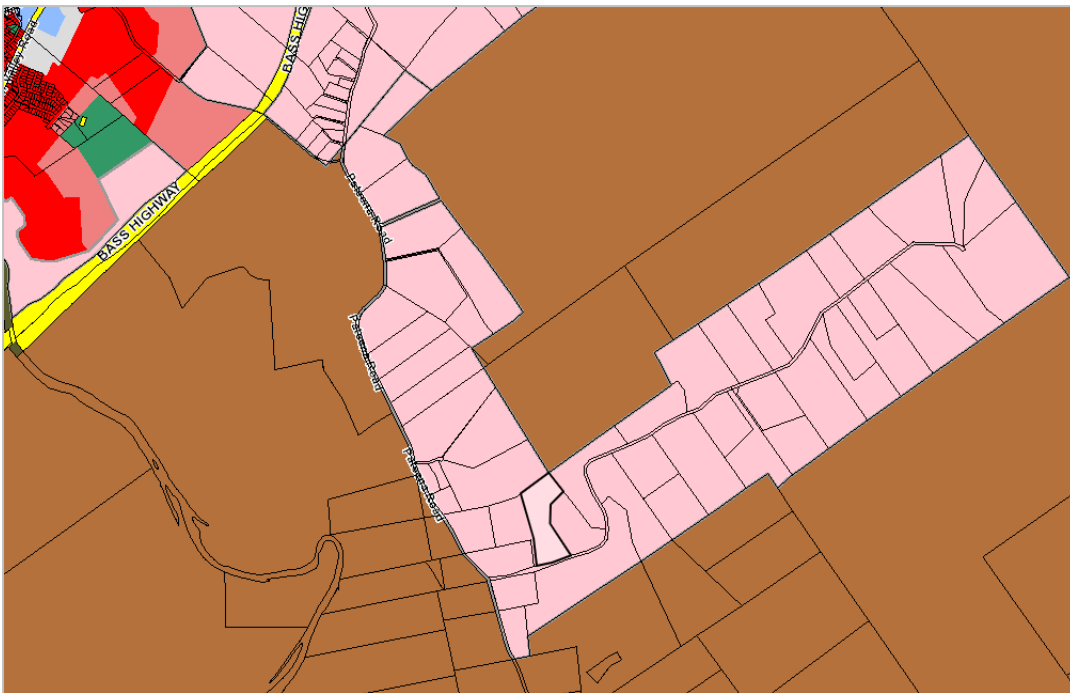


Figure 2 Zoning of the subject site and surrounding area (Source: LIST)

The surrounding area is predominantly Agriculture Zone.

The site is wholly covered by the Bushfire-prone areas overlay and the Airport obstacle limitation area overlay. It is partially affected by the Low landslip hazard band overlay and the Priority vegetation area overlay.

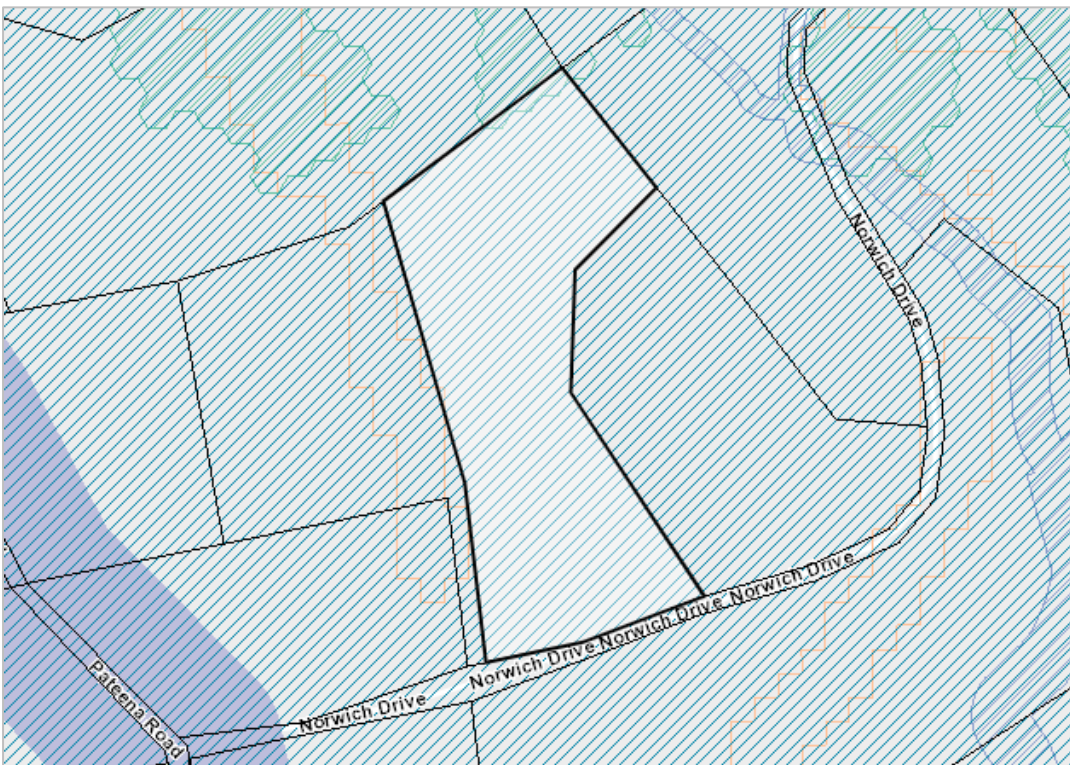


Figure 3 Overlays affecting the subject site (Source: LIST)

2. Planning Assessment

2.1 Planning Scheme Zone Assessment

11.0 Rural Living Zone

11.1 Zone Purpose

Zone purpose	
11.1.1	To provide for residential use or development in a rural setting where: (a) services are limited; or (b) existing natural and landscape values are to be retained.
11.1.2	To provide for compatible agricultural use and development that does not adversely impact on residential amenity.
11.1.3	To provide for other use or development that does not cause an unreasonable loss of amenity, through noise, scale, intensity, traffic generation and movement, or other off site impacts.
11.1.4	To provide for Visitor Accommodation that is compatible with residential character.

Response

Clause 11.1.4 is relevant to the application. The proposal is for 2 visitor accommodation units on the site where there is an existing dwelling. The two units are each modest in size and each have only one bedroom. The Use of them would be comparable and compatible to residential character, being stand-alone units. The size of the units means use will be low impact, limiting the number of visitors to the site and keeping the use compatible to the residential and rural character. The proposed is compliant with the purpose of the zone.

11.2 Use Table

Permitted	Qualification
Visitor Accommodation	-

11.3 Use Standards

11.3.1 Discretionary uses

Objective	
That Discretionary uses do not cause an unreasonable loss of amenity to adjacent sensitive uses.	
Acceptable Solutions	Performance Criteria
<p>A1 Hours of operation for a use listed as Discretionary, excluding Emergency Services or Residential use, must be within:</p> <ul style="list-style-type: none"> a) 8.00am to 6.00pm Monday to Friday; b) 9.00am to 12.00 noon Saturday; and 	<p>P1 Hours of operation for a use listed as Discretionary, excluding Emergency Services or Residential use, must not cause an unreasonable loss of amenity to adjacent sensitive uses, having regard to:</p> <ul style="list-style-type: none"> a) the timing, duration or extent of vehicle

c) nil on Sunday and public holidays.	movements; and b) noise or other emissions.
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Response

A1 The proposed Use is not Discretionary.

A2 External lighting for a use listed as Discretionary, excluding Residential use: a) must be within the hours of 7.00pm to 7.00am, excluding any security lighting; and b) security lighting must be baffled so that direct light does not extend into the adjoining property.	P2 External lighting for a use listed as Discretionary, excluding Residential use, must not cause an unreasonable loss of amenity to adjacent sensitive uses, having regard to: a) the number of proposed light sources and their intensity; b) the location of the proposed light sources; c) the topography of the site; and d) any existing light sources.
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Response

A2 The proposed Use is not Discretionary.

A3 Commercial vehicle movements and the unloading and loading of commercial vehicles for a use listed as Discretionary, excluding Emergency Services or Residential use, must be within the hours of: a) 7:00am to 5:00pm Monday to Friday; b) 9:00am to 12 noon Saturday; and c) nil on Sunday and public holidays.	P3 Commercial vehicle movements and the unloading and loading of commercial vehicles for a use listed as Discretionary, excluding Emergency Services, must not cause an unreasonable loss of amenity to adjacent sensitive uses, having regard to: a) the extent and timing of traffic generation; b) the dispatch of goods and materials; and c) the existing levels of amenity.
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Response

A3 The proposed Use is not Discretionary.

11.3.2 Visitor Accommodation

Objective	
That Visitor Accommodation: (a) is compatible with the character and use of the area; (b) does not cause an unreasonable loss of residential amenity; and (c) does not impact the safety and efficiency of local roads or rights of way.	
Acceptable Solutions	Performance Criteria
A1 Visitor Accommodation must: a) accommodate guests in existing habitable buildings; and b) (b) have a gross floor area of not more than 200m ² per lot.	P1 Visitor Accommodation must be compatible with the character and use of the area and not cause an unreasonable loss of residential amenity, having regard to: a) the privacy of adjoining properties; b) any likely increase in noise to adjoining properties; c) the scale of the use and its compatibility with the surrounding character and uses within the area; d) retaining the primary residential function

	<p>of an area;</p> <p>e) the impact on the safety and efficiency of the local road network; and</p> <p>f) any impact on the owners and users rights of way</p>
--	--

Response

P1 The proposed Visitor Accommodation is not in an existing building. Each unit is 110m² so exceeds the allowable area for the acceptable solution. The performance criteria are addressed.

The proposed Units are compatible with the surrounding character and low impact to residential amenity being modestly sized and set into the landscape.

- a. The setbacks of the Units are 25.3m and 37m from the west side boundary. Although they are higher in elevation, they are offset to the neighbouring dwelling, set further south than the existing dwelling on the site, preventing direct views to the dwelling and private open space at 27 Norwich Drive.
- b. The units are each only 1 bedroom so large groups of people are not possible. Setbacks are sufficient to ensure normal household noise will not interfere with neighbouring amenity.
- c. The two units are modest in scale, being slightly larger than a typical granny flat. The spacing between the units, and the existing dwelling, combined with additional plantings result in a compatible scale and spatial relationship of development to the surrounds.
- d. The dwelling on the site is to be retained and the addition of the units is low impact to the residential use on site and in the area. The area is not at risk of a proliferation of accommodation over residential use.
- e. As there are only two units, the addition to the road network will not be significant. It is likely that each unit would only have a single vehicle, and visitor accommodation tends to generate only 3 vehicle trips (average) a day¹.
- f. The section of easement for right of way is 40m in length, the section of shared driveway is closer to 27m in reality. The driveway has a distinct fork so it is clear how the access is used, and there is adequate space for sightlines and ability to yield. The access section is sealed and free from encumbrances. Being only located at the access point, or front boundary, additional users as a result of the proposed, will not impact on the dwelling at 27 Norwich Drive, or their usable space.

<p>A2 Visitor Accommodation is not for a strata lot that is part of a strata scheme where another strata lot within that strata scheme is used for a residential use.</p>	<p>P2 Visitor Accommodation within a strata scheme must not cause an unreasonable loss of residential amenity to long term residents occupying other strata lots within the strata scheme, having regard to:</p> <ul style="list-style-type: none"> a) the privacy of residents; b) any likely increase in noise;
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¹ Rate taken from 'Motels' in the RTA Guide to Traffic Generating Developments as the nearest available comparison. (Roads and Maritime Services NSW, 2002)

	<ul style="list-style-type: none"> c) the residential function of the strata scheme; d) the location and layout of the strata lots; e) the extent and nature of any other non-residential uses; and f) any impact on shared access and common property
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Response

A2 The acceptable solution is achieved. The proposed is not for a strata lot and no strata scheme has been applied to the land.

11.4 Development Standards for Buildings and Works

11.4.1 Site coverage

Objective	
<p>That the site coverage:</p> <ul style="list-style-type: none"> a) is compatible with the character of existing development in the area; and b) assists with the management of stormwater runoff. 	
Acceptable Solutions	Performance Criteria
<p>A1 The site coverage must be not more than 400m².</p>	<p>P1 The site coverage must be consistent with that existing on established properties in the area, having regard to:</p> <ul style="list-style-type: none"> a) the topography of the site; b) the capacity of the site to absorb runoff; c) the size and shape of the site; d) the existing buildings and any constraints imposed by existing development; e) the need to remove vegetation; and f) the character of development existing on established properties in the area.

Response

P1 The performance criteria are addressed. The site coverage exceeds 400m² as the site has an existing dwelling (approximately 200m²) and numerous farm sheds.

- a. The site is elevated with a ridgeline running almost north south, with the down slope to the west.
- b. The site is over 10ha and has capacity to accommodate runoff through retention and/or absorption. The land is not serviced for reticulated water, so catchment is required. Overflow can be managed on site.
- c. The site is irregular in shape but is suitably dimensioned to allow generous setbacks to all boundaries.

- d. The existing buildings do not constrain the proposed. The existing buildings are generally clustered to the central part of the lot.
- e. No removal of vegetation is proposed.
- f. Development in the area exhibits, for the most part, generous setbacks and open fields with scattered vegetation (mature trees). The area is very rural in character, however, the style of building design is varied with the dwelling at 48 Norwich Drive being prominent in the landscape, and other dwellings shielded from view by the topography or the vegetation. Likewise, the two units will be setback from the frontage and partly shielded from view by the topography of the site and by existing and planned vegetation.

11.4.2 Building height, setback and siting

Objective	
<p>That height, setback and siting of buildings:</p> <ul style="list-style-type: none"> a) is compatible with the character of the area; b) does not cause an unreasonable loss of amenity; c) minimises the impact on the natural values of the area; and d) minimises the impact on adjacent uses. 	
Acceptable Solutions	Performance Criteria
<p>A1 Building height must be not more than 8.5m.</p>	<p>P1 Building height must be compatible with the character of the area and not cause an unreasonable loss of amenity to adjoining properties having regard to:</p> <ul style="list-style-type: none"> a) the topography of the site; b) the height, bulk and form of existing buildings on the site and adjoining properties; c) the bulk and form of proposed buildings; d) sunlight to habitable rooms and private open space in adjoining properties; and e) any overshadowing of adjoining properties or public places.

Response

A1 The acceptable solution is achieved, the building height on each unit does not exceed 8.5m.

<p>A2 Buildings must have a setback from a frontage of not less than 20m.</p>	<p>P2 Buildings must be sited to be compatible with the character of the area, having regard to:</p> <ul style="list-style-type: none"> a) the topography of the site; b) the setbacks of adjacent buildings; c) the height, bulk and form of existing and proposed buildings; d) the appearance when viewed from roads and public places; and e) the retention of vegetation.
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Response

A2 The acceptable solution is achieved. The setback from the frontage is 160m.

<p>A3 Buildings must have a setback from side and rear boundaries of not less than 10m.</p>	<p>P3 Buildings must be sited to not cause an unreasonable loss of amenity to adjoining properties, having regard to:</p> <ul style="list-style-type: none"> a) the topography of the site; b) the size, shape and orientation of the site; c) the setbacks of surrounding buildings; d) the height bulk and form of existing and proposed buildings; e) the character of the development existing on established properties in the area; and f) any overshadowing of adjoining properties or public places
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Response

A3 The acceptable solution is achieved. Development has a setback of more than 10m from all side and rear boundaries.

<p>A4 Buildings for a sensitive use must be separated from an Agriculture Zone or Rural Zone a distance of:</p> <ul style="list-style-type: none"> a) not less than 200m; or b) if the setback of an existing building is within 200m, not less than the existing building. 	<p>P4 Buildings for a sensitive use must be sited so as to not conflict or interfere with uses in the Agriculture Zone or Rural Zone, having regard to:</p> <ul style="list-style-type: none"> a) the size, shape and topography of the site; b) the separation of any existing buildings for sensitive uses on adjoining properties; c) the existing and potential use of adjoining properties; d) any proposed attenuation measures; and e) any buffers created by natural or other features.
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Response

A4 The acceptable solution is achieved. No proposed buildings are within 200m of the Agricultural Zone.

2.2 Planning Scheme Code Assessment

C2.0 Parking and Sustainable Transport Code

C2.5 Use Standards

C2.5.1 Car parking numbers

A1 The acceptable solution is achieved. Each unit requires 1 car parking space. Two are included on each together with manoeuvring space.

C2.6 Development Standards

C2.6.1 Construction of parking areas

A1 The acceptable solution is achieved. The proposed sections of driveway and parking area will be sealed to match the existing.

C2.6.2 Design and layout of parking areas

A1.1 The parking areas will be compliant with the relevant standards (Australian Standard AS 2890- Parking facilities, Parts 1-6).

C2.6.3 Number of accesses for vehicles

A1 The acceptable solution is achieved. The accesses will remain as existing (b).

C3.0 Road and Railway Assets Code

C3.5 Use Standards

C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction

A1.4 The acceptable solution is achieved. According to the RMS guide (Roads and Maritime Services NSW, 2002) (Roads and Maritime Services NSW, 2013) a regional residence generates an average of 7.4 vehicle movements per day, and each unit would generate 3 vehicle movements per day (average). Combined this equates to 13.4 movements per day which is within the acceptable increase according to Table C3.1.

C7.0 Natural Assets Code

The overlay only applies to the north east end of the lot. No vegetation is proposed to be removed. The Code does not apply.

C15.0 Landslip Hazard Code

The overlay only applies to the west boundary area of the lot. No part of the proposal is affected. The Code does not apply.

C16.0 Safeguarding of Airports Code

Not applicable as development is below the height criteria.

3. Conclusion

The proposal for use and development seeks the development of two visitor accommodation units on the land. The land has an existing dwelling and several outbuildings accessed from Norwich Drive. The Units will be well setback from the frontage and generally only visible to the public from vantage points on Norwich Drive on the west side of the lot. Additional plantings will provide visual shield from the road and from neighbours to the development, and further soften the landscape against the boundary.

Exhibited

The Units are modestly sized, suitable for one or two persons each, so the impact from the and development use will be low. Additional vehicle movements are not expected to create an impact of significance and the access point is well designed and surfaced for the existing and proposed use.

Each Unit will be self sufficient in terms of servicing, with water catchment, wastewater management and stormwater overflow area.

The proposal meets the relevant standards of the Tasmanian Planning Scheme – Northern Midlands, and a planning permit from Council is sought accordingly.

Annexure 1 – Copy of title plan and folio text **Annexure 2 – Proposal plan**

Exhibited



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